



December 21, 2021

Mr. Dave Whitfield, Planning Manager
Planning Department
Municipality of Anchorage
P.O. Box 196650
Anchorage, Alaska 99519-6650

SUBJECT: Glacier City Center
Development Master Plan

Dear Mr. Whitfield:

Z Architects is submitting a development master plan review application package, per AMC 21.09.030.F, for the Glacier City Center to be located on Tract E3 (*property identification numbers: Parcel: 075-031-41-000*) located within Girdwood Elementary School Subdivision in Girdwood, Alaska.

Property owner and applicant is Glacier City Development LLC. Mailing address: PO Box 274, Girdwood, AK 99587. Phone: (907) 351-6636. Email: nathen49@gmail.com

Schematic design drawings including site and grading plan, floor plans, building elevations, renderings, and a landscape plan have been provided as part of this Development Plan. However, the plans may change slightly as the designs are further developed. The drawings will be finalized and submitted for a Land Use permit for summer 2022 construction.

It is our understanding that the project will be placed on the February 14, 2022 Planning and Zoning Commission agenda. If this is not the case, please let me know.

If you have any questions or require additional information, please contact me at 907-783-1090 or marco@zarch-ak.com. Thank you.

Sincerely,
Z Architects, LLC

Marco Zaccaro
Attachments: As stated.

*P2C: 02/14/2022
CASE No. 2022-0017*



Glacier City Center Development Master Plan

December 21, 2021



21-25 Glacier City Center Development Master Plan Application rev. 12-18-21

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Glacier City Center
Development Master Plan Narrative
December 2021

Request for Approval

We are requesting a Development Master Plan Approval for the development of the Glacier City Center located on Tract E3 of the Girdwood School Subdivision in Girdwood, Alaska. Please see the narrative below and the attached plans for more information.

Introduction

The project is located on Tract E3, Girdwood School Subdivision in Girdwood, Alaska. The parcel is 1.5 acres situated between California Creek and Hightower Rd and located approximately 40 miles south of Anchorage. In 2001, the Girdwood Commercial Areas and Transportation Master Plan was developed to assist in guiding development in the Girdwood area.

Nathen and Liz Ellis, who recently purchased Tract E3 are developing Glacier City Center, they have lived in Girdwood for several years and have children attending Girdwood School. The previous owner had worked to develop a mixed commercial center on the site but the 2008 financial Crisis caused the project to be put on hold. The Ellis' saw the parcel as having potential to alleviate some of Girdwood's housing needs as well as provide economic support with a commercial building. This horizontal mixed-use development concept allows them to develop their vision.

The project proposes development in two phases of ten townhouse-style multifamily dwellings and a separate commercial building. The dwellings will be market rate condominium units. The commercial building will come in a later phase and will contain a craft distillery, sandwich shop and coffee shop. The total gross floor area of the project will be 28,326 sf (19,440 sf in the townhouses and 8,886 sf in the commercial building (including a 685 sf mechanical mezzanine)). Housing continues to be a significant obstacle to current and would-be residents of Girdwood. Local businesses identify recent upward trends in housing prices as an obstacle to employee retention. The intent of the project is to provide more housing options in the New Townsite area.

The purpose of the Development Master Plan is to provide a clearly articulated vision for the character, layout, and design of the new Glacier City Center in accordance with local adopted plans and the MOA Girdwood Land Use Regulations AMC 21.09, while also complying with the requirements of AMC 21.09.030F. Development Master Planning.

Community Meeting

Representatives of Z Architects and the Ellis' presented the project to the Girdwood Land Use Committee (LUC) on 11 October 2021 and the Girdwood Board of Supervisors (GBOS) on 18 October 2021. The 11 October meeting served as the community meeting, and notices were mailed more than 21 days before. The MOA provided the mailing list. The submittal of this application occurs prior to the cut-off date for the Planning and Zoning Commission hearing on 14 February 2022. The project team anticipates presenting to the LUC on 10 January 2022 and to GBOS on 17 January 2022 requesting a letter of support.

Attendees generally supported the project and saw this as a welcomed addition to the community (see appended meeting minutes).

Conformance with Municipality of Anchorage Title 21.09.030F Development Master Plan

A. Submittal Requirements:

A. Lot Description

The legal description, acreage, and boundaries of the proposed petition area, an explanation of boundary delineation, pursuant to subsection 21.09.030C, if applicable.

The Development Master Plan area is Tract E3. The project area is approximately 1.5 acres (65,512 sq ft) and is located on the west side of Hightower Rd immediately north of the Girdwood Post Office. Total building footprint is approximately 10,765 sf of the project area.

The project site is zoned gC-8, New Townsite North Commercial. The property is surrounded on two sides by the gC-6, Crow Creek Road Commercial/Residential zoning (three sides if the zoning west of California Creek is counted), and the areas to the north and east, immediately across platted rights-of-way, have been developed exclusively with multifamily housing in accordance with the gC-6 standards. The parcel has much more in common in both size and conditions with this existing development, surrounding it, than with the much smaller lots and densities along Hightower Rd to the south. Southeast of the parcel, across Hightower Rd, is a mobile house park with gC-8 zoning. The parcel immediately south of the site is the Girdwood Post Office. It has gC-8 zoning as does a small HLB parcel (Girdwood Post Office Subdivision, Lot 2) just west of the Post Office. The Anchorage School District parcel to the west (Tract E5, Girdwood

Elementary School) is zoned GOS (Girdwood Open Space). This parcel contains California Creek.

B. Existing Site Plan

A site plan of any existing development, including buildings, roads, utilities, drainage systems, trails, and a general description of existing vegetation.

See attached sheet C1.0 for existing site plan. Tract E3 is currently vacant land, it was cleared of natural vegetation at some time in the past and has areas of new vegetative growth. This vegetation consists of grasses and low woody plants, mature shrubs like alder and willow, and cottonwood trees. Larger isolate conifers predate previous development. The west property line roughly parallels an earthen dike that defines the California Creek channel.

There are several easements that encumber the property. There is a T&E easement along Hightower Rd. Along the west side of the property are two easements, a 25' building setback that was intended to provide protection to California Creek. This stream protection easement may be vacated because the stream protections in AMC Title 21 are a better method for regulating development. Another T&E easement follows this easement along the property line. Chugach Electric Association has agreed to re-align a portion of this easement (see attached sketch dated December 2, 2021). All the utility easements are in use. A water main and sewer main run below Hightower Rd, and a storm drain system terminates at the very south corner of the property.

There has been nascent community use including camping, dumping, and during Girdwood Forest Fair the site served as a temporary parking area. Tiny Creek Trail runs adjacent to California Creek off the property and approximately 90 feet west of the nearest proposed development.

C. Site Topography

The topography of the petition area, with contours lines shown at intervals of four feet or less, including any unique natural or historical features.

Tract E3 is generally level. However, there is an inconsistent downward slope toward the south. There are local low areas as well as a low ridge west of the property boundary. See attached existing site plan C1.0 with one foot interval contour lines.

There are no known historic features on the property and development is unlikely to yield any archeological features because of prior disturbance (clearing and grubbing). The Girdwood area may be within the traditional lands of the Upper Inlet Dena'ina, and Tribal consultation will occur if construction does identify historic features.

D. Existing Streams and Waterbodies

The location of existing streams, water bodies, wetlands, drainage courses, and flood plains, and proposed changes to such features.

California Creek runs parallel to the west boundary of Tract E3. FEMA mapping for the area designates most of the site as Zone AE, which indicates areas with 0.2% annual chance flood hazard. A portion of the lot is in Zone AO which is a special flood hazard area with a base flood elevation. No building is located in this portion of the site. The southeast corner of the site is outside of mapped flood hazard zones. There are no mapped wetlands that impact the property.

E. Grading Plan

A grading plan and detailed study of existing and proposed drainage, using baseline climatic and environmental data that reflects Girdwood conditions.

See attached sheet C2.0 for the proposed drainage plan.

The proposed development for Glacier City Center will disturb approximately 1.25 acres of vegetated ground cover. The Anchorage Stormwater Manual (ASM) classifies this level of disturbance as a Large Project which must meet the following requirements: Water Quality Treatment; Extended Detention; Conveyance; Detention and Peak Flow Control; Operation and Management Plan; Stormwater Management Report; and Wetland Compliance.

The completed improvements will follow the existing topography to the extents possible to maintain existing drainage pathways and use new landscaped areas to meet the water quality treatment objective. Other stormwater controls that may be considered to meet the Water Quality and Detention objectives are: vegetative swales, filter strips, natural vegetation retention, landscaped depressions, and dry ponds.

The soils on site are generally well draining alluvial gravels and deposits. A geotechnical report is available upon request.

F. Proposed Site Plan

A proposed site plan, showing roads, trails, building locations and uses, parking lots, open space, and any other proposed development. The site plan shall include the total number and type of dwelling units, and the total combined floor area of commercial and industrial uses.

See sheet A0.1 for proposed site plan. Development will include construction of three structures: a commercial building having 8,886 sf and two structures each with five townhouse-style dwelling units. These 10 dwellings will have two bedrooms and an attached garage. Other site development will be a parking area and landscaped areas. Part of the site is reserved for a future building that is not part of this Development Master Plan. A pedestrian pathway will be on-site that parallels Hightower Road and provides connection to the New Girdwood Townsite to the south and adjacent parcels to the north.

As discussed in *B. Existing Site Plan*, the proposed development will require vacation of the building setback stream protection easement and the electrical and telephone lines that run through the western T&E easement will need re-aligning and vacation of a portion of the utility easement.

G. Landscape Plan

A landscape plan, including vegetation retention areas.

See sheet L0.1 for proposed landscape plan. The much of the west portion of the tract is within the stream setback of California Creek. The project will retain this area as natural vegetation. Other setbacks will have new vegetation, as will islands in the parking area.

H. Building Design

Floor plans, building elevations, and renderings for all buildings.

See architectural drawings. The proposed residential buildings have three levels. The commercial building has two levels.

I. Road Cross-Sections

Road cross-sections.

See civil drawings. Section drawings provided through driveways.

J. Other Development Details

Details of any other development proposed.

See Landscape drawings for planting area details.

K. Schedule

An implementation schedule. See attached Design, Permitting and Construction Timeline.

- Spring 2022: Approval of Development Master Plan and Easement Vacation approval.
- Early summer 2022 through early summer 2023: Residential building and site improvement construction. Construction will take roughly twelve months to complete.
- Summer 2023: Residential buildings approved for occupancy.
- Summer 2023 through 2028: Commercial Building A and associated site improvement construction. Construction will take roughly one calendar year to complete. However, changes in market demands, material and labor availability, and other unforeseen factors may delay groundbreaking from Summer 2023.
- Summer 2024 through 2029: Building A approved for occupancy.
- Development Master Plan will be fully implemented by 2029.

B. Development and Design Standards

The development master plan shall establish specific circulation systems; land uses; site dimensional, design, and development standards; and building design standards for its development areas. These elements may set different development standards than those found in title 21 including sections 21.09.060, 21.09.070, and 21.09.080. The development master plan shall list the specific sections of title 21 for which different standards are to be established. Where different standards are approved in the development master plan, those standards shall be applied instead of the corresponding

standards in title 21.

This section details the code standards this plan modifies, subject to approval. Each code reference includes its Anchorage Municipal Code citation, title, and text. Following is a proposed alternative standard or statement demonstrating the proposed design meets or exceeds the code standard. Each of these will be noted as a DESIGN ALTERNATIVE or INFORMATION. Design alternatives propose an equal or better standard to a section of code to meet the intent of adopted standards. Informational sections do not offer an alternative but note another action from a different agency like an Easement Vacation. At the end of each bullet, a commentary section provides greater clarity to the proposed alternative and offers justification. For ease of review, the citations start in Chapter 9, Girdwood Land Use Code, and then continue with pertinent sections from the remainder of Title 21.

- 21.09.040D.3.a. [District-Specific Standards for Commercial Districts] Ground-Floor Residential:
 - *In gC-2, gC-5, gC-7 and gC-8, residential uses are permitted only on the upper story above the ground floor. Commercial and/or other non-residential uses shall occupy the ground floor.*
 - DESIGN ALTERNATIVE:
 - Provide two stories of commercial uses along Hightower Road to encourage pedestrian traffic extending north of the New Girdwood Townsite. Multiple entries and garage doors associated with residential units are away from Hightower Road, so they do not disrupt the pedestrian edge.
 - Encourages residential units to occupy the most desirable part of the site from a livability standpoint. Discourage sandwiching of residential units between Hightower Road and the parking lot as the standard suggests. Residential units occupy a more private and desirable part of the site adjacent to the California Creek riparian edge zone, making the residences more attractive to live in. An entry vestibule is the only living space on the ground floor of each residential unit.
 - Commentary: The intents of these zones are to create a commercial-lined street front that encourages pedestrian traffic. The buildings are to have

residential uses in the upper floors. The main reasons for this are to a) provide a welcoming street edge and shopping for visitors b) encourage people to live in the district c) provide privacy for the residents and d) reduce residential disruptions, on the ground floor, to the commercial street edge. These proposed townhouses are away from Hightower Rd and any pedestrian traffic along it. These buildings enhance the desired outcomes: placing the unit garages on the ground floor provides the residential areas greater privacy, placing them away from Hightower on the most desirable part of the site, from a residential standpoint, encourages people to live in the district and allows individual garages (also highly desirable) without disrupting the street edge. The proposed design exceeds the standard in code.

- 21.09.040C.3.b. Arcades and Covered Walkways in the New Girdwood Townsite:
 - *In [...] gC-8 [...], arcades or roofed walkways are required along the street face of all new buildings and as part of any application for modification or redevelopment. In the gaps between buildings within these districts, privately maintained walkways are required. Development standards for arcades and walkways on building frontages are established in subsection 21.09.080E.5.d.*
 - DESIGN ALTERNATIVE:
 - Building A provides a covered walkway along its street-face per the district standard. A sidewalk, North of building A, dedicated to public use, privately maintained and lighted, occupies the same portion of the front yard setback as the arcade. This sidewalk extends to California Creek Way right-of-way and if, in the future, the northeast corner of the site becomes economically developable, the resulting commercial building will have an arcade meeting code. In the gap between the two commercial buildings the walkway will meet the standard. As a result, Hightower Road edge will meet the district standard. Building A main entry is connected to the Hightower Road arcade by a roofed walkway, a feature which exceeds the district standard.
 - Multifamily buildings are located away from the street edge to a private part of the site; the design alternative deletes the continuous roofed walkway from their front façade because these

are private front yard spaces, and it is not desirable to encourage pedestrian traffic through them. Additionally, a covered walkway at this location would be impractical.

- Commentary: This desire for catering to pedestrians appears in the requirements for arcades and covered walkways. Girdwood has heavy rain and snow quantities, and these are nice amenities. For this reason, the commercial building incorporates roofed walkways in its design. However, the multifamily buildings do not provide a continuous covered walkway. Instead, a deck on the level above and six-foot-deep front roof overhangs offers weather protection for each unit and garage entrance. Pedestrian traffic along the front of these buildings would be unsafe and should not be passing along their fronts. Although they face a driveway, they are not adjacent to the street. The design directs this traffic along Hightower Rd and around the edges of the commercial building. This portion of proposed design meets and exceeds the standard in code.
- 21.09.040C.3.c. iv. Roof overhangs in commercial districts
 - *Except for arcaded or roofed walkways, no architectural elements attached to a building, such asroof overhangs ... shall project more than two feet into a setback area.*
 - DESIGN ALTERNATIVE: Per 21.09.040C.3.c.v Walkways and terraces may use up to 50 percent of the side and rear setback. The design makes use of the side setback, immediately south of building A, as a pedestrian walkway, see A0.1, accessing the south facing public seating areas. The roof overhangs above project 4 feet into the side setback to act as a roof for that walkway. This feature exceeds the district standard.
 - Commentary: the intent of restricting projections into side setbacks is to ensure fire separation distances between neighboring buildings. In this case there is no immediately adjacent neighboring building, but instead, a paved publicly accessible driveway parallels the south property line. This edge of the property is more like a south facing street front, is accessible by pedestrians, and warrants weather protection similar to other pedestrian areas adjacent to buildings.

- 21.09.060B. Tbl. 21.09-2
 - *Min. Front Setback: 16 ft, Maximum setback 20 ft. (Footnote 8: Maximum Front Setback Exception: limited portions of building frontage may, subject to admin site plan review, be allowed to step back further than the maximum allowed front setback to create small usable public spaces in the front of the building.*
 - DESIGN ALTERNATIVE: See site plan A0.1. Building A meets this standard. The residential buildings for the reasons discussed above are set on the west portion of the property.
 - Residential buildings are allowed to exceed the maximum front yard setbacks.
 - Commercial buildings are required to meet the standard.
 - Commentary: To provide quality public pedestrian spaces along a street this standard requires buildings be close to the front property line. The current commercial building and any future commercial buildings meet this standard. However, the residential buildings do not have public functions and are not accessible to the public; their distance from the street comes from a desire to maintain a public street front for the commercial uses on the site. Because of the depth of the lot, it can have commercial spaces along the rights-of-way that are separate from residential uses. These can be adjacent to the natural vegetation near California Creek and distant from the collector roads that border the site. This is a more desirable location for residents which will encourage people to live in the district. The existing standard was crafted around the much smaller lots in the gC-7 and gC-9 zoning districts and does not fully allow for taking advantage of a large site with multiple attributes. The proposed design meets the standard in code.
- Tbl. 21.09-2 Footnote (3):
 - *Gross floor area: permit/review required: Permitted (P): <2000 sf, Admin Site Plan Review (S): 2000-4000 sf, Major Site Plan Review (M): 4001-10,000, Prohibited: >10,000 sf.*
 - INFORMATION: This project includes over 8,800 sf of commercial space requiring an Administrative Site Plan Review. Because this project is

proceeding with a Development Master Plan, an Administrative Site Plan Review would be duplicative and is unnecessary.

- Commentary: A Development Master Plan is more intensive and provides greater public input than an Administrative Site Plan Review. This provides community members the chance to voice concerns or wishes about a certain development. The DMP proposes alternative standards than in code and the Planning Department reviews the development with compliance with these and those in code. Conducting an Administrative Site Plan Review would not provide additional information to the planning department and would be duplicative to a DMP. The proposed process exceeds the Administrative Site Plan Review standard in code.
- 21.09.070G.2 Exterior Site Lighting:
 - INFORMATION: The sidewalk parallel to Hightower Road will be lit for pedestrian safety, especially at the driveway crossing, but care will be taken to make sure that site lighting is at the minimum levels necessary to light the paths and parking areas. The intent is meet Girdwood land use regulations desire for dark skies, 'to minimize glare or light trespass onto adjacent properties or light pollution in the valley' the design goal is to not inconvenience the neighboring multifamily units across Hightower Road to the east with excessive light spilling off the GCC site into their living spaces.
- 21.09.080F.2.d. [Commercial, Resort and Public/Institutional Building Design Standards] Size and Scale:
 - *No wall shall be longer than 42 feet without a change or alteration in alignment of at least four feet in depth from the plane of the referenced wall. For building sides longer than 64 feet, the combined length of the segments not in plane with the primary wall plane of the building side shall equal at least one-third of the building side length. Curved walls that include a change in wall plane of at least four feet depth in 40 feet of wall length shall satisfy this requirement. Where two arms or elevations of a building are separated by a change in wall plane of six degrees or more, these shall be considered as separate building sides for the measurement purposes of this provision.*
 - DESIGN ALTERNATIVE: See Building A architectural plans Aa1.1. Commercial building walls visible from the public way are 38'-6" in the

longest plane. The west wall of Building A is angled to match the property line and is approximately 67 feet long total.

- Design alternative addressing long wall: west point of building is made completely transparent through this narrow part of the floor plan via the use of large windows on opposing walls. the solid portion of the west wall is shorter than 42 feet. A viewer can completely see through any portion of the building with a wall longer than 42 feet.
 - Commentary: Except for the angled wall, all the walls of Building A comply with the standard. This angled wall faces away from the street and therefore is not readily visible to the public. The intent of this code section is to reduce the visual mass of a building by introducing changes in wall plane. This same sense of smaller scale can come from the use of windows and storefront wall systems especially when these are used to make a portion of the building completely transparent and showcase the distillery equipment inside (see rendering on sheet Aa2.1). This design alternative accomplishes what the standard intends in a far more architecturally interesting way. The visual depth of the room and landscape beyond achieves the same effect as would modifying the wall plane. In addition to the visual effect of façade modulation is the desire for interesting building designs. Having the wall angled and meeting at a knife-edge point significantly heightens the interest of the massing. The proposed design exceeds the standard.
- 21.09.080F.3.a. [Roof Form] Pitch:
 - *Roofs visible from a public way shall be primarily pitched. The pitch for the primary roof structure shall be between 8:12 and 12:12, but may be as shallow as 5:12, as long as the eave overhangs are at least two and one half feet. Segments of the roof may be flatter or steeper slopes, however, to achieve a specific design effect. Wherever a portion of the roof has a shallower pitch than the standard, or if it is flat, provision shall be made to avoid glaciation.*
 - DESIGN ALTERNATIVE: Building A Roof forms are pitched at 2:12.
 - Roof pitches as steep as required create unsafe snow sliding conditions especially on larger buildings. The roof will be designed to hold the snow and prevent snow slides.

- Roof design and materials will not create conditions for glaciation.
 - The Roof form is designed to direct water away from all perimeter pedestrian areas unless those areas are protected by a canopy.
 - The form maximizes solar orientation into second floor building spaces.
 - All of these desirable design goals are difficult to achieve with steeper roofs without the roofs becoming much more visually imposing and out of scale with the existing structures in the New Girdwood Townsite to the south.
 - Commentary: As Building A will be a “cornerstone” building, it is appropriate that it have heightened aesthetic attributes but not an overbearing massing. There is also a desire for a wholistic design aesthetic through the site. With the residential buildings having low-slope roofs, keeping with that style in the commercial building allows the site to feel like a cohesive campus. This code section makes allowances for “flatter or steeper” roofs, and these proposed roofs will not cause glaciation. Because of the large area of the building, steeper roofs will have more surface area and pose a greater danger for snow sliding which is detrimental to the desire to have pedestrian spaces around the perimeter of the building. This improvement on safety, a roof designed for snow county, and heightened aesthetic attributes shows the proposed development meets the intent of the design guidelines. The proposed design exceeds the standard.
 - Note: the new Girdwood Clinic being constructed to the south of Building A has a very similar roof design and slope. Similar architectural language in new similarly sized buildings in the New Girdwood Townsite is desirable.
- 21.03.230B. [Vacation of Public and Private Interest in Lands] Application Submittal:
 - Applications for vacation requests shall contain the information specified in the title 21 user’s guide, and shall be submitted to the platting officer on a form provided by the department.
 - INFORMATION: This project as proposed will require the vacation of a

stream protection easement.

- Commentary: The proposed design places one of the residential buildings in a platted stream protection easement that parallels the west property boundary. There is precedent for vacating these easements as the stream protection standards in Anchorage Municipal Code are more favorable to agencies and property owners. Before receiving a Land Use Permit, the project team will submit an application to vacate the easement per the procedure outlined in 21.03.230.

C. Approval Criteria

General Design Objectives:

Townhouse-style multifamily dwelling units continue to be popular for developers and residents. The buildings can have small footprints and each unit has multiple levels and views from two directions. These units provide the amenities a resident of Girdwood needs. As they have two bedrooms, they can accommodate small families as well.

Each unit has a garage and entry on the main level. The entry has areas for storage and the garage has greater depth than would accommodate a car, leaving additional storage space or a work area. The second floor contains the living spaces for the unit, with a kitchen and open dining and living room. There is also a small half-bath. The double-frontage of the townhouse gives each unit two decks. The smaller front deck will provide transitional living area as the evening sun warms this space. The back deck faces the vegetated riparian areas along California Creek and is more private than the front deck. The top floor of the unit has two bedrooms and two baths. The master bedroom has en suite bathroom and closet. These residential units meet a demand in the current housing market.

Beside the layouts of the units, the buildings will have materials that fit within the existing built environment. The building form, materials, and colors will meet and exceed the expectations of the prescriptive requirements in the Girdwood Land Use Code. Horizontal and vertical siding will ground the buildings to Girdwood while bright trim colors and design features will add visual interest. The roof forms will be atypical butterfly roofs. These help address the challenges of living in a town with heavy snows and rains. These also break up the mass of the building by fracturing its top into chiseled crags like the mountains beyond.

The site design will provide easy connexions to adjacent areas. A pedestrian way will enter the site from Hightower Rd and extend to the dwellings. The parking area will function as a

buffer for the residential units from the street. Vegetation around these areas will give greater senses of privacy to the residents. The site will be insulated but also reaching out to the surrounding townsite.

a. Planning Objective and Design Considerations

The development master plan substantially conforms to the principles and objectives of the Girdwood Area Plan, any approved neighborhood, district, or area plans, and the general purposes of this chapter, as stated in section 21.09.010

Girdwood Area Plan

The GAP identifies four primary goals, two of which pertain to the project. The proposed project meets these:

1. Perpetuate Girdwood's small-town atmosphere:
 - "Situate multi-family housing in dispersed pockets with higher density development located within the resort areas, or next to major commercial centers." These multifamily buildings are adjacent to the New Townsite commercial area instead of near residential neighborhoods.
 - "Expand the town core as a commercial and community center." This development includes new commercial spaces that will extend the town core further along Hightower Rd.
 - "Reduce vehicular congestion through efficient land use, local circulation patterns, and adequate parking. The site layout provides an efficient vehicular circulation and the necessary off-street parking spaces. This does not burden other businesses in the area. Road access will be at only two driveways in order to reduce the traffic impacts to Hightower Rd.
2. Preserve Girdwood's Natural Environment:
 - "Avoid placement of buildings intended for human occupancy in natural hazard area". The project site is outside of avalanche hazard areas and flood hazard areas.

The GAP also includes a Land Use Plan Map. This designates the site with Commercial land use. "This classification is for areas substantially developed for commercial purposes and are expected to remain so for the duration of the Plan, and for vacant areas best suited to commercial development." However, it is immediately adjacent to an area designated with Commercial/Residential land use. "This classification is for areas substantially developed with a mix of commercial and residential uses and area

expected to remain so for the duration of the Plan, and for vacant areas best suited to a mix of commercial/residential uses. Later, the GAP clarifies that “The Plan also allows multi-family housing in the areas classified commercial/residential, commercial, [...]”. The Land Use Map treats the New Townsite as a commercial hub with mixed residential and commercial areas between it and the school. This project meets the density goals for the New Townsite and addresses contemporary demands for residential and commercial space in Girdwood.

The GAP also provides provisions for multi-family housing and its location and scale. The buildings in this project are dispersed throughout the site and do not exceed three stories. The new buildings will be no taller than nearby buildings. The economy of construction inherent (the repetition of design reduces labor costs) with townhouse-style multifamily developments will allow the units to be more affordable to full-time residents. Seasonal residents will find them attractive as well for their proximity to the New Townsite, but the distance from the resort may discourage second-home buyers who ski.

Compliance with codified development requirements will ensure the form of the project will not disrupt the visual context of the area. Landscaped areas around the building will improve the visual appearance and ecological function of the site as well. Building will be small in scale and provide commercial and residential areas. For these reasons, the project meets the goals of the GAP.

Girdwood Commercial Areas and Transportation Master Plan

The GCATMP identifies community-derived objectives for the New Townsite. Generally, the GAP notes a community goal to “make the New Townsite the true heart and center of the Girdwood community.”

- Encourage the development of a diverse mix of commercial and retail services that appeal to both locals and visitors.
 - The project proposes a café and craft distillery that will have appeals for both locals and visitors. There is not currently a business in the New Townsite for grab-and-go café foods and sandwiches. The distillery is without precedent in Girdwood and the popularity of such distilleries in other Alaskan towns implies a similar reaction locally.
- A conceptual plan for commercial infill in the New Townsite identifies the site as a potential site for lodging.
 - The earlier development proposal included a hotel for this site. However, because of changes to market demands in Girdwood, the

current proposal seems more appropriate. The plan does generally describe the area north of Town Square as an ideal location for residential units as well.

b. Street Design

The streets, roads, and other transportation elements are in conformance with applicable transportation plans.

Official Streets and Highways Plan

The OSHP identifies Hightower Rd as a Class IA industrial commercial collector with an average daily traffic load of 2,000 to 10,000. The addition of these units will not require upgrades to Hightower Rd. The OSHP does show an additional Class IA industrial commercial collector in the right-of-way along the north of Tract E3. This project does not propose its construction, but does have a planned driveway connexion when that occurs.

Girdwood Area Plan

The GAP identifies three key factors for the Girdwood “circulation system”. The first is that the system be “efficient and free of congestion”. This project does not propose a new roadway or any improvements to Hightower Rd. It will likely continue to have the same levels of congestion.

The second factor is to provide multi-modal facilities. Active transportation is important in Girdwood and throughout the year. There is a bike path along the east side of Hightower Rd that connects the New Townsite and Lower Iditarod Trail at the Girdwood School and a sidewalk along the west side of the road that starts at the Post Office. This project proposes a pedestrian connexion to a new sidewalk along the west side of the road.

Third, the GAP encourages development along roadways that “add to, rather than detract from, the small-town resort character of the valley.” This project places the commercial building close to Hightower Rd. This enhances the streetscape by continuing the sense of a commercial center along the road. The design responds to the architectural and natural context with multiple roof lines and large areas of glazing. The building materials will be appropriate as well and make the building visually interesting while matching the surrounding materiality.

The GAP proposes a new connector road in the vicinity of the project, but it does not appear that this proposed building or parking area improvements will impede this in the future. The other applicable recommendations include creation of a transit system. The Glacier Valley Transit is a free shuttle service and all routes service the New Townsite. Hightower Rd has service to the school as well. These transit stops are with walking distance of the proposed building site, although the shuttles will frequently pick up riders between stops. Another specific GAP direction is the encouragement for reduction of vehicle trips between Anchorage and Girdwood. We anticipate providing these additional housing units will help meet this goal.

Girdwood Commercial Areas and Transportation Master Plan

The GCATMP identifies transportation infrastructural improvements throughout the Girdwood Valley. In the New Townsite area some of these improvements have already occurred, like the reconstruction of Hightower Rd. The plan identifies a new roadway from Hightower to Crow Creek Rd that runs along the north side of the property.

Girdwood Valley Trails Management Plan

The GVTMP creates a framework for area trails and their development. Specifically it aims “to create a highly functional, interconnected, multi-use trail system that meetings current and future needs of the community.” While this project does not propose any new trails or improvements to existing ones, the proposed development is approximately 90 feet from Tiny Creek Trail also known as California Creek Trail. This trail is a Class 3 biking and hiking trail that parallels California Creek from the school to the New Townsite. The trail is completely within Tract E5 and does not have a dedicated easement. Connection to this trail will be possible for residents but is not planned. The plan calls for trail easements for some trails like Tiny Creek on private land. Because the existing trail is coincident with a utility easement and stream setback, a specific trail easement is not necessary.

c. Project Costs

The development has no substantial adverse fiscal impact on the municipality.

The project is entirely on private property. The only impacts anticipated for municipal systems will be improvements in the Hightower Rd right-of-way. This may include connexion to the waterline under the road. These improvements will impede traffic on the road for a short time. Other costs will include staff time.

d. Community Benefits

The development provides significant community benefits in terms of design, community facilities, open space, and other community amenities.

This development provides several amenities to the benefit of the community. Generally, the project is creating 10 residential units. We anticipate this to alleviate some of the existing demand for housing throughout Girdwood. The project also adds two unique commercial uses to the New Townsite area, a distillery and a café.

e. Adverse Impacts

The development minimizes any potential adverse impacts to surrounding residential areas to the maximum extent feasible.

Tract E3 is between a multifamily residential development and the Post Office, and between multifamily residential areas and California Creek. As development completely surrounds the lot, with the exception of California Creek, this project will bring an otherwise vacant lot into a condition more akin to its neighbors. The scale will be harmonious and does not introduce a commercial use that will adversely impact the neighborhood. Like Girdwood Brewing Company, the proposed distillery will close at 8 PM. Patron noise will therefore not extend into the night, disturbing nearby residents.

Road closures associated with construction will occur in one season. Because construction of the buildings will occur in two phases, there will be multiple seasons of construction noise. This will not extend beyond a year for either phase. The added traffic to Hightower Rd will not be significant. Because of its proximity to the New Townsite and community parking areas, visitors to the café and distillery will likely come by non-vehicular means. This will not generate high levels of traffic and parking congestion as seen at Girdwood Brewery, which is isolated from other uses. The planned intent for the New Townsite is a commercial core, and like the most successful examples it has facilities for central parking. Visitors to the area can park and walk to multiple businesses. Future development of the planned roadway to Crow Creek Rd will alleviate some vehicle traffic congestion associated with this development.

Any impacts to runoff that the proposed development creates will not affect residential areas downstream. The project will be subject to a Storm Water Pollution Prevention Plan and will implement Best Management Practices for impacts to California Creek.

f. Service Levels

Sufficient public safety, transportation, and utility facilities and services are available to serve the subject property at the proposed level of development, while maintaining sufficient levels of service to existing and anticipated development in the surrounding areas.

Tract E3 is accessible to fire apparatus and public safety. The addition of 10 residential units in this area does have the possibility to increase the number of calls and trips to the area. However, this additional density is not likely to adversely affect service to other areas of Girdwood. The units represent a significant increase to the density of neighborhood, but because the units are in a two buildings response times will remain the same as the current time. Similar outcomes will result from the addition of the commercial building to the site.

Transportation needs of residents may increase ridership of public transit shuttles and vehicle trips on Hightower Rd. However, shuttle service is frequent and rarely at capacity. Although residents may be driving along Hightower Rd, they will be using new on-site parking facilities on Tract E3.

The utilities currently serving the site are electrical power and sanitary sewer. There is a water main in Hightower Rd that will provide public water to the development. Some utility improvements may be necessary as the particular service companies require.

g. Design Standards

The design standards are equivalent to or exceed the generally applicable development standards of sections 21.09.060, .070, and .080, and result in high quality, environmentally sensitive development in keeping with the Girdwood Area Plan, the intent of this chapter, and the character of Girdwood.

See Section B for commentary. We propose several alternative standards that meet or exceed the development standards in Chapter 9 and other areas of Title 21.

APPENDIX A: PUBLIC MEETING COMMENTS:

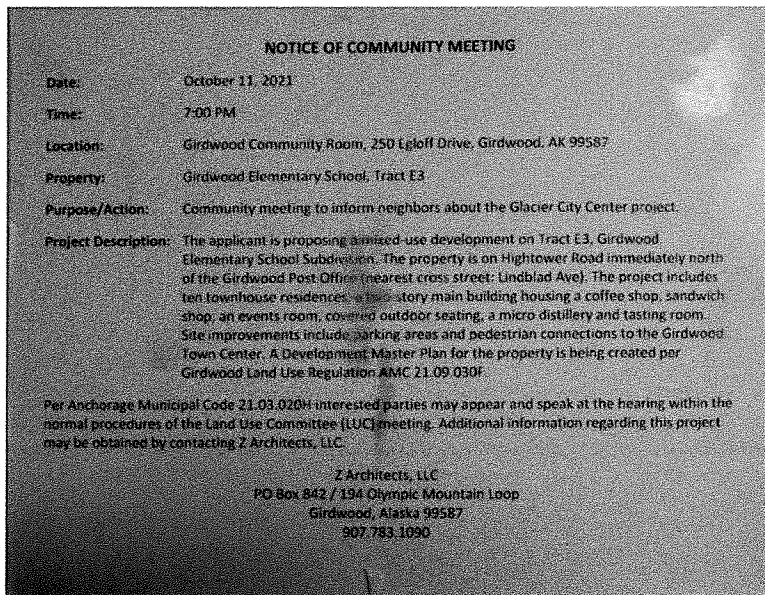
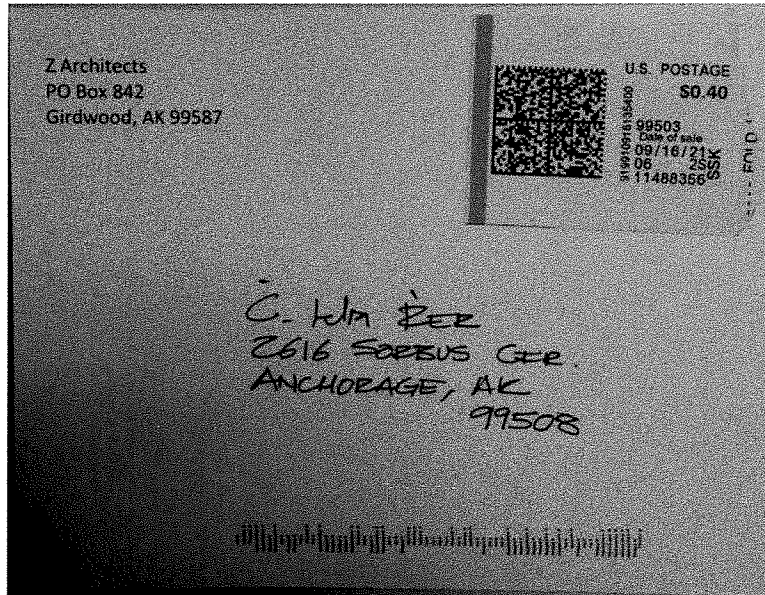
On 11 October and 18 October the project team presented to the Girdwood Board of Supervisors (GBOS) Land Use Committee and GBOS respectively. The first meeting served as the community meeting required for the DMP. Mailers went to addresses provided by the MOA Planning Department (see below). The bullets below provide a list of comments and questions from members of the public and Board of Supervisors for both meetings.

- 11 October 2021: Girdwood Board of Supervisors (GBOS) Land Use Committee – Served as Community Meeting
 - Shannon O’Brien: Are we asking for any variances?
 - Mike Edgington: Are we expecting all ten to be second homes or will there be provisions for locals to live in them?
 - Kalie Harrison: What is the price point on the townhouses?
 - Jerry Fox: Concerned about short term rentals. Do you have a schedule?
 - Brian Burnett: Is the tiny creek trail corridor drawn into the plans? Will the trail be respected?
 - Kyle: Request that no bump-outs are installed and something about drainage
- 18 October 2021: GBOS
 - Mike Edgington: What happens next?
 - Jen Wingard: Is the overflow parking for the townhomes the same as the commercial parking?
 - Christina Hendrickson: Need adequate parking so we don’t impact the post office parking which is already being used by other businesses. Need direct crosswalk to the bike path. With these new townhouses and Holton Hills, does the post office have enough PO boxes?

The project team responded to the questions during the meeting and the owner was present at the meetings. There were few design changes that came from the comments. Because of the

DMP, the project team does not anticipate applying for any variances. The owner said that they were not planning on creating covenants prohibiting short-term rentals. Because the trail is not on the project parcel this development does not create an easement. The project team presented the question about the curb and street edge design to the pre-application conference. The agencies determined an acceptable alternative would be a rolled curb. All the parking for the residential structures will be on-site and there is adequate parking when including district parking and land banked parking in the undeveloped ROW. Unless required by a reviewing agency, the project does not propose a crosswalk. The project team believes there is adequate parking

Public Notice:



APPENDIX B: PREAPPLICATION CONFERENCE NOTES:

Attached in following pages.

December 21st 2021

Subject: Letter of Authorization for Permitting

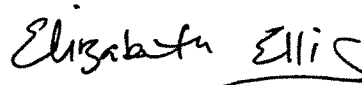
This letter serves as authorization for Z architects, LLC and staff members thereof, to act on the behalf of Glacier City Development LLC owned by Nathen and Elizabeth Ellis, for permitting applications and procedures through the Municipality of Anchorage for our property at Tract E3 of the Girdwood Elementary School Subdivision in Girdwood Alaska.

Sincerely,

Glacier City Development LLC owners

A handwritten signature in black ink, appearing to read "Nathen Ellis". The signature is fluid and cursive, with the first name "Nathen" being more prominent than the last name "Ellis".

Nathen Ellis

A handwritten signature in black ink, appearing to read "Elizabeth Ellis". The signature is fluid and cursive, with the first name "Elizabeth" being more prominent than the last name "Ellis".

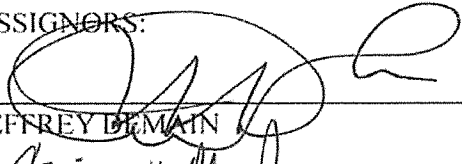
Elizabeth Ellis

**ASSIGNMENT OF
GLACIER CITY DEVELOPMENT, LLC
MEMBERSHIP INTEREST**

JEFFREY DEMAIN and EILEEN DEMAIN whose address is 625 Highlander Circle, Anchorage, Alaska 99518 (the "Assignors"), hereby grants, transfers and assigns to NATHEN ELLIS and ELIZABETH ELLIS whose address is _____ (the "Assignees") Assignors' entire one hundred percent (100.00%) ownership interest in GLACIER CITY DEVELOPMENT, LLC, an Alaska limited liability company (the "Company").

Dated this 30 day of ~~June~~, 2021.

August
ASSIGNORS:


JEFFREY DEMAIN


EILEEN DEMAIN

**ACKNOWLEDGEMENT AND ACCEPTANCE
OF ASSIGNMENT**

Assignees hereby acknowledge and accept the foregoing Assignment and agree to become members in the Company and to be bound by the terms and conditions of the Company's articles of organization and operating agreement.

Assignees agree to indemnify and hold harmless the Assignors from and after the date of this Assignment from and against any and all claims asserted against Assignors as a member in the Company.

Aug. *EE*
Dated this 19 day of ~~June~~, 2021.

ASSIGNEES:


NATHEN ELLIS


ELIZABETH ELLIS

December 4, 2021

RE: **Glacier City Center** **PARKING ANALYSIS** - Rev.4

Building Code:

Anchorage Municipal Code (AMC) Title 21

21.07.080 E.2c Parking Lot Interior Landscaping

See landscape plan.

Table 21.07-4 Off Street Parking

Commercial and Manufacturing Uses.

Building A - Parking Required:

See building plans for use areas and associated parking counts.

Second Floor:

- | | |
|--|--------------|
| • Tasting Room: 1,731 s.f. Parking 1:100 GFA (Bar)= | 17.31 Spaces |
| • Event Space: 2185 s.f. Parking 1:100 GFA (Restaurant)= | 21.85 Spaces |
| • Outdoor seating: 298 s.f. Parking 1:100 GFA (Bar)= | 2.98 Spaces |

First Floor:

- | | |
|--|--------------------|
| • Distillery: 1,012 s.f. Parking 1:750 GFA (Manufacturing) = | 1.35 Spaces |
| • Coffee and Sandwich shop: 3,273 s.f. Parking 1:100 GFA (Restaurant)= | 32.73 Spaces |
| • <u>Outdoor seating: 298 s.f. Parking 1:100 GFA (Restaurant)=</u> | <u>2.98 Spaces</u> |

Building A Parking Spaces Required	79 Spaces
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Residential Uses.

Townhouse- Parking Required:

All units are two-bedroom.

1 per one bedroom Unit	1.0
Add 0.5 for each additional bedroom	.5
Add .15 guest parking for each townhouse	.15
<u>Base onsite parking required per two-bedroom residence</u>	<u>1.65</u>
<u>Townhouse Parking Spaces Required</u> 10 units x 1.65 =	17 Spaces

Total base parking required on site: 79 (building A) + 17 (Residential) =	96 Spaces
--	------------------

21.09.070L.2 Girdwood Specific Parking Regulations

gC-5, gC-8, and gC-9 districts. Required parking for any non-residential use in the gC-5, gC-8 or gC-9 districts may be located off-site within the adjoining right-of-way or within 600 feet of the site in community parking facilities under a municipal parking agreement. Properties may also use the reduced parking requirements and parking reductions provided in Section 21.07.090. (RESERVED: Tailored parking requirements and reductions for the new townsite area will be further developed in coordination with the update to the Girdwood Area Plan and adopted as an amendment to this section.)

Community Parking within New Girdwood Townsite:

70 community parking spaces are within 600' walking distance of Glacier City Center. See drawing A0.5 Community Parking Access Plan.

Parking Provided:**On Site:**

Townhouse garage and driveways:	20 spaces
Townhouse guest parking:	2 spaces
Glacier City Center Parking Lots:	40 spaces

Off Site:

Community Parking facilities:	34 spaces
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Total Provided:	96 spaces
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Table 21.07-6 Off Street Loading Berths for building A

Not required for less than 10,000 s.f.

Table 21.07-8 Accessible Parking Spaces for building A

Total Parking Spaces Provided	Accessible	Number that shall be Van-Accessible
76 to 100	4	1

21.07.090E.5. Parking location

Except as provided in subsection 21.07.090 F., all required parking shall be on the same lot as the use served. However, required parking may be on an abutting or adjacent lot provided the zoning district in which the lot is located allows for off-street parking as a permitted principal use, site plan review use, or conditional use; in which case there shall be a parking agreement which meets the requirements of subsection F.1. below.

The Following allowed reductions have not been used in the above parking calculations but are noted here for reference. All are based on total base parking requirement (96 spaces).

21.07.090F.8 Uses adjacent to transit service

Uses adjacent to transit service. A use is eligible for a reduction of up to five percent of the minimum number of required parking spaces if it is located within 800 feet of the street right-of-way centerline of any municipal public transit route, subject to approval by the traffic engineer and the director. The public transportation department may require a public use easement or transit stop and/or transit shelter improvements if the subject property abuts an existing or planned transit stop. If the public transportation department requires such an easement or improvements, then the use is eligible for an additional reduction of two percent or one more parking space, whichever is greater.

Nearest public bus stop is at the corner of Hightower Road and Linblad Ave. 300 feet walking distance from Glacier City Center.

Reduces parking count by 4.8 ($96 \times .05 = 4.8$) to 91.2

21.07.090F.10 Transit pass benefits.

A use in which the owner or employer offers transit passes cost-free to all employees or residents is eligible for a parking reduction of up to ten percent of the minimum number of required parking spaces. The use shall be located within 800 feet of the street right-of-way centerline of any municipal transit route. The public transportation department may require a public use easement or transit stop and/or transit shelter improvements if the subject property abuts an existing or planned transit stop. If the public transportation department requires such an easement or improvements, then the use is eligible for an additional reduction of two percent or one more parking space, whichever is greater.

Reduces parking count by 9.12 ($91.2 \times .1 = 9.12$) to 82

21.07.090F.21 Smaller parking spaces for parking structures and low-turnover uses.

If approved by the traffic engineer, up to 20 percent of the total number of required parking spaces located in a parking structure and/or designated for employee or resident parking only may be eight feet six inches wide, subject to the requirements of Table 21.07-7, Parking Space and Aisle Dimensions. Such spaces shall be signed for employee or resident parking only.

21.07.090F.22 Bicycle parking. Bicycle parking. A use is eligible to permanently or seasonally substitute bicycle parking spaces for required automobile parking spaces. Each automobile parking space shall be replaced by a minimum of six bicycle parking spaces not required by this title. Bicycle parking spaces shall comply with the standards of subsection 21.07.060 F.15. and be separated from motor vehicle areas by bollards or other physical buffer approved by the traffic engineer.

12 bicycle parking spaces are shown on drawing A0.1 Site Plan.

Reduces parking count to 80

Girdwood Land Use Committee

Notice of Meeting on October 11, 2021

7PM

Agenda Final

This meeting will be held via Teams teleconference.

The following items have been submitted as agenda items for consideration at the meeting of the Girdwood Land Use Committee. A voting member is any Girdwood Valley resident eighteen (18) years of age or older. Non-resident property owners, businesses, and non-profit associations are allowed one (1) vote per entity. Detailed voting qualifications can be found in the rules outlined in the Girdwood Land Use Committee Operating Principles.

The Girdwood Board of Supervisors, its committees, and subcommittees are subject to the Alaska Open Meetings Act as found in Alaska Statute 44.62.310 and Anchorage Municipal Code 1.25 - Public Meetings.

The Girdwood Land Use Committee operates under the Girdwood Public Meetings Standards of Conduct.

Agenda Item LUC 2110-01:

Call to order

Agenda Approval for October 11 2021 meeting

Minutes Approval for the September 13 2021 meeting

LUC Officer reports

Announcements & Presentations:

LUC is seeking a volunteer to attend Federation of Community Councils meetings, which are held the 3rd MON each month 6PM-8PM.

Agenda Item LUC 2110-02: Public Comment (3 minutes each) Persons offering public comment must state their full name and address. Public Comment must be on subjects not listed on the agenda.

Agenda Item LUC 2110-03: Committee written reports are included in the meeting packet monthly.

Any items that require additional discussion should be brought up at this time.

Old Business:

Agenda Item LUC 2008-04: Update on draft land acknowledgment statement (Diana Stram).

Agenda Item LUC 2109-04: Review LUC Operating Procedures re ability to hold hybrid and/or virtual meetings.

Agenda Item LUC 2109-05: Notice of Liquor license renewals in 2022.

Request for LUC recommendation for GBOS Letter of Non-Objection Re: 2022 Annual Liquor License Renewals 2022

5466	Bore Tide Deli	1632	Jack Sprat	3446	Seven Glaciers Lounge
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287	Crow Creek Mercantile	3263	Chair 5 Restaurant	3447	Sitzmark Bar & Grill
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996	La Bodega	3445	Hotel Alyeska	5498	Girdwood Picnic Club
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Agenda Item LUC 2110-07: Review MOA Omnibus Ordinance (Case #202-0126) scheduled for planning and zoning commission hearing on Nov 8, 2021. (Mike Edgington)

Agenda Item LUC 2109-06: Bikewood (formerly Girdwood Mountain Bike Alliance) request for LUC recommendation of GBOS Resolution of Support for proposed Mountain Bike Master Plan to be submitted with construction grant applications.

Agenda Item LUC 2110-06: Girdwood Community Land Trust request for LUC recommendation for a GBOS Resolution of support for grant applications for the Girdwood Community Land Trust. (Item moved to Old Business due to time sensitive RURAL CAP grant application cycle). (GCLT, Krystal Hoke)

New Business:

Agenda Item LUC 2110-04: Initial presentation of Glacier City Center, proposed mixed use development located off Hightower Road immediately north of the Post Office. Group will be seeking LUC recommendation for GBOS Resolution of Support for Development Master Plan. this meeting will also serve as the community meeting as required by AMC 21.03.020C. (Marco Zaccaro, presenting).

Agenda Item LUC 2110-05: Initial Presentation of Alyeska Employee Housing II, proposed for Tract B, Alyeska Prince Addition on Northface Road immediately west of Hotel Alyeska, east of existing Employee Housing. Group will be seeking LUC recommendation for GBOS Resolution of Support for Development Master Plan. This meeting will also serve as the community meeting as required by AMC 21.03.020C. (Connor Scher, presenting)

Agenda Item LUC 2110-08: HLB request for LUC Recommendation to GBOS Resolution of Support for Rezone of approximately 6.35 acres from GIP (Institutions & Parks) to GR3 (Residential), located within Tract B, Girdwood Elementary School Subdivision (Plat 85-38). (Shelley Rowton, HLB)

Request for GBOS/LUC Joint Meeting (1 of 2 completed in 2021): Held April 29, 2021
Adjourn

Girdwood Land Use Committee Meeting Agendas and minutes are available on line: <http://www.muni.org/gbos>

Girdwood Land Use Committee
Notice of Meeting on October 11, 2021
7PM
Minutes Final

This meeting will be held via Teams teleconference.

The following items have been submitted as agenda items for consideration at the meeting of the Girdwood Land Use Committee. A voting member is any Girdwood Valley resident eighteen (18) years of age or older. Non-resident property owners, businesses, and non-profit associations are allowed one (1) vote per entity. Detailed voting qualifications can be found in the rules outlined in the Girdwood Land Use Committee Operating Principles.

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Agenda Item LUC 2110-01:

Call to order 7PM, Kevin McDermott, Chair

Agenda Approval for October 11 2021 meeting with revisions

Mike Edgington/Brian Burnett

Item 2110-07 moved to Old Business as it will be addressed by Planning before the next LUC meeting

Item 2001-06 moved to Old business during the meeting as RurAL CAP grant cycle has just been announced.

Application is due prior to next LUC meeting.

Minutes Approval for the September 13 2021 meeting

Kalie Harrison/Shannon O'Brien

LUC Officer reports: None

Announcements & Presentations:

LUC is seeking a volunteer to attend Federation of Community Councils meetings, which are held the 3rd MON each month 6PM-8PM. Any member of LUC is invited to volunteer for this assignment. Individual would attend FCC meetings and report back to LUC. Community councils across Anchorage share information and resources through the FCC meetings. It's a good opportunity for context and perspective of items impacting other neighborhoods in the MOA.

Agenda Item LUC 2110-02: Public Comment - None

Agenda Item LUC 2110-03: Committee written reports are included in the meeting packet monthly.

HLBAC is discussing purchase of USFS land that they occupy. This would allow USFS to build a dormitory.

Existing developed 5 lots at Industrial Park: leaseholders have right of first refusal for purchase their lots.

Old Business:

Agenda Item LUC 2008-04: Update on draft land acknowledgment statement (Diana Stram). No report, no public input received.

Agenda Item LUC 2109-04: Review LUC Operating Procedures re ability to hold hybrid and/or virtual meetings.

GBOS made change to their Rules and Procedures to allow for virtual meetings during federal health emergency. LUC made their determination to hold this meeting virtually due to COVID case counts. Hybrid meetings remain difficult due to technology issues. LUC could consider adding option of hybrid meetings if community invests in a system that makes access for all attending equal.

Agenda Item LUC 2109-05: Notice of Liquor license renewals in 2022.

Request for LUC recommendation for GBOS Letter of Non-Objection Re: 2022 Annual Liquor License Renewals 2022

5466	Bore Tide Deli	1632	Jack Sprat	3446	Seven Glaciers Lounge
287	Crow Creek Mercantile	3263	Chair 5 Restaurant	3447	Sitzmark Bar & Grill
996	La Bodega	3445	Hotel Alyeska	5498	Girdwood Picnic Club

No negative comments have been brought forward regarding any of the upcoming liquor licenses.

Motion:

The Girdwood Land Use Committee moves to recommend a GBOS Letter of Non-Objection to the 2022 liquor license renewals.

Motion by Ron Tenny, 2nd by Kalie Harrison

22 in favor, 0 opposed, 3 abstaining

Motion carries

Agenda Item LUC 2110-07: Review MOA Omnibus Ordinance (Case #202-0126) scheduled for planning and zoning commission hearing on Nov 8, 2021. (Mike Edgington). Public can view this draft ordinance on line at this location:

<https://www.muni.org/CityViewPortal/Planning/GetFile/99244>

Girdwood Land Use Committee Meeting Agendas and minutes are available on line: <http://www.muni.org/gbos>

One element specifically is related to Girdwood T21C9, a minor text correction related to private vs public parking.

Other elements are Areawide, but there are several points that will relate to Girdwood, if approved:

- Land Use Permit currently requires that construction begin within a year and then can be delayed for 12 months. Proposed change would allow for construction to be delayed by 2 years with extension application, still requires start of project within a year.
- Paving currently required would not be required if project is connecting 2 gravel roads or small parking area. Girdwood already has proposal in commercial parking amendment that is similar to this.
- Parking calculations at restaurants would calculate parking needs differently so that SF of storage, for example, is not calculated at the same rate as calculation for restaurant dining area.
- Section 15 regarding subdivision of property currently capped at 8 lots to remove that restriction.

Group discusses whether to recommend to GBOS a resolution of support or other feedback. Group ultimately decides that no formal action is needed. As no action is recommended, item will be removed from GBOS agenda.

Agenda Item LUC 2109-06: Bikewood (formerly Girdwood Mountain Bike Alliance) request for LUC recommendation of GBOS Resolution of Support for proposed Mountain Bike Master Plan to be submitted with construction grant applications.

Dan Starr with Bikewood presents that this request is based on need for Bikewood to have such a recommendation in order to apply for grant funding opportunities that will come up this fall for summer 2022 construction. Adoption of Trails Master Plan is farther off, although local process has begun. Bikewood received support from GTC at their September meeting. This proposal was made in 2019 but was dropped while the community pursued work on the Trails Master Plan instead. At that time LUC made the recommendation to GBOS to write a Resolution of Support. Content of proposal has not changed, although Girdwood Mountain Bike Alliance has rebranded to Bikewood. If changes occur to the Trails Master Plan, they will be reflected in Bikewood plans.

Motion:

Land Use Committee moves to recommend a GBOS Resolution of Support for the proposed Mountain Bike Master Plan for Bikewood to use in pursuing grant funds for trail construction in the Mountain Bike Park, as presented.

Motion by Brian Burnett, 2nd by Lisa Miles

24 in favor, 0 opposed, 4 abstaining

Motion carries

Agenda Item LUC 2110-06: Girdwood Community Land Trust request for LUC recommendation for a GBOS Resolution of support for grant applications for the Girdwood Community Land Trust. (Item moved to Old Business due to time sensitive RurAL CAP grant application cycle).(GCLT, Krystal Hoke)

Item is moved to Old Business during the meeting as a grant opportunity with RurAL CAP has just been announced and the application deadline is prior to the next LUC meeting. (Motion by Krystal Hoke, 2nd by Jerry Fox, group assent, no formal vote).

RurAL CAP grant offers funding for community gardens and other sustainability projects. The current grant is focused on planting fruit trees in rural areas. Group discusses location for this, process is in GCLT hands to pursue change of code to allow community gardening on public land. Girdwood Community Land Trust has identified private land that would allow for such use.

Motion:

Land Use Committee moves to recommend a GBOS Resolution of Support for RurAL Cap grant application by the Girdwood Community Land Trust.

Motion by Krystal Hoke, 2nd by Ron Tenny

26 in favor, 0 opposed, 3 abstaining

Motion carries

New Business:

Agenda Item LUC 2110-04: Initial presentation of Glacier City Center, proposed mixed use development located off Hightower Road immediately north of the Post Office. Group will be seeking LUC recommendation for GBOS Resolution of Support for Development Master Plan. this meeting will also serve as the community meeting as required by AMC 21.03.020C. (Marco Zaccaro, presenting). Location is between the Post Office and Creekside apartments. Nate and Liz Ellis are working with the Demains on this project, which is mixed use multi-family. Original project on this location was proposed in 2006 however was dropped prior to ground breaking.

Residential part of project is 10 townhouses in 2 buildings at the west side of the lot, backing up on California Creek setback, 100' from the creek. Set up is bottom floor garage, 2nd floor living area, 3rd floor bedrooms. Possible that there may be covenants regarding nightly rental, to encourage longer minimum stay.

Commercial part of the project is a single separate building envisioned to hold a coffee shop, distillery, sandwich shop. These will have south facing windows and outdoor space as possible. Development of this is on the Southeast corner of the lot, facing south toward the

back of the Post Office. This provides separation of the housing and commercial elements, and also keeps those separate from other residential development in the area.

Concepts for both residential and commercial buildings includes solar panels.

Project requires a Development Master Plan, which means that as long as the project meets or exceeds the underlying design standards, the architect can create their own standards. This was the case with the Nordic Spa as well. Areas that they will pursue that is different than existing code are:

- Mixed use code specifies lower level to be commercial use and upper levels residential in the same building. Separating these uses seems beneficial both to the commercial entities and to the residential owners.
- Increase roof overhang from 2' to 4' to allow more overhead coverage for pedestrians using the sidewalk
- Less incline of roof pitch to eliminate unintended roof avalanches. The pitch of this roof is similar to that of the new clinic design and currently under construction nearby.

Public discussion:

Developers hope to sell condos to local families but there are no planned restrictions.

Price point is currently not known.

Impact to Tiny Creek/California Creek – staying out of the setback, no impact.

Goal is to break ground summer 2022.

Request not to use bump-outs on road. These are difficult for plowing and melt/drainage. Request tie into existing drain system around Town Square.

Concept will be presented at GBOS next week as well, then project packet will be submitted to Planning, it is expected to return to local public process for review and vote in January 2022.

Agenda Item LUC 2110-05: Initial Presentation of Alyeska Employee Housing II, proposed for Tract B, Alyeska Prince Addition on Northface Road immediately west of Hotel Alyeska, east of existing Employee Housing. Group will be seeking LUC recommendation for GBOS Resolution of Support for Development Master Plan. This meeting will also serve as the community meeting as required by AMC 21.03.020C. (Connor Scher, presenting)

This project was part of the Nordic Spa concept as there will be need for housing of new employees. Adjacent to the current employee housing, this project will push back existing vegetative buffer into the area used for storage currently. Footprint of this building is similar to that of the existing employee housing. It is 3 floors tall and will be apartment style, not dormitory style. Goal for this housing is mid-management, long term housing, possibly families. Total of 58 units in 1 building, compared to 24 units in 3 buildings. No additional parking will be needed as there is a surplus of parking currently as established in parking study performed for the Nordic Spa, Arlberg Rd was developed for this amount of traffic, no road upgrades are needed. Project will be able to tie into existing utility infrastructure, no additional lines are required.

No negative impacts to the MOA are anticipated. Positive impacts are additional housing, ability to retain long term employees to join the community. Transportation options with intermodal options (tie in to trails for non-motorized access for recreation and other uses). Possible that a play area may be included, this depends on study of expected user group.

This project also requires Development Master Plan. Anticipate Planning and Zoning review in February 2022.

Public discussion:

58 units compared to 24 is denser than existing employee housing.

Encourage resort to consider green options to offset construction impact.

Agenda Item LUC 2110-08: HLB request for LUC Recommendation to GBOS Resolution of Support for Rezone of approximately 6.6 acres from GIP (Institutions & Parks) to GR3 (Residential), located within Tract B, Girdwood Elementary School Subdivision (Plat 85-38).(Shelley Rowton, HLB)

Rezone of GIP to GR# to allow for attainable housing development in these two areas. One is within the Holtan Hills, accessed from the existing water road, other is parcel accessed off Crow Creek Road. Buildable acreage is about 1.1 acre in Holtan Hills and 2.25 acres off Crow Creek Rd.

This project will be presented at GBOS this week and 2nd presentation next month at LUC and GBOS.

Public Discussion:

Add Athabaskan Trail and Iditarod Trail to mapping.

Concern of proximity to school and concern for possible expansion of school if population increases. – School district owns other parcels to the south and east of existing building.

Has school district been approached about this development? This is not a requirement of rezone.

Possible to replace land that is rezoned from GIP to other location? This is not a requirement of rezone.

Request for GBOS/LUC Joint Meeting (1 of 2 completed in 2021): Held April 29, 2021

Adjourn 9:37PM

Municipality of Anchorage



P.O. Box 390
Girdwood, Alaska 99587
<http://www.muni.org/gbos>

David Bronson, Mayor

GIRDWOOD VALLEY SERVICE AREA BOARD OF SUPERVISORS

*Mike Edgington & Briana Sullivan, Co-Chairs
Jennifer Wingard, Amanda Sassi, Guy Wade*

October 18, 2021 GBOS Regular Meeting Minutes Final 7:00 p.m. via Microsoft Teams

The Girdwood Board of Supervisors, its committees, and subcommittees are subject to the Alaska Open Meetings Act as found in Alaska Statute 44.62.310 and Anchorage Municipal Code 1.25 - Public Meetings.

The Girdwood Board of Supervisors operates under the Girdwood Public Meetings Standards of Conduct.

Call to Order 7:00 p.m. Briana Sullivan, Co-Chair

Roll Call: Attending are Briana Sullivan (BS), Mike Edgington (ME), Jennifer Wingard (JW), Amanda Sassi (AS)
Guy Wade is excused

Disclosures:

Briana Sullivan is supporter of Bikewood (#6) No substantial conflict determined

Mike Edgington is also a supporter of Bikewood (#6) and occasionally works for Z Architects (#10, #11). No substantial conflict determined.

Amanda Sassi is former volunteer with Girdwood Fire Dept (#5). No substantial conflict.

Prior to discussion of liquor licenses, Amanda also discloses that she works for Alyeska Resort. Group votes (3-0) on nature of this conflict finding it to be substantial enough to require her abstention from the discussion and vote on license renewals.

Agenda Revisions and Approval

October 18 2021 meeting agenda approved

ME/JW 4-0

Amended topic # 12 to make this topic generally supporting community garden grants

ME/JW 4-0

September 20 2021 GBOS Regular meeting minutes approved

ME/JW 4-0

Introductions, Presentations and Reports:

1. Legislative Report

Sen. Roger Holland – No report

Rep. James Kaufman, staff Matthew Harvey provides update. Working with Legislative Legal Dept to understand the Economic Development Tax exemptions mismatch of state law and municipal code that Mike Edgington has brought forward. IT would be good to get this to the legislature in January to address. Currently in Special Session #4, with the following items to be addressed: supplemental permanent fund dividend; revision of PFD formula consideration; appropriation limits; new revenue sources.

Rep Kaufman website: [Representative James Kaufman - House District 28 - Alaska \(akrepkaufman.com\)](http://RepresentativeJamesKaufman-HouseDistrict28-Alaska.akleg.gov)

To contact Rep Kaufman and his staff: Representative.James.Kaufman@akleg.gov

Assembly members J. Weddleton, S. LaFrance: John Weddleton reports that Assembly is working on 2022 budget currently, encourages public to review and provide input. Townhall on WED 6PM at the Wilda Marston Theater regarding homelessness. Mask Emergency Ordinance passed by Assembly after Mayor vetoed it originally. This expires after 60 days. Original Mask Ordinance is still on the table. There has been a lot of testimony on this in person and via email/comment form to the assembly.

GBOS Meeting Agendas and minutes are available on line: <http://www.muni.org/gbos>

Motion:

GBOS moves to encumber \$60,088 from the GVSA Roads Capital account to complete engineering, design, permitting and construction support by HDR of fish passages to be rebuilt at Alyeska Creek on Mt. Hood Dr. and Davos Rd and at Moose Creek at Lake Tahoe Rd.

Motion by Mike Edgington, 2nd by Jennifer Wingard

4 in favor, 0 opposed in roll call vote

Motion carries

NEW BUSINESS:

10. **Agenda Item LUC 2110-04:** Initial presentation of Glacier City Center, proposed mixed use development located off Hightower Road immediately north of the Post Office. Group will be seeking GBOS Resolution of Support for Development Master Plan. (Marco Zaccaro, presenting).

Updated meeting packet has majority of the presentation. Plan is for 10 condo units in 2 buildings along the California Creek side of the lot. Corner near the post office will have commercial building that holds distillery, coffee shop and sandwich shop, with upstairs space for events. Ellis Family has acquired the property from the Demains and plan to build a mixed use project that will benefit the community through additional housing, meeting and social space.

Development Master Plan will deviate from existing code requirements as long as the project meets or exceeds the existing standards/code. 3 areas that will differ from code in the GC8 zoning area are: horizontal rather than vertical mixed use; deeper overhangs of roof, which will protect sidewalks/pedestrians; shallower roof pitch, which is also safer for pedestrians than steep pitch.

This item will be sent to the MOA for review, and will return to LUC and GBOS once that review is complete and prior to Planning and Zoning hearing. Group plans to begin construction in summer, 2022.

Group discusses parking. It is noted that parking at Post Office is now more full because of clinic construction. Staff are likely to park offsite of clinic after construction is complete as well. No direct crosswalk is currently in place from this lot to the bike path. Bike path is across the street, access currently is either at the Post Office or Creekside. Question if Post Office can accommodate need for more PO Boxes related to various proposed housing developments.

11. **Agenda Item LUC 2110-05:** Initial Presentation of Alyeska Employee Housing II, proposed for Tract B, Alyeska Prince Addition on Northface Road immediately west of Hotel Alyeska, east of existing Employee Housing. Group will be seeking GBOS Resolution of Support for Development Master Plan. (Connor Scher, presenting)
- Updated meeting packet has most of the presentation. Plan is for 56 units on 3 floor development, mostly 1 bedroom and studio apartment style living in similar footprint as existing Alyeska Employee housing, which is more dormitory style. Surplus of parking already, no need to provide more. Square footage of secure storage areas exceeds amount needed, although configuration is different that what is outlined in code.
- This project also is working through Development Master Plan. Areas that differ from existing code include appearance, need for kids play space (as demographic is not likely to have kids), and walkways to access existing paths, onsite storage configuration. Project will return to LUC and GBOS for action once MOA review is complete and prior to Planning and Zoning hearing. Group plans to begin construction in summer 2022.

Motion:

GBOS moves to extend meeting to 10:30PM.

Motion by Mike Edgington, 2nd by Jennifer Wingard

4-0 motion carries

12. **Agenda Item LUC 2110-06:** Girdwood Community Land Trust request for GBOS Resolution of Support for Grant Applications related to community gardens by Girdwood Community Land Trust (GLCT, Krystal Hoke)
- GCLT originally was working on RurAL CAP grant with imminent deadline, however that grant is not available for Girdwood projects. Now seeking a more general Resolution of Support to include with grant applications related to community gardens.
13. **Agenda Item LUC 2110-08:** HLB request for GBOS Resolution of Support for Rezone of approximately 6.6 acres from GIP (Institutions & Parks) to GR3 (Residential), located within Tract B, Girdwood Elementary School Subdivision (Plat 85-38). James Winegarner, Acting MOA Real Estate/HLB Director, presenting.
- Rezone request is for the acreage to be removed from Girdwood Institutions and Parks (GIP) and rezoned as GR3, which allows for single family homes. Plan is for this area to provide for Girdwood residential housing. School enrollment is at 76% of capacity, ASD has no plans to expand Girdwood School in the foreseeable future, or to add a high school within Girdwood.

Concerns expressed are:

Proximity of the proposed development to the Athabaskan Trail and Iditarod Trail. The Athabaskan Trail is within school property. Although not removed by the development, the Athabaskan trail could be impacted by nearby development. A buffer is requested to maintain the character of the trail.



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PARCEL: 075-031-41-000 01/01 Commercial Vacant Land 12/03/21

GLACIER CITY DEVELOPMENT LLC

GIRDWOOD ELEMENTARY SCHOOL

TR E3

625 Highlander Cir

Anchorage AK 99518 0000 [Site](#)

Lot Size:	65,512	---Date Changed---	---Deed Changed---	GRW: PIWt
Zone :	GC8	Owner : 07/15/21	Stateid: 2021 0031029	
Tax Dist:	004	Address: 07/15/21	Date : 06/02/21	
Grid :	SE4715	Hra # :	Plat : 950035	
GRW: PIWC			REF #: 06/09/95 075-031-31-000	

ASSESSMENT HISTORY

	---Land--	--Building-	---Total---	
Appraised Val 2019:	424,100	0	424,100	
Appraised Val 2020:	432,900	0	432,900	--Exemption--
Appraised Val 2021:	441,700	0	441,700	-----Type-----
Exempt Value 2021:	0	0	0	
State Exempt 2021:			0	
Resid Exempt 2021:			0	
Taxable Value 2021:			441,700	

Liv Units: 000 Common Area: Leasehold: Insp Dt: 08/19 Land Only

[Feedback E-mail: wwfipa@muni.org](mailto:wwfipa@muni.org)

Notes from Development Master Plan Pre-Application Conference
Girdwood Mixed Use Development
November 3, 2021

Review from the Concept Meeting

- The microdistillery would be allowed by-right without a conditional use permit. If the square footage of the microdistillery is over 2,000 square feet, additional site plan review would be required (see discussion under Current Planning).
- Discussed the setback from California Creek during the concept meeting; since then, the ordinary high-water mark had been surveyed.
- The proposed design of the townhouses is a residential garage on the first floor with residential living space above. Title 21 requires second-story residences to be above a commercial ground floor use within the gC-8 District. An approved development master plan calling out the proposed design as varying from Title 21 would allow for the residential garages to be on the first floor with living space above.
- Marco (Z Architects) added some thoughts on the proposed design: Wanted the residences on the west side and the commercial space on the south side. The proposed site plan does not impede views from residential buildings on neighboring properties. The commercial building abides with Girdwood commercial guidelines and parking is centralized. The parking strategy uses a two-way driveway with a hammerhead turnaround for fire safety and takes advantage of several parking reductions.
- Nathan (petitioner) added that the project has a huge amount of public support. The project will improve both the need for housing and need for commercial gathering space in Girdwood.
- Municipal staff asked for clarification on proposed Building B that is also marked as a snow storage location. The petitioner clarified that there are not definitive plans for Building B yet and its construction would likely be 4 years out. It would likely have a commercial use. The area of proposed Building B is needed for snow storage, but may not be in the future if they are able to reduce required parking spaces. Another option allowed in Girdwood could be truck snow off the site. Municipal staff recommended removing the markings for proposed Building B since it is not part of the current proposed site layout; having it there may lead to confusion since you would need to show enough parking for Building B.

Current Planning

- Current Planning asked about the size of the microdistillery. If the microdistillery is under 2,000 square feet it is permitted by-right. An administrative site plan review is required for a microdistillery between 2,000 to 4,000 square feet. If an administrative site plan review is required, this could possibly be incorporated into the development master plan—talk to Planning staff if this will end up being a requirement for a distillery of over 2,000 square feet in size. Contact Francis with additional questions on the Alcohol SLUP.
- Alcohol SLUP separation distances: okay for distance to Girdwood Elementary parcel because there is no separation distance requirement for a microdistillery classified as a bar.

- Current Planning asked about the event space use. The petitioner replied that it would be an extension of the bar/brewpub. Current Planning agreed the intend use fell under the bar/brewpub use and did not meet the definition of a club/lodge/meeting hall or a restaurant.
- Dave commented on the three parking stalls nearest the residential area. It looks like you may not have turning maneuver for that without the fire lane being built. Coordinate with Traffic on this. Traffic added that 24-feet would be adequate and to include dimensions on the site plan as it does look tight. It looks like those 3 space would be for visitor parking and they should be parked as such.
- Remove the note showing snow storage in the fire lane.
- Consider plan north vs. true north in explaining projects (not a requirement; just consider in explaining the project to Commissioners since often they assume plan north is true north).
- Ensure scale bar is accurate in application submittal.
- The intent in Title 21 for the gC-8 District includes a human scale and strong relationship to the street; ensure you address the zoning district intent in the application.
- Ensure there are pedestrian connections within the parking lot for people to cross and roofed entrances; understand that a walkway across all driveways for townhouses does not make sense – still need pedestrian connection from the townhouses to the mixed use development.

Long Range Planning

- Consider improving street frontage and connection to the street.
- Review the description for the town center in the [Girdwood Area Plan](#).
- Also review the [Girdwood South Townsite Master Plan](#), and the [Girdwood Commercial Areas and Transportation Master Plan](#).

Private Development

- Development Services confirmed no required improvements in California Creek right-of-way and petitioner confirmed no plan for improvements in California Creek right-of-way.
- Curb, gutter, and sidewalk will be required along the full property frontage along Hightower Road.
- Discussion on drainage: There is a ditch on west side of Hightower Road. When that is filled in for the sidewalk, drainage will push into the curb line and the curb line is funky because of the driveways and curb bumps. There is an existing storm drain to that you can tie into. Drainage is coming south in the ditch. The drainage will need to go somewhere and will need to be diverted; cannot block drainage with the new sidewalk. Could it be taken down California Creek Way towards California Creek? Discussed and that is unlikely. Road drainage as well as parking lot drainage will have to be accommodated; the curb line is tricky. Chris Burt on the petitioner's team will address drainage. Could potentially have an inlet between the driveways with ditches on the other side of the sidewalk. Town center areas have 11-foot lanes, so 22 feet of pavement excluding shoulders for the roads. Drainage plan is still being developed.
- Discussion on curb and gutter: Rolled curb or no curb is the petitioners' preference. No curb is not an option; curb and gutter must be provided. Municipal staff to have follow-up discussion on curb requirements. WMS interested in curb follow-up discussion as it relates to drainage. 21.09 requires barrier curb in the commercial town site. Petitioner stated rolled curb is easier to maintain and variances from Title 21 may be requested with the development master plan.

- Discussion on lighting: See that the street lighting stops at the post office and then picks up north of the site towards Girdwood Elementary. Will leave to Traffic to determine street lighting requirements. Municipal staff to have follow-up discussion on lighting requirements.
- Separate internal meeting held (see comments at end of notes) to clarify guidance on drainage, curb and gutter, parking, and lighting.

Traffic Engineering

- Two driveways are allowed if they are separated by 20 feet and they will improve site circulation. The proposed layout seems to meet those requirements.
- ADA parking is sharing an aisle.
- Confirmed on diagram the parking and land banking is shown in the right-of-way for California Creek. This is an issue because land banking is for developed rights-of-way.
- Confirmed the proposed parking spaces are outside of required setbacks.
- Marco flashed parking reduction draft document on screen, which was later shared with Traffic Engineering for initial review.
- A separate discussion is necessary for the parking reductions. Make sure you are able to get the parking reductions before the Municipality reviews the site plan. It would make permitting go more smoothly to do that. Reductions are approvable by the Traffic Engineer and Planning Director. The new Municipal Traffic Engineer is Brad Coy.
- There is not residential office support for the townhouses for parking space considerations.
- If there is rolled curb, it should be detached from the sidewalk. If there is barrier curb, then it can have attached sidewalk.

Girdwood Service Area Manager

- Tying into existing drainage system that is working in Town Square is preferable.
- The undeveloped right-of-way is important for snow storage.

Watershed Management

- Most of the site is within the 500-year floodplain. There are no municipal requirements for the 500-year floodplain, so this would be an advisory comment. In general, flood maps in Girdwood are out of date, so an area within the 500-year floodplain may have a greater chance to be mapped more restrictively in the future. That mapping change is probably out of the timeframe for this project. Be aware that different lending institutions have different constraints in case the 500-year floodplain would potentially affecting funding.
- Building A is outside of the 100-year floodplain. The northeast corner of proposed future Building B might be within the 100-year floodplain.
- Jeff Urbanus will share a flood map for Girdwood made by HDR a few years ago.
- Setbacks should be based upon the most recent 2021 survey marking ordinary high water and not the 2006 survey; show that clearly on submitted maps. Communicate with Kyle Cunningham if necessary.
- The back side of the townhouses are really close to the riparian edge zone. The riparian edge zone must not be disturbed during construction.

- Decks, not to exceed 500 square feet, can overhang into the riparian edge zone. There may be multiple 500 square-foot decks and up to half of the riparian edge setback area.
- T&E utilities can be within the riparian edge zone. Marcus confirmed the lines are underground and would remain underground.
- Support design showing no basements for this site.

Fire, AWWU, State of Alaska DOT&PF

Not present.

Additional post-meeting comments from municipal staff:

- General:
 - Development Master Plan must specifically call out any proposed variances from Title 21 code requirements.
- Lighting:
 - Hightower Road is classified as a collector roadway in the [OS&HP](#). This parcel also fronts a portion of Hightower Road that would meet Girdwood "Town Site Commercial Area Streets". Petitioner should expect that additional street lighting will be required with the development. The fixture type and quantity will require submittal from electrical engineering showing that minimum standards of Chapter 5 of the DCM are met for roadway and pedestrian safety parameters.
 - A map of existing streetlights is available here: <https://muniorg.maps.arcgis.com/apps/webappviewer/index.html?id=4d15f7b65cdb4ddc930ad1feb6f40758>
- On-site parking:
 - The petitioner needs to provide parking calculations for all uses on the site.
 - The preliminary site plan shows Building B. If this remains, then calculations will be required based on retail use per AMC Chapter 7.
 - There are no calculations supplied for the proposed residential uses, but it appears that adequate parking may be provided for that use. Designate guest parking spaces on the plans.
 - Parking requirements will go down slightly since petitioner was using restaurant use requirements for initial parking calculations.
 - Provide turning radii for Fire Department review.
- Parking reductions:
 - Petitioner needs to set up a meeting that includes both Traffic Engineering and Planning staff, to include both Planning Director and Traffic Engineer.
 - Traffic Engineering had the following comments on draft proposed reductions in a document sent to them for review:
 - The percentage reductions are multiplicative and not additive. For example, $0.9 \times 0.95 = 0.855$ reduction.
 - F.8: Use adjacent to Transit. Petitioner needs to provide information that there is an active bus stop with 800 feet of the property. Since Glacier Transit is not a municipal bus route, the bus stop information needs to be provided for determination. (5%)

- F.10. Transit Pass Benefits: Is the Girdwood shuttle free? How do we grant transit pass benefits for a free shuttle? (10%) There may be a precedent to grant this reduction from Girdwood Brewery.
 - F.12. Land Banking: Proposed area of land banking is in the municipal right-of-way. Land banking in the right-of-way is not allowed since the property is not owned by the petitioner. If this parking were needed in the future, then California Creek Way would be required to be developed.
 - F.16.D Shared parking within 800 feet. The site plan does not show any other use that they would be sharing with at this time. This would also only be eligible if the adjacent shared use had parking in excess of their needs. (Not applicable to this development)
 - F.21. Small Parking Spaces for Employee Parking. The current site plan shows full-size spaces. This reduction only allows for 20% of required parking to be narrower in width. This does not appear to be applicable at this time. It also does not reduce any number of required spaces, just the space size.
 - F.22. Bicycle Parking. Petitioner will be required to show the 12 proposed bicycle spaces on drawings that meet standards in 21.07.060 F.15
 - 21.09.070 L.2 Girdwood Specific Parking Regulations. Petitioner needs to identify the spaces that are within 600 feet of the property that they propose to use.
- Curb and gutter:
 - Curb and gutter is required per 21.09.070F.4.b.
 - The curb bulbs will be problematic for drainage. Recommend getting rid of parking bulbs for the bays on Hightower Road and straightening the curb line.
 - There is no rolled curb in the existing developed areas of the Girdwood town square area. All of it is barrier (vertical) curb. Municipality would like Hightower Road to have vertical curb to match existing development in the Girdwood town center and to meet intent of gC-8 District.
 - Vertical barrier curb is okay with Girdwood Service Area Manager for maintenance if the curb bump out bulbs in the design along Hightower Road are removed to create a straight curb line.
 - Traffic Engineering okay if sidewalks are attached to back of vertical curb (but must not be attached for rolled curb).
 - Vertical curb will direct runoff better.
- Drainage issues:
 - Use existing town square drainage system.
 - Tie into existing storm drain system to beehive and send to inlet. There is no ditch leading to inlet; it ends at the post office.
 - On-site drainage treatment: need to show swales or other LID.
 - Undeveloped right-of-way important for snow storage.

DRAINAGE PROJECT NOTIFICATION AND WMS MAPPING REQUEST

Applicant Name*	Nathen Ellis	Contact Information* (Phone and email)	nathen49@gmail.com
Mailing Address*	PO Box 274 Girdwood, AK 99587		
Property Description* (subdivision, lot(s), and block)	Commercial Vacant Land; Lot		
Plat/Grid Number	Grid: SE 4715 / Plat: 950035	MOA Assessor's Office Property Identification Number*	075-031-41-000
MOA Case Number(s) (Indicate which provided)			
Project Category* (Check one)	<div style="display: flex; justify-content: space-between;"> <div> Crossing: Single-Lot Residential: <input type="checkbox"/> Small: <input type="checkbox"/> Large: <input type="checkbox"/> </div> <div> <input type="checkbox"/> Class 1A <input type="checkbox"/> Simple <input type="checkbox"/> Simple </div> <div> <input type="checkbox"/> Class 1B <input type="checkbox"/> Complex <input checked="" type="checkbox"/> Complex </div> <div> <input type="checkbox"/> Class 1C <input type="checkbox"/> Class i D </div> </div>		
Parcel Physical Location* (Driving directions, roads, etc)	Drive North on Alyeska Highway, take left on Hightower Rd, the property is		

Requested Services*

- ☐ Review watercourse mapping
- ☐ Special review services requested
- ☒ Flag stream features on-site
☒ Notify prior to site visit
 Attempt to notify will be made using the contact information provided; however, pre-visit contact or scheduling for applicant presence cannot be guaranteed

on the left right
after the Post Office.

Requested completion date: As soon as possible; 08/11/21 (Preferred completion date)

No later than completion date: 08/20/21 (Later completion may incur significant project delay)

Dates and service availability cannot be guaranteed. Scheduling is based on Planning Department priorities, request receipt order, and seasonal constraints (at minimum, mapping review requires channels to be free of snow and ice)

* Required Information

Attachments: ← Site-plans previously submitted.

- ☐ Draft map of known watercourses showing all streams and major drainageways entering, crossing or exiting the project, and the location(s) of downstream receiving waters.
- ☐ Preliminary post-development site map
- ☐ Threshold calculation (required for Single-Lot Residential and Small Projects only)
- ☐ Tabulation of pre- and post-development landcover (required for Small and Large Projects only)

Certification:

By signature below, I certify that I am legally entitled to authorize the requested services and that the attachments provided are complete and accurate representations of known site conditions and project plans. I further authorize Municipality of Anchorage (MOA) personnel to access the referenced site on foot for the purposes of identifying and / or mapping drainage features

This form and its attachments constitute my notice to the MOA that I am developing plans for a drainage project or platting action and will be submitting a report of existing or proposed drainage conditions. I understand that all drainage projects are governed by the MOA Project Management and Engineering Design Criteria Manual, the MOA Drainage Design Guidelines, the Anchorage Municipal Code, and other state and federal regulations and permits

NE

Signed

8-5-21

Date



Marco Zaccaro
PO Box 842
Girdwood, AK 99587

(907) 783-1090
(907) 783-1095 fax
zarchitects@alaska.net

GLACIER CITY CENTER Design, Permitting, and Construction Timeline **rev. 12/20/21**

Items in green are completed.

Items in blue are proposed design/progress meeting dates

Items in black are in process or to be done.

Project Schedule

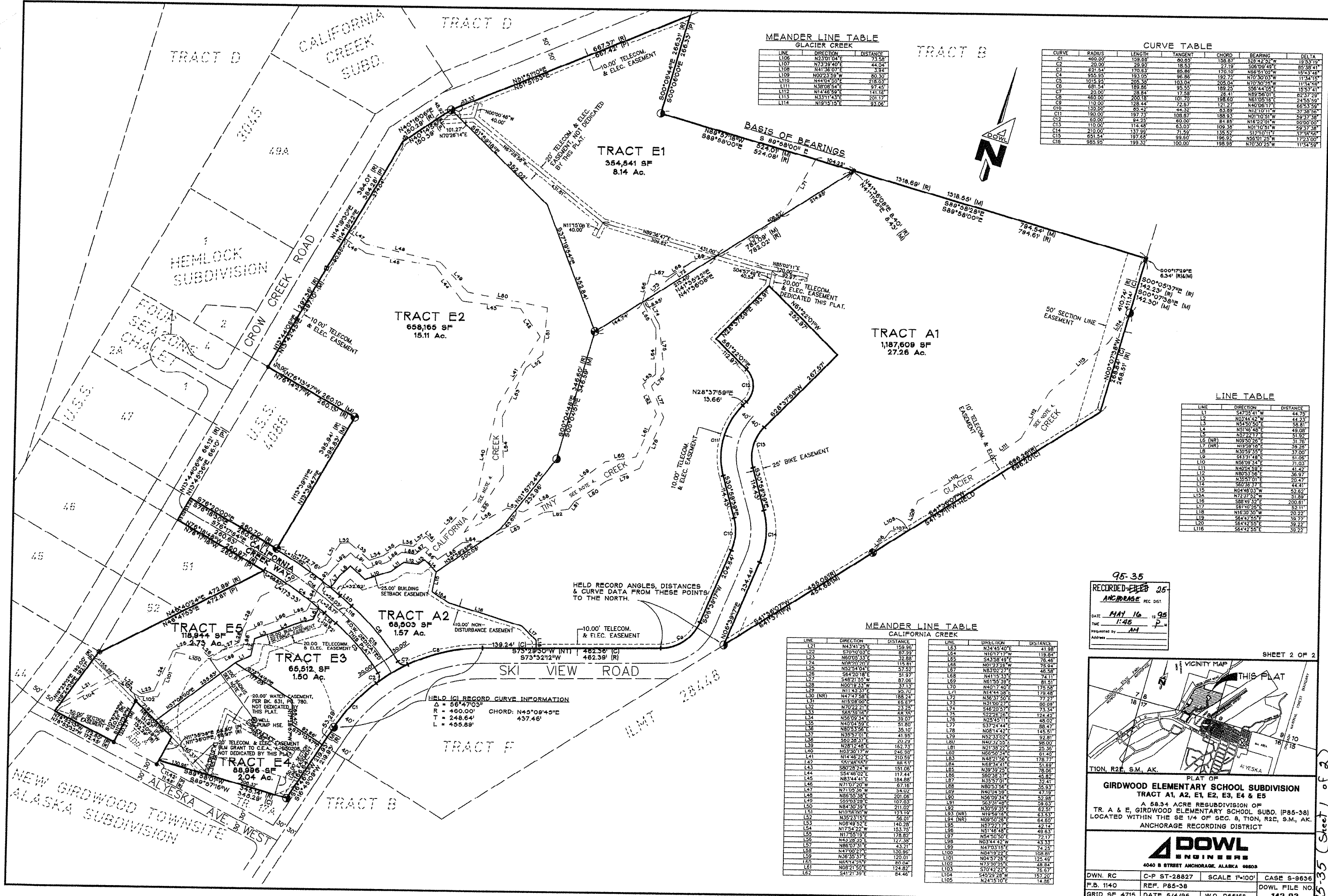
- May 6, 2021 Initial Site Visit
- May 11, Start programming effort
- May 25, Finish Zoning Analysis.
- May 26, GCC Client Meeting - programming and stacked condo concept, Design Review with client.
- June 1, Received program info from client.
- June 2, Change condo buildings to townhouse.
- June 3, Start townhouse footprint studies site plan.
- June 20, Completed first pass concept design for Building A.
- June 20, Preliminary parking analysis completed.
- June 29, GCC Meeting – Review site plan concepts and Building A concept design.
- July 6, Start addressing stream setback ordinance change on GCC site plan.
- July 8, Received old stream edge AutoCAD drawing from surveyor.
- July 9, Finish site plan D addressing new stream setback and taking advantage of vacating stream plat note.
- July 12, MOA design concept meeting narrative completed.
- July 27, GCC Meeting – MOA Planning Department design concept meeting.
- July 30, MOA determination received regarding residential uses on first floor, distillery, and new stream setback ordinance.
- August 5, Informal permitting discussion with Mike Edgington.
- August 9, Start Development Master Plan.
- August 20, Building A code analysis (partial).
- August 25, GCC stream edge mapping – MOA Watershed Management and F.R. Bell survey of mapping.

- September 3, 3D Massing model for building A due for client review and comment.
- September 14, Stream edge survey document due from F.R. Bell.
- September 14, 3D Townhouse massing model due for client review and comment.
- [September 14,](#) 3D Concept Design Review Meeting; Attendees: Marco, Nathen, and Liz. Review project schedule.
- September 20, Deadline for notice of community meeting to be sent out per AMC 21.03.020.C.4
- **September 20,** **15% 3D Conceptual site model with building massing models submittal** for client review and comment.
- September 30, Owner comments regarding concept site and building 3D models due.
- October 1, Start Architectural 35% Schematic Design phase.
- [October 11,](#) **35% Architectural Design Review Meeting and submittal: Site, Building A and Townhouses;** Attendees: Marco, Nathen, and Liz.
- October 11, Girdwood Land Use Committee first presentation. This will serve as the Community meeting as required by AMC 21.03.020.C.2.a.vi
- October 18, Girdwood Board of Supervisors first presentation.
- November 3, MOA Planning Department Pre-Application conference.
- November 4-15, Sent: DMP engineering assistance RFP's:
 - Landscape Architect, Site Electrical, and Civil RFP for entire project. requested separate fee for Development Master Plan Component.
 - Requested fee for building A mechanical to 15% for civil assistance.
- November 4, Sent: Structural engineering RFP for townhouses.

- November 5, State Fire Marshal determination that townhouses will require stamped M&E drawings.
- November 5, Sent: Mechanical and Electrical RFP for townhouses.
- November 9, Received preapplication meeting notes from MOA planning department.
- November 9-16, Received: Landscape Architect, and Civil proposals for DMP.
- November 8-17, Received: Structural, Mechanical, and Electrical Engineering proposals for townhouses. Site electrical engineering for Development Master Plan.
- November 11, Client – Contractor initial constructability meeting.
- November 19, N.T.P.: 35% Civil, electrical and Landscape site design for DMP.
- November 19, N.T.P.: 35% Electrical, and Structural design for Townhouses.
- November 22, N.T.P. Mechanical Design for Townhouses.
- November 22, Resume electrical line reroute effort based on surveyed riparian zone edge.
- November 30, Plan review meeting with MOA traffic engineer.
- December 2, Electrical easement reroute proposal from CEA.
- **December 5,** **35% Design Submittal for Development Master Plan:** Architectural, Civil, Landscape and Site Lighting Plan as required for MOA 'Development Master Plan'.
- **December 6,** **Submit 'Development Master Plan'** to MOA planning department. Cut-off date for February 14 Planning and Zoning Hearing.
- **December 13,** **35% Townhouse Schematic Design Submittal:** Architectural and Structural.

- December 21, End of MOA completeness check.
- December 23, DMP application sent to GBOS/LUC.
- **January 4,** **35% Townhouse Schematic Design Submittal: Mechanical and Electrical.** 35% electrical Site plan and mechanical assist civil in sizing utilities to building A.
- January 10, Girdwood LUC hearing. Cannot hold in February as it conflicts with the P&Z hearing. LUC will have application but not staff report.
- January 17, Girdwood GBOS hearing: request a resolution of support.
- January 31, End of MOA review period and MOA staff report sent.
- **January 31,** **65% Townhouse Design Development Submittal - Architectural and Structural.** These documents will also respond to any MOA comments that have been received.
- **February 7,** **65% Townhouse Schematic Design Submittal: Mechanical and Electrical.** 65% electrical Site plan.
- **February 14,** **Planning and Zoning Commission Hearing.** Development Master Plan approved. Date preliminary pending publication of 2022 hearing dates.
- **March 4,** **Civil Documents Submittal:**
 - AWWU plan set and application for the water and sewer to building A and townhouses
 - 100% stamped drawings for water and sewer to townhouses.
 - Grading and drainage plan for the entire site.
 - Townhouses Plot Plan
- **March 7,** **AWWU Water and Sewer Permit Application.**
- **March 14,** **95% Townhouse Construction Documents and Land Use Permit Submittal.** All disciplines. *LUP submittal requires architectural plans, section, elevations, civil and landscape permitting drawings*

- March 21, Anticipated AWWU permit approval date.
- March 30, Start deep utility installation – only requires an AWWU permit.
- April 1, **100% Townhouse Construction Documents Submittal.** All disciplines. Submit townhouses to Alaska DPS (state fire marshal) for full Plan Review (if required). *Date dependent on receipt of plot plan.*
- April 14, Anticipated MOA Land Use Permit approval date.
- May 1, Start of migratory bird restrictions (no tree clearing allowed after this date).
- May 1-30 Typical load restriction period.
- May 20, Anticipated Alaska DPS townhouses full approval.
- May 30, 2022 Start site and townhouses construction, after load restrictions lifted.
- May 30, 2023 Finish major construction. Start FF&E.
- June 14, 2023 Site and Townhouses completed.
- **Note: Building A construction schedule to be determined.**



MEANDER LINE TABLE
GLACIER CREEK

LINE	DIRECTION	DISTANCE
L106	N23°01'04"E	73.59
L107	N73°39'40"E	44.04
L108	N41°38'07"E	3.94
L109	N00°23'59"W	80.30
L110	N44°04'50"W	218.03
L111	N38°08'54"E	97.45
L112	N14°46'59"E	141.16
L113	N35°11'43"E	201.17
L114	N19°15'19"E	93.06

CURVE TABLE

CURVE	RADIUS	LENGTH	TANGENT	CHORD	BEARING	DELTA
C1	400.00	138.80	60.80	108.67	S28°42'52"W	19°33'19"
C2	20.00	29.90	18.53	27.19	S02°09'19"E	85°38'41"
C3	621.54	170.63	85.86	170.10	N56°51'02"W	15°43'46"
C4	955.95	193.05	96.86	192.72	N70°30'03"W	11°34'18"
C5	1015.24	205.38	103.04	203.04	N70°30'25"W	11°34'46"
C6	681.54	189.86	96.55	189.25	S56°44'05"E	15°37'41"
C7	20.00	28.84	17.58	26.41	N89°56'01"E	62°37'20"
C8	460.00	200.18	101.70	198.60	N61°05'18"E	24°55'59"
C9	110.00	128.44	63.52	121.27	N40°06'17"E	66°53'59"
C10	130.00	135.42	72.67	121.27	N40°06'17"E	66°53'59"
C11	180.00	197.73	108.87	188.93	N01°10'51"W	37°38'59"
C12	60.00	84.25	60.00	84.85	N16°22'01"W	00°00'00"
C13	114.48	114.48	63.03	109.38	N01°10'51"W	37°38'59"
C14	210.00	137.99	71.59	135.52	S12°01'17"E	57°14'52"
C15	651.54	197.68	99.50	196.92	N56°01'25"W	17°23'00"
C16	985.95	199.32	100.00	198.98	N70°30'25"W	11°34'59"

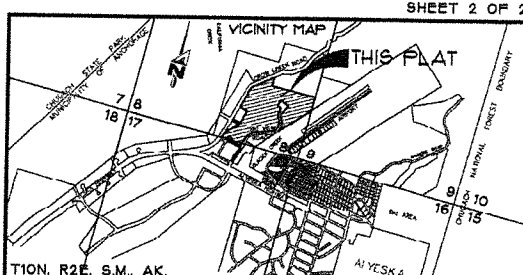
LINE TABLE

LINE	DIRECTION	DISTANCE
L1	S47°35'41"W	44.79
L2	N03°44'42"W	44.23
L3	N54°50'50"E	36.81
L4	N51°46'48"E	49.08
L5	N57°22'17"E	51.92
L6 (NR)	N09°50'26"E	31.76
L7 (NR)	N15°28'10"E	29.26
L8	N30°59'35"E	37.00
L9	S63°31'48"E	51.05
L10	N65°59'34"E	71.03
L11	N40°04'58"E	41.42
L12	N80°53'56"E	36.97
L13	N35°57'01"E	20.47
L14	S20°39'37"E	44.41
L15	N04°48'03"W	52.62
L15A	N72°37'52"W	51.89
L16	S88°49'32"E	200.61
L17	S81°05'35"E	52.11
L18	N16°30'30"W	20.22
L19	S84°42'55"E	39.22
L20	S64°42'55"E	39.22
L116	S64°42'55"E	39.22

MEANDER LINE TABLE
CALIFORNIA CREEK

LINE	DIRECTION	DISTANCE
L1	N43°41'23"E	159.96
L2	S70°10'03"E	87.92
L3	N60°09'33"E	32.88
L4	N08°20'20"E	115.81
L5	N52°34'04"E	57.52
L6	S64°20'18"E	51.97
L7	S48°21'55"W	87.06
L8	N00°19'33"W	37.13
L9	N11°43'37"E	20.29
L10 (NR)	N47°47'58"E	95.70
L31	N15°08'00"E	65.82
L32	N70°22'21"E	23.25
L33	S65°38'10"E	68.25
L34	N55°09'34"E	19.07
L35	N40°04'59"E	51.80
L36	N80°53'56"E	35.10
L37	N35°57'01"E	41.95
L38	S60°26'27"E	20.29
L39	N28°12'48"E	162.73
L40	N03°30'17"W	246.90
L41	N14°46'22"E	210.59
L42	S51°46'52"E	88.51
L43	S80°28'24"W	151.06
L44	S54°48'02"E	117.44
L45	N83°44'41"E	136.60
L46	N71°07'20"W	97.16
L47	N71°02'36"W	54.02
L48	N86°55'38"E	201.06
L49	S29°03'28"E	107.03
L50	N84°30'19"E	210.02
L51	N10°58'00"W	173.19
L52	N35°23'15"E	56.01
L53	N08°48'52"E	140.28
L54	N17°54'22"W	153.75
L55	N17°55'19"E	178.82
L56	N43°28'30"E	127.38
L57	N86°07'31"E	43.21
L58	N47°20'23"E	136.60
L59	N36°35'37"E	120.01
L60	N64°44'25"E	80.04
L61	N08°21'50"E	124.82
L62	S41°21'39"E	84.46
L63	N34°45'40"E	41.98
L64	N01°21'17"W	119.84
L65	S43°58'49"E	76.48
L66	N01°23'28"W	75.94
L67	N83°02'27"E	46.58
L68	N41°15'33"E	74.11
L69	N61°35'59"E	81.51
L70	N40°17'40"E	175.58
L71	N14°44'38"E	179.48
L72	N36°19'50"E	77.27
L73	N31°00'27"E	60.09
L74	S46°20'57"E	73.34
L75	S22°28'26"E	124.42
L76	N23°45'11"E	48.02
L77	S37°24'44"E	88.47
L78	N08°14'42"E	145.51
L79	N52°33'02"E	92.81
L80	N40°32'55"E	98.00
L81	N21°18'22"E	25.35
L82	N66°50'34"E	61.40
L83	N48°21'56"E	178.77
L84	N66°44'41"E	51.69
L85	N39°39'37"E	78.00
L86	S80°38'37"E	45.82
L87	N35°57'01"E	32.41
L88	N80°53'56"E	35.93
L89	N40°04'59"E	37.19
L90	N08°19'33"W	52.98
L91	S63°31'48"E	59.63
L92	N30°59'35"E	62.51
L93 (NR)	N15°28'10"E	62.51
L94 (NR)	N09°50'26"E	64.60
L95	N57°22'17"E	42.14
L96	N51°46'48"E	49.83
L97	N54°50'50"E	72.17
L98	N03°44'42"W	45.33
L99	N47°03'18"E	74.25
L100	N84°19'22"E	108.81
L101	N04°37'26"E	125.43
L102	N73°30'25"E	48.84
L103	S70°42'22"E	76.67
L104	S45°29'28"W	157.20
L105	N24°15'10"E	14.85

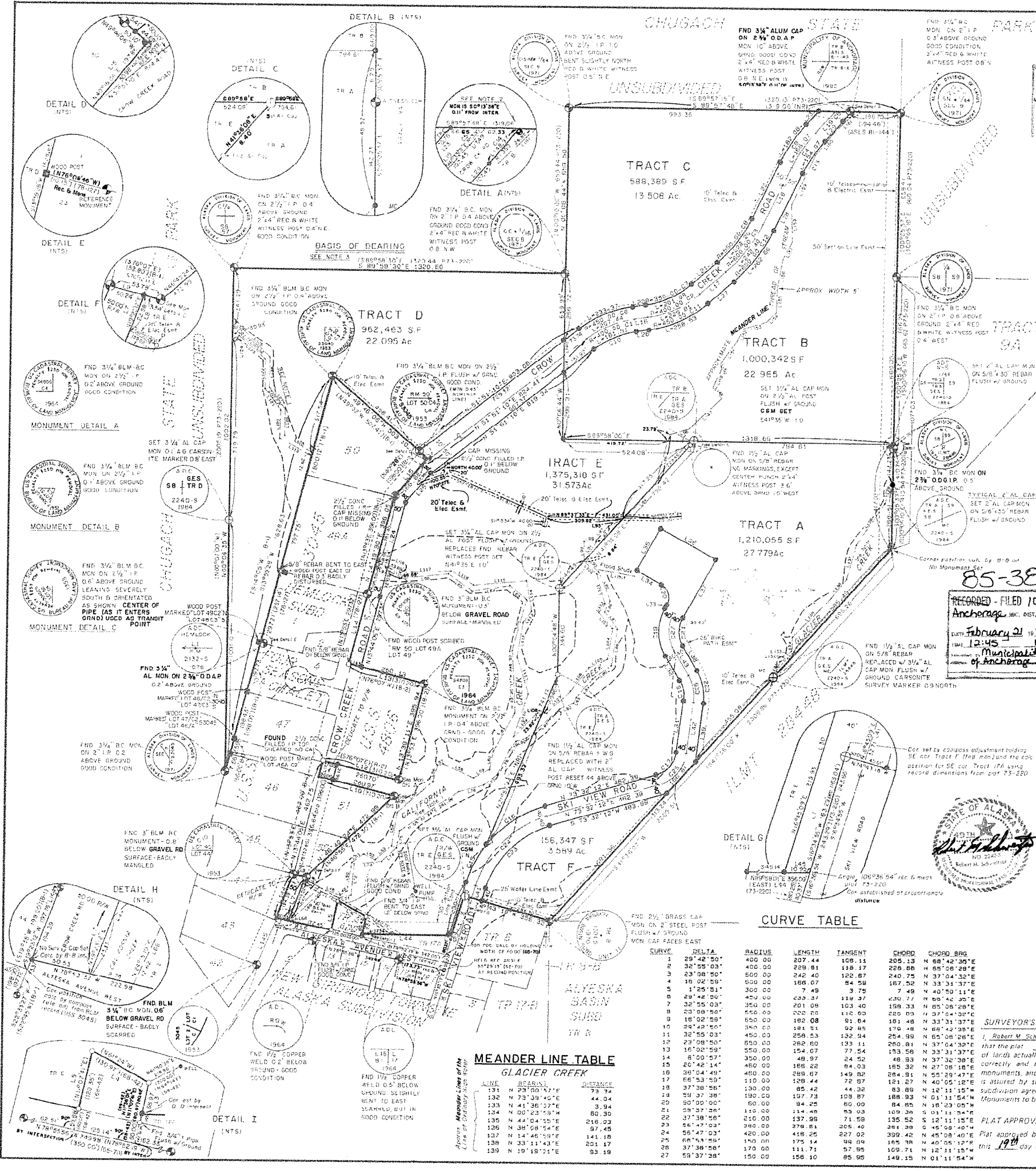
95-35
RECORDED FILE 25-
ANCHORAGE REC. DIST.
DATE MAY 16 1995
TIME 1:45 P.
Requested by AM
Address



PLAT OF
GIRDWOOD ELEMENTARY SCHOOL SUBDIVISION
TRACT A1, A2, E1, E2, E3, E4 & E5
A 58.34 ACRE RESUBDIVISION OF
TR. A & E, GIRDWOOD ELEMENTARY SCHOOL SUBD. (P85-38)
LOCATED WITHIN THE SE 1/4 OF SEC. 8, T10N, R2E, S.M., AK.
ANCHORAGE RECORDING DISTRICT

DOWL
ENGINEERS
4040 B STREET ANCHORAGE, ALASKA 99503
DWN. RC C-P ST-28827 SCALE 1"=100' CASE S-9636
F.B. 1140 REF. P85-38 DOWL FILE NO.
GRID SE 4715 DATE 5/4/95 W.O. D55168 142-93

95-35 (Sheet 1 of 2)



LEGEND

- RECOVERED PRIMARY MONUMENT AS SHOWN
- RECOVERED SECONDARY MONUMENT AS SHOWN
- RECOVERED 1 1/2" X 2 1/2" WOOD POSTS W/SCRIBING
- SET 3 1/4" ALUM. CAP MONUMENT ON 2 1/2" ALUM. PIPE 30" LONG, WITH 2 5/8" X 1/2" ORANGE CARBONITE SURVEY MARKER, (CSM) 6" LONG, 2" IN THE GROUND.
- SET 2" ALUM. CAP ON 5/8" X 30" REBAR

(B-1) RECORD DIMENSIONS FROM BLM PLAT OF USS 3045 DATED MARCH 18, 1954

(B-2) RECORD DIMENSIONS FROM PLM PLAT OF USS 4806 DATED APRIL, 1965

(65-70) RECORD DIMENSIONS TAKEN FROM PLAT OF NEW GIRDWOOD TOWNSHIP ALASKA SUBDIVISION (P465-70)

(68-42) RECORD DIMENSIONS TAKEN FROM PLAT OF TRACT A ADDITION TO NEW GIRDWOOD TOWNSHIP ALASKA SUBDIVISION (P465-42)

(73-320) RECORD DIMENSIONS TAKEN FROM STATE OF ALASKA SUPPLEMENTAL CADASTRAL SURVEY PLAT OF T10N, R2E, S.M., AK. DATED 11-14-73

(78-127) RECORD DIMENSIONS TAKEN FROM PLAT OF HEMLOCK SUBD. (P478-127)

MEANDER LINE - SEE NOTE 5

100 YEAR FLOOD LIMIT (APPROX.) TAKEN FROM GAAB AERIAL PHOTO MAP, GIRDWOOD GRID 7561, DATED 1974 SCALE 1"=100'

(C-9) CURVE NO - SEE CURVE TABLE

(L-1) LINE NO - SEE LINE TABLE OR MEANDER LINE TABLE

(NR) NOT RADIAL

TELE 5 ELEC TELECOMMUNICATIONS AND ELECTRICAL EASEMENT

NOTES

- A stream maintenance easement 25-feet wide along each bank, measured from the line of ordinary high water, provided that all stream maintenance easements along water or drainage courses less than 5-feet at ordinary high water shall be 25-feet wide centered on the thread of the stream. (Per M.O.A. 21.80.040)
- Centerline and right-of-way information for Crow Creek Road is taken from an unrecorded plat of ASLS 81-149, Tract A & Tract B Girdwood Municipal Land Selection. The bearings and distances were input and adjusted by compass rule to tie between the east boundary of Lot 50 USS 3045 and the right-of-way monument on the north line of this subdivision (NW Corner Tract B).
- Basis of bearing is taken from State of Alaska Supplemental Cadastal Survey Plat of T10N, R2E, S.M., AK. dated 11-14-73.
- Well Site Reserve: No buildings to be constructed in this area, per Plat 68 42.
- Meander lines for California Creek and Glacier Creek were obtained from aerial topo map by Walker-Alexis Aerial Surveys, Inc. Scale 1"=60', date of photography 7/15/84, and supplemented by ground traverse.

MEANDER LINE TABLE
CALIFORNIA CREEK

LINE	BEARING	DISTANCE	LINE	BEARING	DISTANCE
1	N 22°26'28"W	124.42	89	N 22°26'28"W	124.42
2	N 78°55'56"W	241.67	90	N 46°20'57"W	73.47
3	N 1°10'33"E	68.47	91	N 31°05'45"E	60.08
4	N 51°20'20"W	129.20	92	N 38°33'37"E	137.27
5	N 10°52'53"W	120.51	93	N 61°50'39"E	61.51
6	N 78°53'30"W	222.98	94	N 40°17'40"E	175.58
7	N 19°23'10"E	122.00	95	N 14°44'38"E	103.31
8	N 19°23'10"E	20.38	96	N 72°10'03"E	70.25
9	N 76°12'11"E	53.78	97	N 38°15'45"W	42.38
10	N 76°18'48"W	310.97	98	N 33°02'27"W	46.55
11	N 13°44'05"E	66.11	99	N 43°58'49"E	75.89
12	N 38°33'37"E	310.70	100	N 38°15'37"W	159.84
13	N 76°14'27"E	310.13	101	N 34°45'40"W	84.48
14	N 13°44'05"E	297.33	102	N 41°21'39"E	104.05
15	N 40°16'05"E	197.08	103	N 55°14'25"W	60.04
16	N 61°34'05"E	34.09	104	N 27°24'30"E	120.01
17	N 40°30'51"E	134.50	105	N 38°15'37"W	130.98
18	N 27°24'30"E	250.10	106	N 43°58'49"E	5.10
19	N 41°33'07"E	107.45	107	N 43°58'49"E	5.10
20	N 01°34'00"E	50.00	108	N 43°58'49"E	5.10
21	N 48°38'57"E	134.50	109	N 43°58'49"E	5.10
22	N 01°34'00"E	259.48	110	N 43°58'49"E	5.10
23	N 41°33'07"E	70.78	111	N 43°58'49"E	5.10
24	N 14°18'00"E	364.01	112	N 43°58'49"E	5.10
25	N 40°16'05"E	150.29	113	N 43°58'49"E	5.10
26	N 81°34'00"E	52.09	114	N 43°58'49"E	5.10
27	N 48°38'57"E	134.50	115	N 43°58'49"E	5.10

CURVE TABLE
GLACIER CREEK

LINE	BEARING	DISTANCE	LINE	BEARING	DISTANCE
1	N 23°00'47"E	73.34	131	N 73°39'40"E	44.04
2	N 73°39'40"E	44.04	132	N 41°36'27"E	3.94
3	N 41°36'27"E	3.94	133	N 00°23'58"W	60.30
4	N 00°23'58"W	60.30	134	N 44°04'15"E	97.45
5	N 44°04'15"E	97.45	135	N 38°08'54"E	141.18
6	N 38°08'54"E	141.18	136	N 6°14'55"E	201.17
7	N 6°14'55"E	201.17	137	N 33°11'43"E	93.19
8	N 33°11'43"E	93.19	138	N 19°18'21"E	156.10
9	N 19°18'21"E	156.10	139	N 01°11'54"W	149.15

SURVEYOR'S CERTIFICATE
I, Robert M. Schaeffer, a professional land surveyor do hereby certify that the plat shown herein is a true and correct representation of the land actually surveyed and that the distances and bearings are shown correctly and that all monument locations, all other monuments, and lot corners have been set and staked, or if final completion is assured by subdivision agreement, they will be set as specified in said subdivision agreement. Lot corners to be set by NA.
Monuments to be set by NA.

PLAT APPROVAL
Plat approved by the Municipal Planning Authority this 19th day of February, 1985.

VICINITY MAP SCALE 1" = 1 MILE
USS Seward (D-8) AK 1951
Minor Revisions 1974

PLAT OF GIRDWOOD ELEMENTARY SCHOOL SUBDIVISION
TRACTS A, B, C, D, E & F
A SUBDIVISION OF TRACT 8A, SECTION 8, T10N, R2E, S.M., AK (P73-220)
Located Within E1/2 Sec. 8, T10N, R2E, S.M., AK CONTAINING 132.244 ACRES

ALASKA DEVELOPMENT CONSULTANTS, INC.
ENGINEERS SURVEYORS PLANNERS
5313 Arctic Blvd., Suite 201, Anchorage, Alaska 99509

Scale 1"=200' Date 11/01/84 Book P-57 & P-62
Drawn PLH/DCS File No. WO No. Proj No. 84048
GR10 SE 4615 & SE 4715 S-7812

CERTIFICATE OF OWNERSHIP AND DEDICATION

I (we), hereby certify that I (we) hold the herein specified property interest in the property described herein. I (we) hereby dedicate to the Municipality of Anchorage all areas depicted for use as public utility easements, streets, alleys, thoroughfares, parks, and other public areas shown herein. There shall be reserved adjacent to the dedicated streets shown herein, a slope of 1:1 for each 1 foot vertical (11 to 1) of cut or fill for the purpose of providing and maintaining the lateral support of the constructed streets. There is reserved to the grantor, their heirs, successors and assigns, the right to use such area at any time upon providing and maintaining other adequate lateral support, as approved by the Municipality.

I (we) hereby agree to this plat, and to any restriction or covenant appearing hereon and any such restriction or covenant shall be binding and enforceable against present and successive owners of this subdivided property.

Robert M. Schaeffer
Municipality of Anchorage
Planner & Engineer
Anchorage, Alaska

NOTARY ACKNOWLEDGEMENT FOR BILL LURIA
Subscribed and sworn to before me this 17th day of February, 1985

5-18-88
My Commission Expires
Anchorage, Alaska

ACCEPTANCE OF DEDICATION

The Municipality of Anchorage hereby accepts for public uses and for public purposes the real property dedicated on this plat including, but not limited to the easements, rights of way, alleys, roadways, thoroughfares and parks shown hereon.

Dated at Anchorage, Alaska this 19th day of February 1985

Attest:
Lucy E. Smith
Mayor of Anchorage
B. L. Luria
Municipal Engineer

TAX CERTIFICATION

All real property taxes levied by the Municipality of Anchorage on the area shown on this plat have been paid.

1-15-88
Anchorage, Alaska

APPROVALS

Planning Officer *W. W. J.* Date 2-20-85
Municipal Engineer *B. L. Luria* Date 2-14-85
W.B.C. 2-13-85

LINE TABLE

LINE	BEARING	DISTANCE	LINE	BEARING	DISTANCE
1	N 23°00'47"E	73.34	131	N 73°39'40"E	44.04
2	N 73°39'40"E	44.04	132	N 41°36'27"E	3.94
3	N 41°36'27"E	3.94	133	N 00°23'58"W	60.30
4	N 00°23'58"W	60.30	134	N 44°04'15"E	97.45
5	N 44°04'15"E	97.45	135	N 38°08'54"E	141.18
6	N 38°08'54"E	141.18	136	N 6°14'55"E	201.17
7	N 6°14'55"E	201.17	137	N 33°11'43"E	93.19
8	N 33°11'43"E	93.19	138	N 19°18'21"E	156.10
9	N 19°18'21"E	156.10	139	N 01°11'54"W	149.15

MEANDER LINE TABLE
GLACIER CREEK

LINE	BEARING	DISTANCE	LINE	BEARING	DISTANCE
1	N 23°00'47"E	73.34	131	N 73°39'40"E	44.04
2	N 73°39'40"E	44.04	132	N 41°36'27"E	3.94
3	N 41°36'27"E	3.94	133	N 00°23'58"W	60.30
4	N 00°23'58"W	60.30	134	N 44°04'15"E	97.45
5	N 44°04'15"E	97.45	135	N 38°08'54"E	141.18
6	N 38°08'54"E	141.18	136	N 6°14'55"E	201.17
7	N 6°14'55"E	201.17	137	N 33°11'43"E	93.19
8	N 33°11'43"E	93.19	138	N 19°18'21"E	156.10
9	N 19°18'21"E	156.10	139	N 01°11'54"W	149.15

MEANDER LINE TABLE
CALIFORNIA CREEK

LINE	BEARING	DISTANCE	LINE	BEARING	DISTANCE
1	N 22°26'28"W	124.42	89	N 22°26'28"W	124.42
2	N 78°55'56"W	241.67	90	N 46°20'57"W	73.47
3	N 1°10'33"E	68.47	91	N 31°05'45"E	60.08
4	N 51°20'20"W	129.20	92	N 38°33'37"E	137.27
5	N 10°52'53"W	120.51	93	N 61°50'39"E	61.51
6	N 78°53'30"W	222.98	94	N 40°17'40"E	175.58
7	N 19°23'10"E	122.00	95	N 14°44'38"E	103.31
8	N 19°23'10"E	20.38	96	N 72°10'03"E	70.25
9	N 76°12'11"E	53.78	97	N 38°15'45"W	42.38
10	N 76°18'48"W	310.97	98	N 33°02'27"W	46.55
11	N 13°44'05"E	66.11	99	N 43°58'49"E	75.89
12	N 38°33'37"E	310.70	100	N 38°15'37"W	159.84
13	N 76°14'27"E	310.13	101	N 34°45'40"W	84.48
14	N 13°44'05"E	297.33	102	N 41°21'39"E	104.05
15	N 40°16'05"E	197.08	103	N 55°14'25"W	60.04
16	N 61°34'05"E	34.09	104	N 27°24'30"E	120.01
17	N 40°30'51"E	134.50	105	N 38°15'37"W	130.98
18	N 27°24'30"E	250.10	106	N 43°58'49"E	5.10
19	N 41°33'07"E	107.45	107	N 43°58'49"E	5.10
20	N 01°34'00"E	50.00	108	N 43°58'49"E	5.10
21	N 48°38'57"E	134.50	109	N 43°58'49"E	5.10
22	N 01°34'00"E	259.48	110	N 43°58'49"E	5.10
23	N 41°33'07"E	70.78	111	N 43°58'49"E	5.10
24	N 14°18'00"E	364.01	112	N 43°58'49"E	5.10
25	N 40°16'05"E	150.29	113	N 43°58'49"E	5.10
26	N 81°34'00"E	52.09	114	N 43°58'49"E	5.10
27	N 48°38'57"E	134.50	115	N 43°58'49"E	5.10

NOTES TO USERS

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where Base Flood Elevations (BFEs) and/or Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM, Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only landward of 0.0' Mean Sea Level (MSL). Total Datum. Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations table in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations table should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the floodways were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by flood control structures. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The projection used in the preparation of this map was Universal Transverse Mercator (UTM) zone 6. The horizontal datum was NAD83, GRS1980 spheroid. Differences in datum, spheroid, projection or UTM zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the Mean Sea Level (MSL) datum. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov/> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOAA, NNGS12
National Geodetic Survey
SSWC-3, #9202
1315 East-West Highway
Silver Spring, MD 20910-3282

To obtain current elevation, description, and/or location information for bench marks shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov/>.

Base map digital files provided by Municipality of Anchorage DPW and AK DNR. Information compiled at scales of 1:1200 to 1:24000 during 2000-06. Streams and lake shorelines associated with FHAs digitized from Army Corp. of Engineers flood hazard workmaps from MOA DPW.

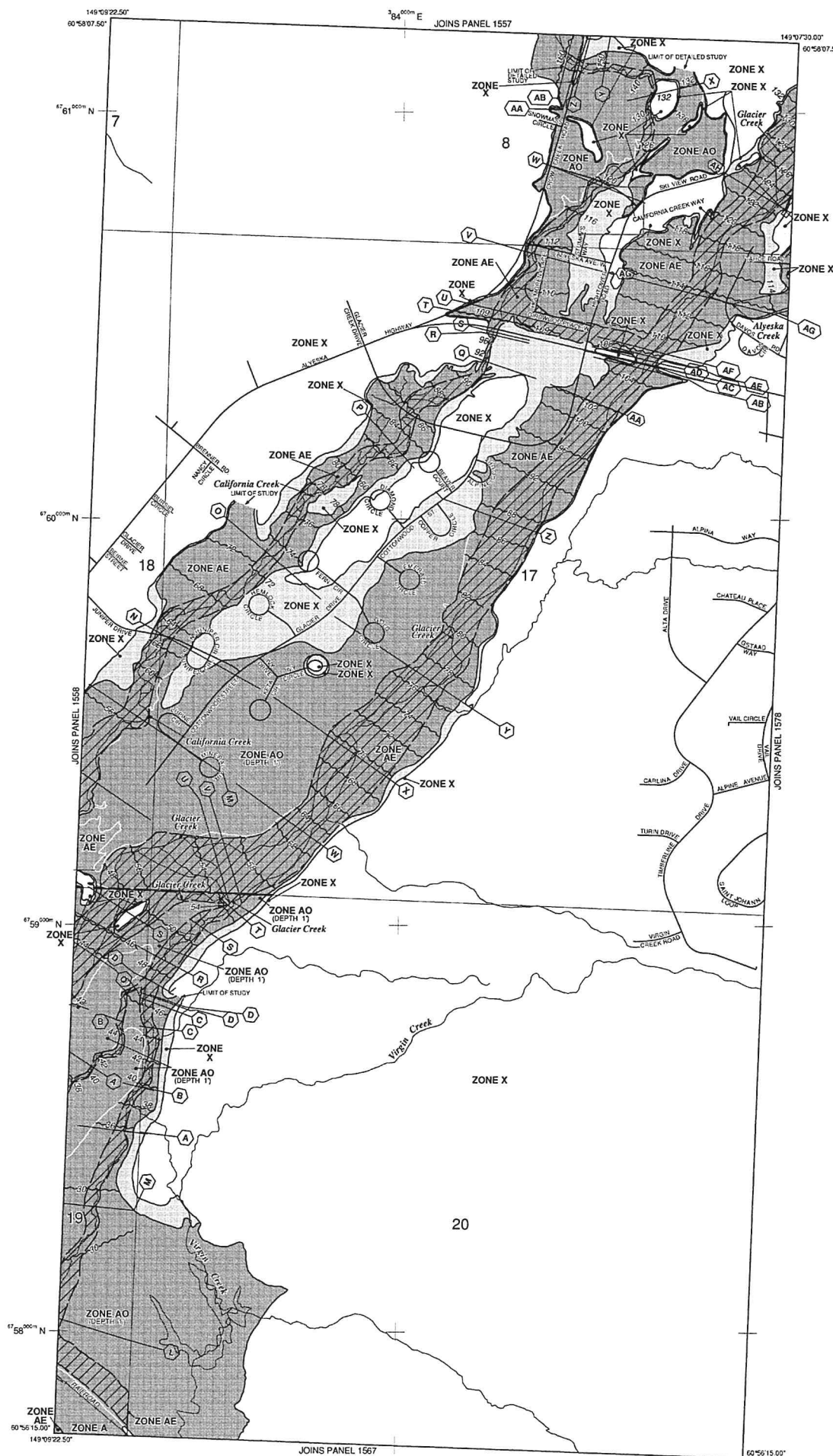
This map reflects more detailed and up-to-date stream channel configurations than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

Corporate limits shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed Map Index for an overview map showing the layout of map panels for this jurisdiction.

Contact the FEMA Map Service Center at 1-800-358-9516 for information on available products associated with this FIRM. Available products may include previously issued Letters of Map Change, a Flood Insurance Study report, and/or digital versions of this map. The FEMA Map Service Center may also be reached by Fax at 1-800-358-9620 and its website at <http://www.msc.fema.gov/>.

If you have questions about this map or questions concerning the National Flood Insurance Program in general, please call 1-877-FEMA MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov/>.



LEGEND

SPECIAL FLOOD HAZARD AREAS (SFHAs) SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual chance flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, A99, V and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

- ZONE A** No Base Flood Elevations determined.
- ZONE AE** Base Flood Elevations determined.
- ZONE AH** Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.
- ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.
- ZONE AR** Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently deteriorated. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.
- ZONE A99** Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.
- ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.
- ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

ZONE X Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood.

OTHER AREAS

ZONE X Areas determined to be outside the 0.2% annual chance floodplain.

ZONE D Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS

OTHERWISE PROTECTED AREAS (OPAs)

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

- Floodplain boundary
- Floodway boundary
- Zone D boundary
- CBRS and OPA boundary
- Boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
- Base Flood Elevation line and value; elevation in feet*
- Base Flood Elevation value where uniform within zone; elevation in feet*
- * Referenced to the Mean Sea Level (MSL) Tidal Datum
- Cross section line
- Transect line
- Geographic coordinates referenced to the North American Datum of 1983 (NAD 83)
- 1000-meter Universal Transverse Mercator grid ticks, zone 6
- 5000-foot grid ticks: New York State Plane coordinate system, east zone (FIPSZONE 3101), Transverse Mercator
- Bench mark (see explanation in Notes to Users section of this FIRM panel)
- River Mile
- MAP REPOSITORY
- 4700 South Bering Street, Anchorage, Alaska 99507 (Maps available for reference only, not for distribution.)

INITIAL NFIP MAP DATE
September 5, 1979

FLOOD HAZARD BOUNDARY MAP REVISIONS
FLOOD INSURANCE RATE MAP EFFECTIVE
September 5, 1979

FLOOD INSURANCE RATE MAP REVISIONS
September 18, 1978 - to update map format

March 5, 1990 - to change Base Flood Elevations and Special Flood Hazard Areas, reflect updated geographic information and to incorporate previously issued Letters of Map Revision.

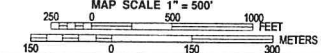
July 2, 2002 - to add Special Flood Hazard Areas and Base Flood Elevations.

September 25, 2009 - to update corporate limits, change zone designations and to incorporate previously issued Letters of Map Revision.

To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-638-6620.



MAP SCALE 1" = 500'



PANEL 1559D

FIRM

FLOOD INSURANCE RATE MAP

MUNICIPALITY OF
ANCHORAGE,
ALASKA
ANCHORAGE DIVISION COUNTY

PANEL 1559 OF 1975

(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
ANCHORAGE MUNICIPALITY	020006	1559	D

ANCHORAGE MUNICIPALITY

020006 1559 D

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.



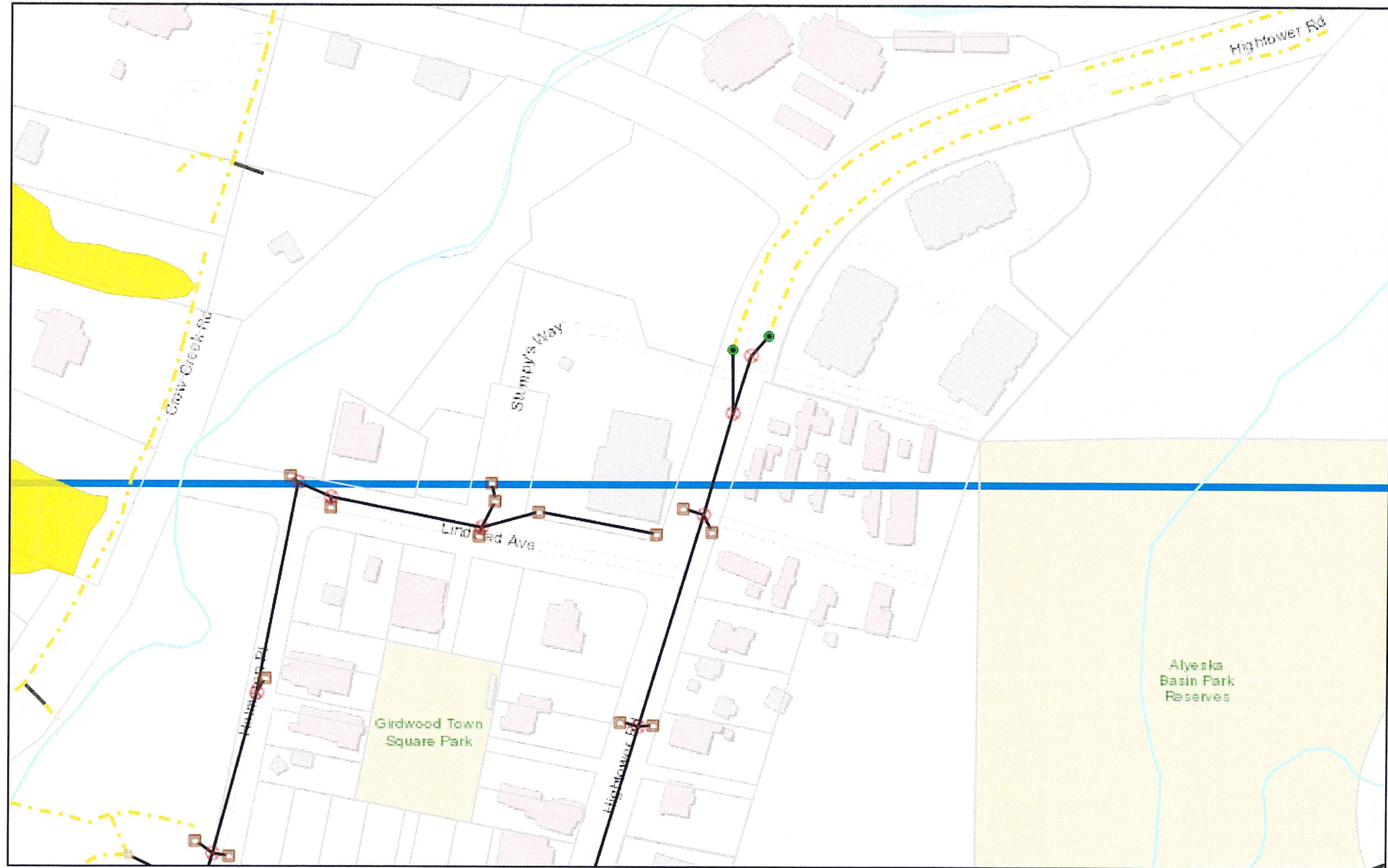
MAP NUMBER

0200051559D

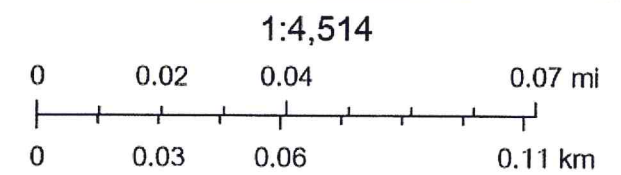
MAP REVISED

SEPTEMBER 25, 2009

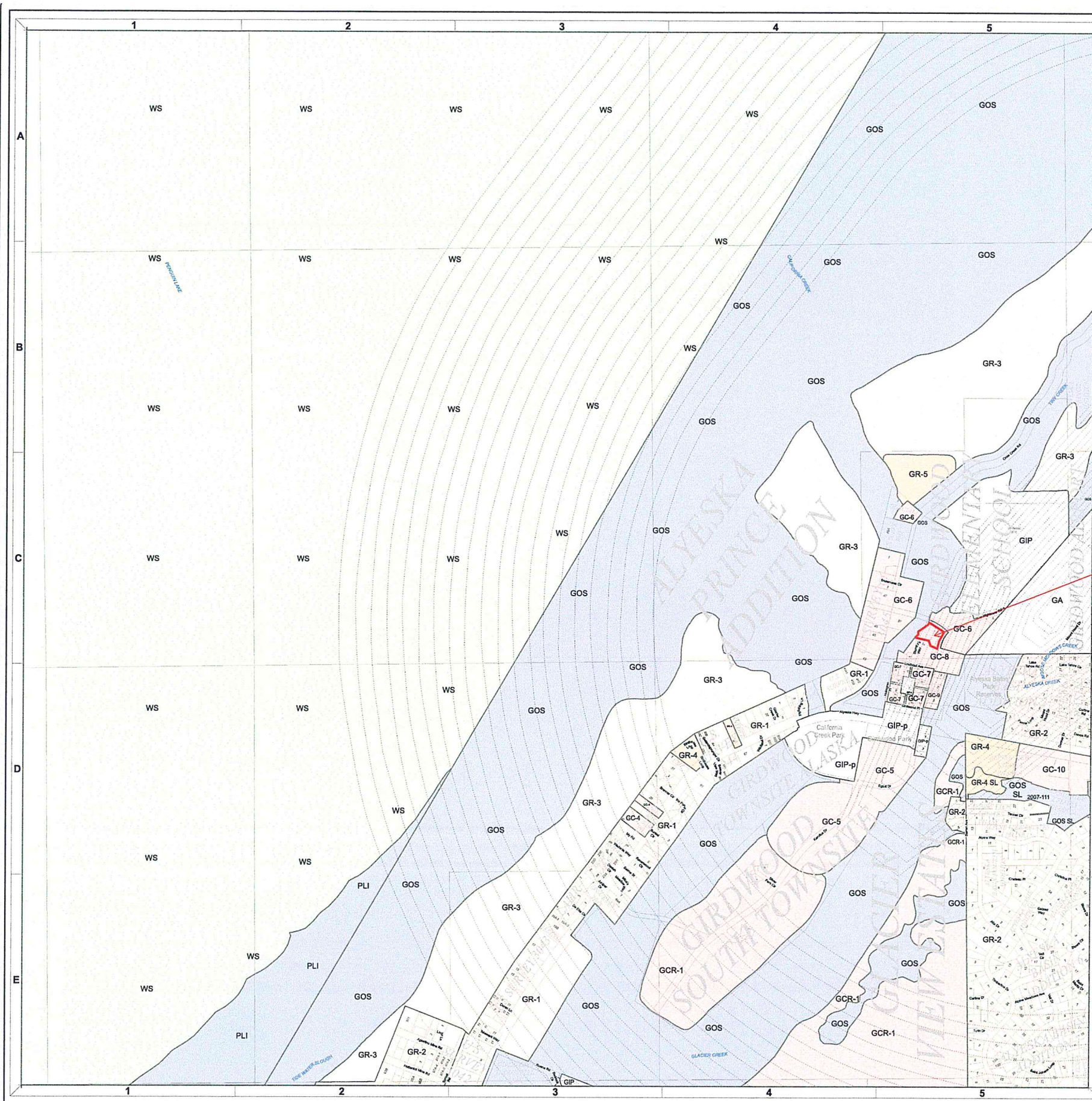
Glacier City Center Wetlands Map



December 17, 2021



MOA Watershed Management, Municipality of Anchorage, Esri, HERE.



Municipality of Anchorage
Anchorage, Alaska

Zoning

Generalized Zoning

- Rural Residential
- Single Family Residential
- Two Family Residential
- Multiple Family Residential
- Grindwood Residential
- Downtown
- Commercial
- Light Industrial
- Heavy Industrial
- Grindwood Industrial
- Resort
- Turnagain Arm
- Marine Commercial
- Marine Industrial
- Aviation
- Public Lands and Institutions
- Parks
- Planned Community
- Watershed
- Antenna Farm
- Transition
- Reserve

Glacier City Center

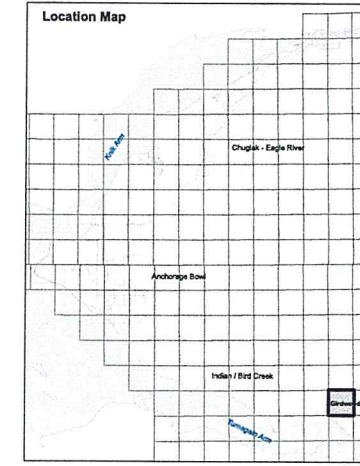
**The flood hazard areas shown on the map are to be used for planning purposes only. Flood hazard areas are mapped and regulated by the Federal Emergency Management Agency (FEMA). Copies of FEMA Flood Insurance Rate Maps (FIRMs) can be obtained by contacting the Municipal Flood Program, 907-343-8023, or visiting FEMA's Map Service Center, <http://mcs.fema.gov>.

Airport Height Ordinance Depicts
F.A.R. Part 77 (20' height contour
intervals unless otherwise noted)

SE4511	SE4512	SE4513	SE4514	SE4515
SE4611	SE4612	SE4613	SE4614	SE4615
SE4711	SE4712	SE4713	SE4714	SE4715
SE4811	SE4812	SE4813	SE4814	SE4815
SE4911	SE4912	SE4913	SE4914	SE4915

The above chart refers to grid numbers for the Municipal 1" = 100' scale map series. Use the coordinate letters and numbers bordering the map area to identify particular grid locations.

These maps are prepared by MOA's Geographic Data Information Center (GDIC) for the Planning Department using the most up-to-date base map information available to the Municipality at the time of preparation.



Zoning Legend

- R-1 Single-Family Residential

R-1A Single-Family Residential (larger lots)

R-2A Two-Family Residential (larger lots)

R-2B Two-Family Residential

R-3 Medium-Density Residential

R-4 Medium-Density Residential

R-4A Medium-Density Residential (larger lots)

R-5 Low-Density Residential

R-6 Low-Density Residential (1 acre)

R-7 Single-Family Residential (200')

R-8 Low-Density Residential (4 acres)

R-9 Low-Density Residential (2 acres)

R-10 Low-Density Residential (Agriculture)

R-11 Residential Development

B-1A Local and Neighborhood Business

B-1B Community Business

B-2A Central Business, Core

B-2B Central Business, Intermediate

B-3 Central Business, Peripheral

B-4 General Business

B-5 Rural Business

RD Residential Office

MC Marine Commercial

LI Light Industrial

H Heavy Industrial

MI Marine Industrial

A Airport

AF Antenna Farm

DR Development Reserve

PCD Planned Community Development

PLI Public Lands and Institutions

PLP Parks and Recreation

TR Transition

WS Watershed

Chugach-Eagle River Zoning Districts

CE-R-1 Single-Family Residential

CE-R-1A Single-Family Residential (larger lots)

CE-R-2A Single-Family Residential (larger lots)

CE-R-2B Single-Family Residential

CE-R-3 Medium-Density Residential

CE-R-4 Medium-Density Residential

CE-R-5 Low-Density Residential

CE-R-6 Low-Density Residential (1 acre)

CE-R-7 Single-Family Residential (200')

CE-R-8 Low-Density Residential (4 acres)

CE-R-9 Low-Density Residential (2 acres)

CE-R-10 Low-Density Residential (Agriculture)

CE-R-11 Residential Development

CE-R-12 Rural Commercial

CE-I-1 Light Industrial

CE-I-2 Heavy Industrial

CE-I-3 Rural Industrial

CE-I-4 Airport Development (Birchwood)

CE-DK Development Reserve

CE-PCD Planned Community Development

CE-PLI Public Lands and Institutions

CE-TR Transition

Grindwood Zoning Districts

GR-1 Alyeska Highway Mixed Residential

GR-2 Single-Family Residential

GR-3 Single-Family Residential (larger lots)

GR-4 Single-Family Residential (larger lots)

GR-5 Multiple-Family Residential

GR-6 Alyeska Highway Commercial

GR-7 Alyeska Highway Commercial

GR-8 Alyeska Highway Commercial

GR-9 Alyeska Highway Commercial

GR-10 Alyeska Highway Commercial

GR-11 Alyeska Highway Commercial

GR-12 Alyeska Highway Commercial

GR-13 Alyeska Highway Commercial

GR-14 Alyeska Highway Commercial

GR-15 Alyeska Highway Commercial

GR-16 Alyeska Highway Commercial

GR-17 Alyeska Highway Commercial

GR-18 Alyeska Highway Commercial

GR-19 Alyeska Highway Commercial

GR-20 Alyeska Highway Commercial

GR-21 Alyeska Highway Commercial

GR-22 Alyeska Highway Commercial

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GR-27 Alyeska Highway Commercial

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GR-34 Alyeska Highway Commercial

GR-35 Alyeska Highway Commercial

GR-36 Alyeska Highway Commercial

GR-37 Alyeska Highway Commercial

GR-38 Alyeska Highway Commercial

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GR-71 Alyeska Highway Commercial

GR-72 Alyeska Highway Commercial

GR-73 Alyeska Highway Commercial

GR-74 Alyeska Highway Commercial

GR-75 Alyeska Highway Commercial

GR-76 Alyeska Highway Commercial

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GR-80 Alyeska Highway Commercial

GR-81 Alyeska Highway Commercial

GR-82 Alyeska Highway Commercial

GR-83 Alyeska Highway Commercial

GR-84 Alyeska Highway Commercial

GR-85 Alyeska Highway Commercial

GR-86 Alyeska Highway Commercial

GR-87 Alyeska Highway Commercial

GR-88 Alyeska Highway Commercial

GR-89 Alyeska Highway Commercial

GR-90 Alyeska Highway Commercial

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GR-92 Alyeska Highway Commercial

GR-93 Alyeska Highway Commercial

GR-94 Alyeska Highway Commercial

GR-95 Alyeska Highway Commercial

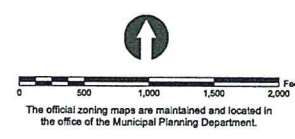
GR-96 Alyeska Highway Commercial

GR-97 Alyeska Highway Commercial

GR-98 Alyeska Highway Commercial

GR-99 Alyeska Highway Commercial

GR-100 Alyeska Highway Commercial



The official zoning maps are maintained and located in the office of the Municipal Planning Department.

Legend

- Confined Space
- Manhole
- Catchbasin Manhole
- Clean-Out
- Catch Basin
- OGS
- Lift Station
- Diverter
- Drywell
- Weir
- Blind Connect
- Top Intake Manhole
- Roof Drain
- Bypass Outlet
- Curb Inlet
- End of Pipe
- Pipe Inlet
- Pipe Cap
- Inlet
- Pipe Outlet
- Control Inlet
- Control Outlet
- Other
- Outfall
- Outfall Major
- Outfall Minor
- Sink - (Closed Drainage Basin)
- Divide
- Feature Start
- Other

Storm Pipes

ADOT

ADOT-Airport

Abandoned

Fed_Military

MOA-ASD

MOA-Facility Maintenance

MOA-Merrill Field

MOA-Other

MOA-Parks and Recreation

MOA-Port of Anchorage

MOA-M&O/CBERRRSA/LRSA/SA

Private

SOA-Alaska Railroad

Unknown

Thaw Wire

MOA-ASD

MOA-Facility Maintenance

MOA-M&O/CBERRRSA/LRSA/SA

MOA-Parks and Recreation

Private

Bridge

Constructed Channels

ADOT

MOA-M&O/CBERRRSA/LRSA/SA

MOA-Other

Port of Alaska; MOA-Port of Alaska

MOA-M&O/CBERRRSA/LRSA/SA

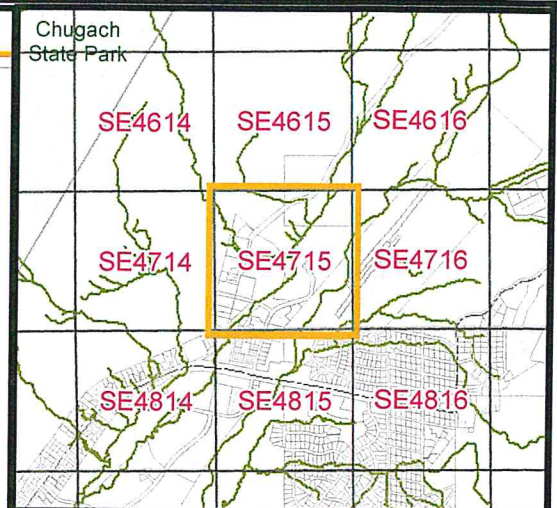
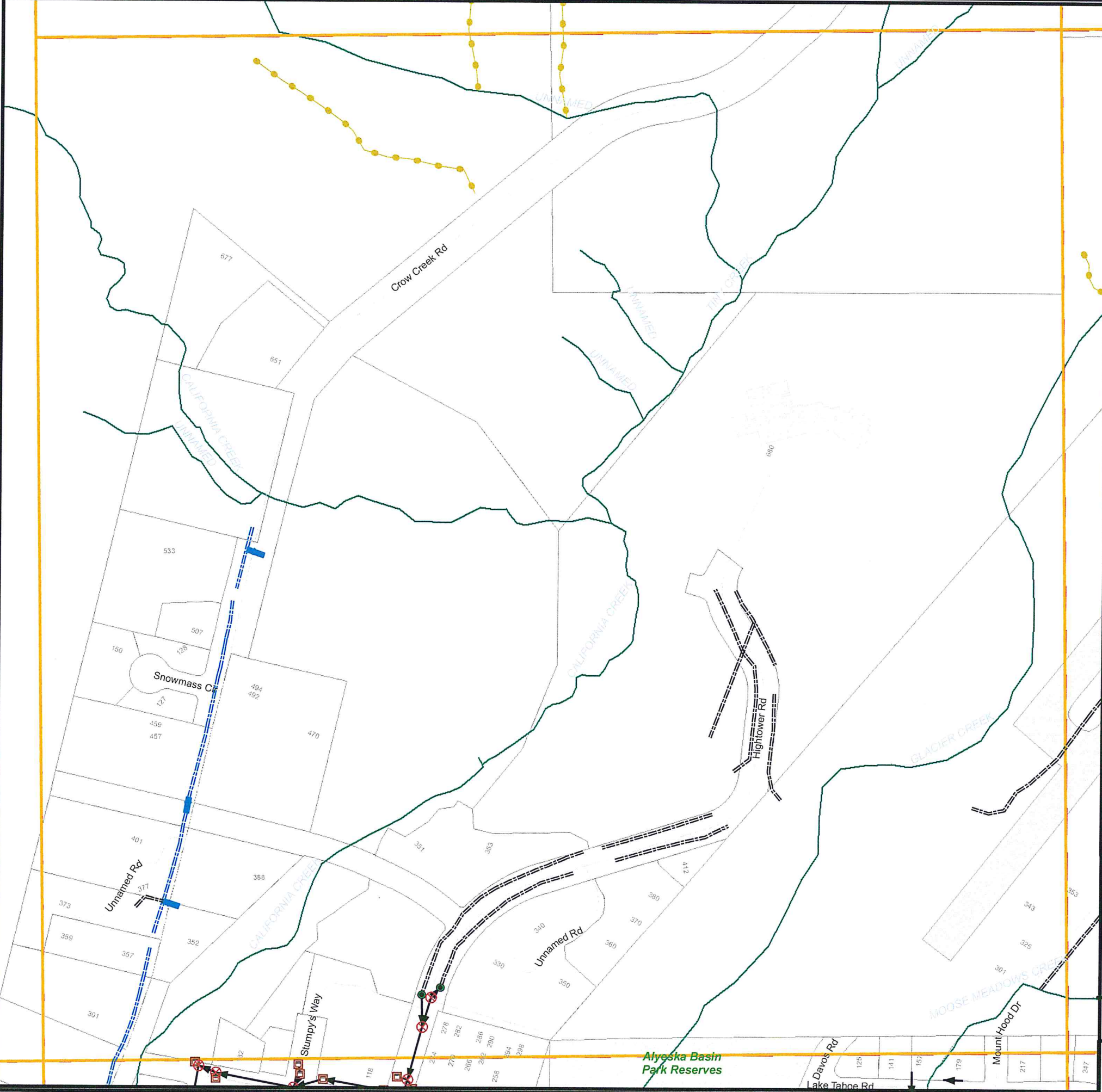
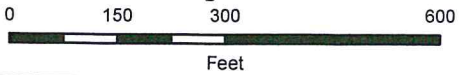
Private

SOA-Alaska Railroad

Unknown

Other Drainageways

Other Drainageways



Turnagain Arm
Legal: SE 1/4 Sec 8 T10N R2E

Notes:

INFORMATION AND DATA CONTAINED ON THIS DOCUMENT IS INTENDED FOR PLANNING PURPOSES ONLY. THE MUNICIPALITY OF ANCHORAGE ASSUMES NO LIABILITY FOR DAMAGES OCCURRING AS A RESULT OF USING THIS DOCUMENT. FOR THE LATEST AND MOST UP TO DATE INFORMATION YOU ARE URGED TO CALL THE MUNICIPALITY OF ANCHORAGE BEFORE STARTING OPERATIONS.

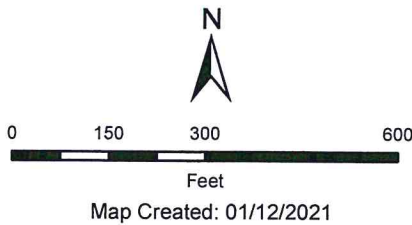
MOA
Storm Drain and
Drainage Atlas
Map Created: 10/30/2021

Grid Number
SE4715

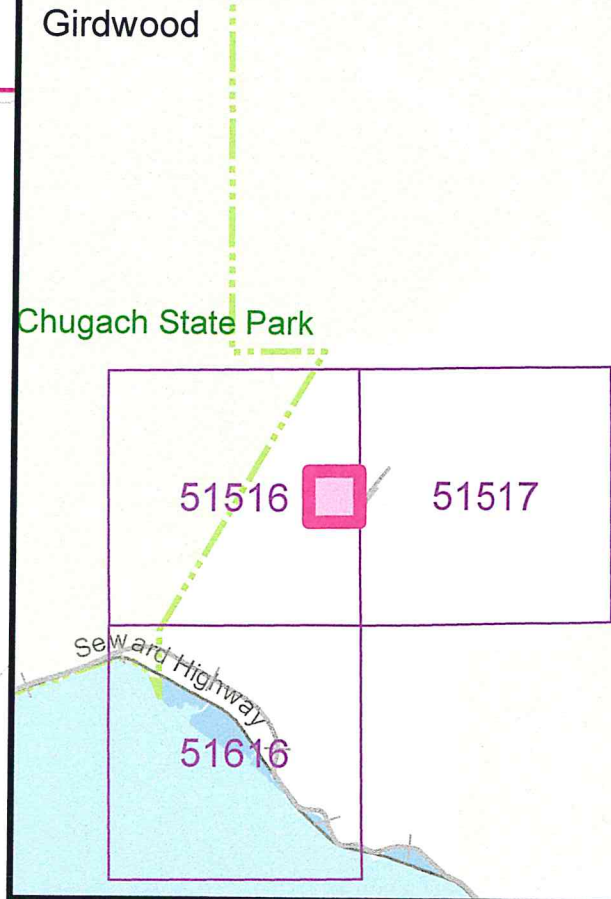
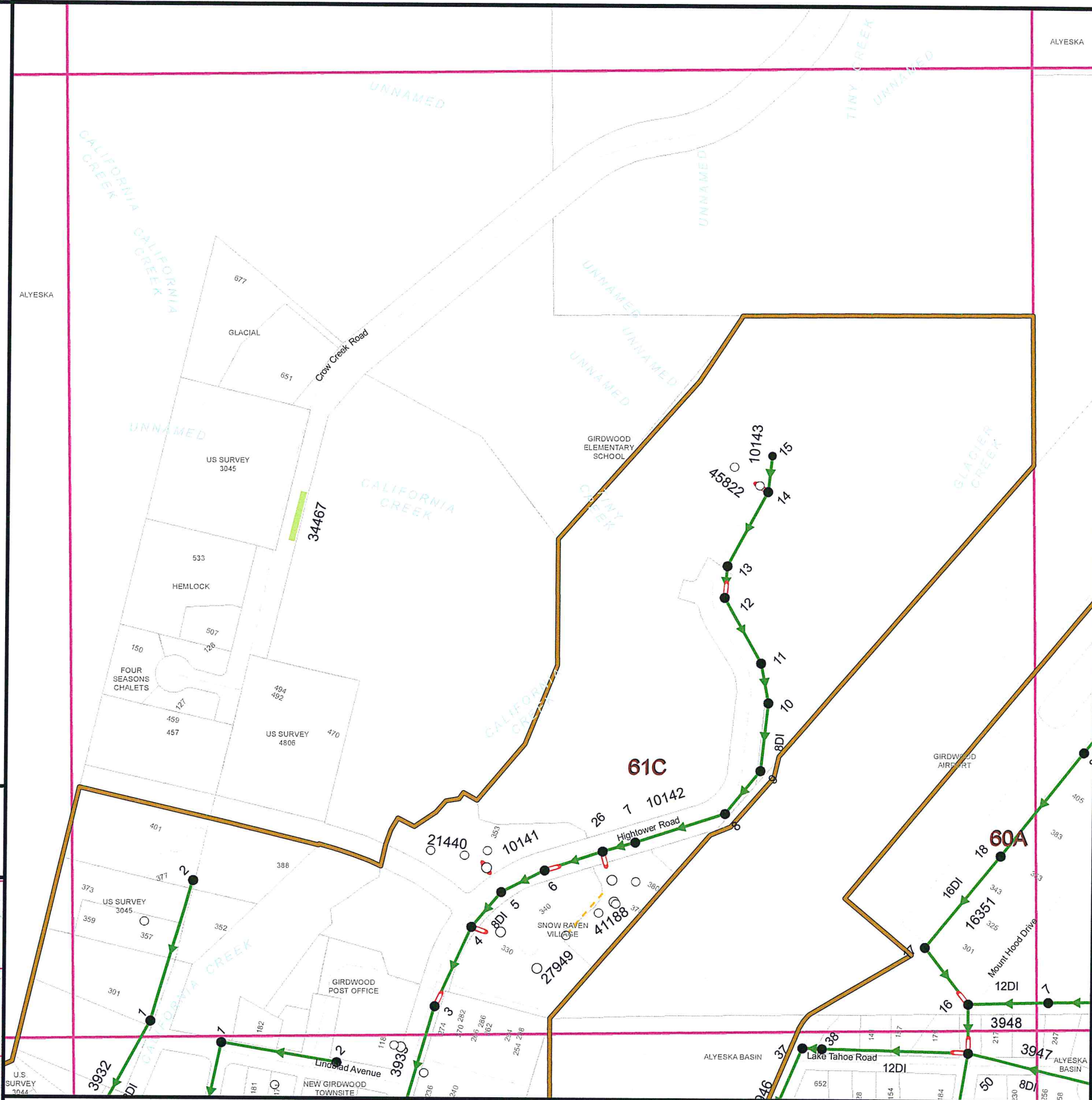
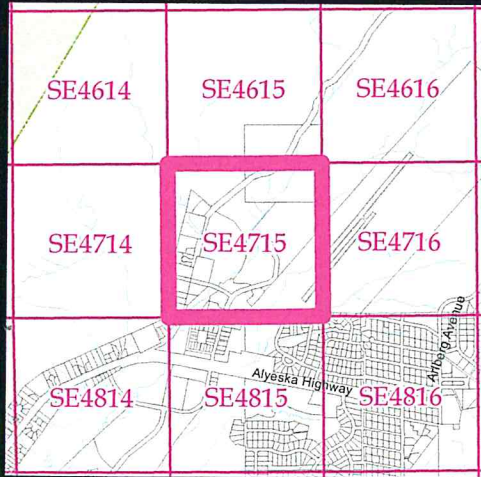


Pipe Types

ABS	Acrylonitrile-Butadiene-Styrene
AC	Asbestos Concrete
CC	Concrete Cylinder
CI	Cast Iron
CIPP	Cured In Place Pipe
CMP	Corrugated Metal
CN	Concrete
CU	Copper
DI	Ductile Iron
FC	Formed Concrete
GI	Galvanized Iron
HDPE	High Density Polyethylene
MLC	Mortar Lined Concrete
PE	Polyethylene
PVC	Polyvinylchloride
RC	Reinforced Concrete
ST	Steel
TC	Techite
UNK	Unknown
VC	Vitrified Clay
WS	Wood Stave
WST	Welded Steel
NP	No Print
*	Private System



Legal:
SE 1/4 Sec 8 T10N R2E



Legend

Constructed and Design	Administration Facility
● Cleanout, AWWU	L Lift/Pump Station
○ Cleanout, Private	M Metering Station
■ Cleanout/Manhole, AWWU	R Septage Receiving Station
★ Dangerous Manhole	WWTF Treatment Facility
⊕ Access Tee, AWWU	
● Manhole, AWWU	
○ Manhole, Private	
⊙ Discharge Point, AWWU	
⊞ Air Relief, AWWU	
⊠ Air Vacuum, AWWU	
Constructed Pipe	Design Pipe
→ Gravity Main, All Owners	○ → Gravity Main, AWWU
→ Casing and Outer Pipe, All Owners	◇ → Gravity Main, Private
→ Force Main, AWWU	→ Force Main, AWWU
→ Force Main, nonAWWU	→ Force Main, Private
Retired Pipe	
→ Abandoned in Place, All Owners	
○ Record Drawing Limit	
○ Sewer Basin	○ MOA Grid

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Wastewater Collection System

Grid Number SE4715

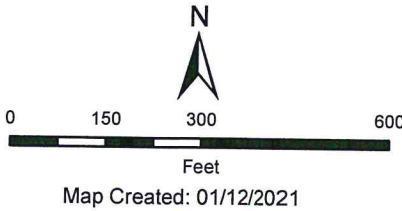
Girdwood

Municipality of Anchorage



Pipe Types

- ABS Acrylonitrile-Butadiene-Styrene
- AC Asbestos Concrete
- CC Concrete Cylinder
- CI Cast Iron
- CIPP Cured In Place Pipe
- CMP Corrugated Metal
- CN Concrete
- CU Copper
- DI Ductile Iron
- FC Formed Concrete
- GI Galvanized Iron
- HDPE High Density Polyethylene
- MLC Mortar Lined Concrete
- PE Polyethylene
- PVC Polyvinylchloride
- RC Reinforced Concrete
- ST Steel
- TC Techite
- UNK Unknown
- VC Vitrified Clay
- WS Wood Stave
- WST Welded Steel
- NP No Print
- * Private System



Legal:
SE 1/4 Sec 8 T10N R2E



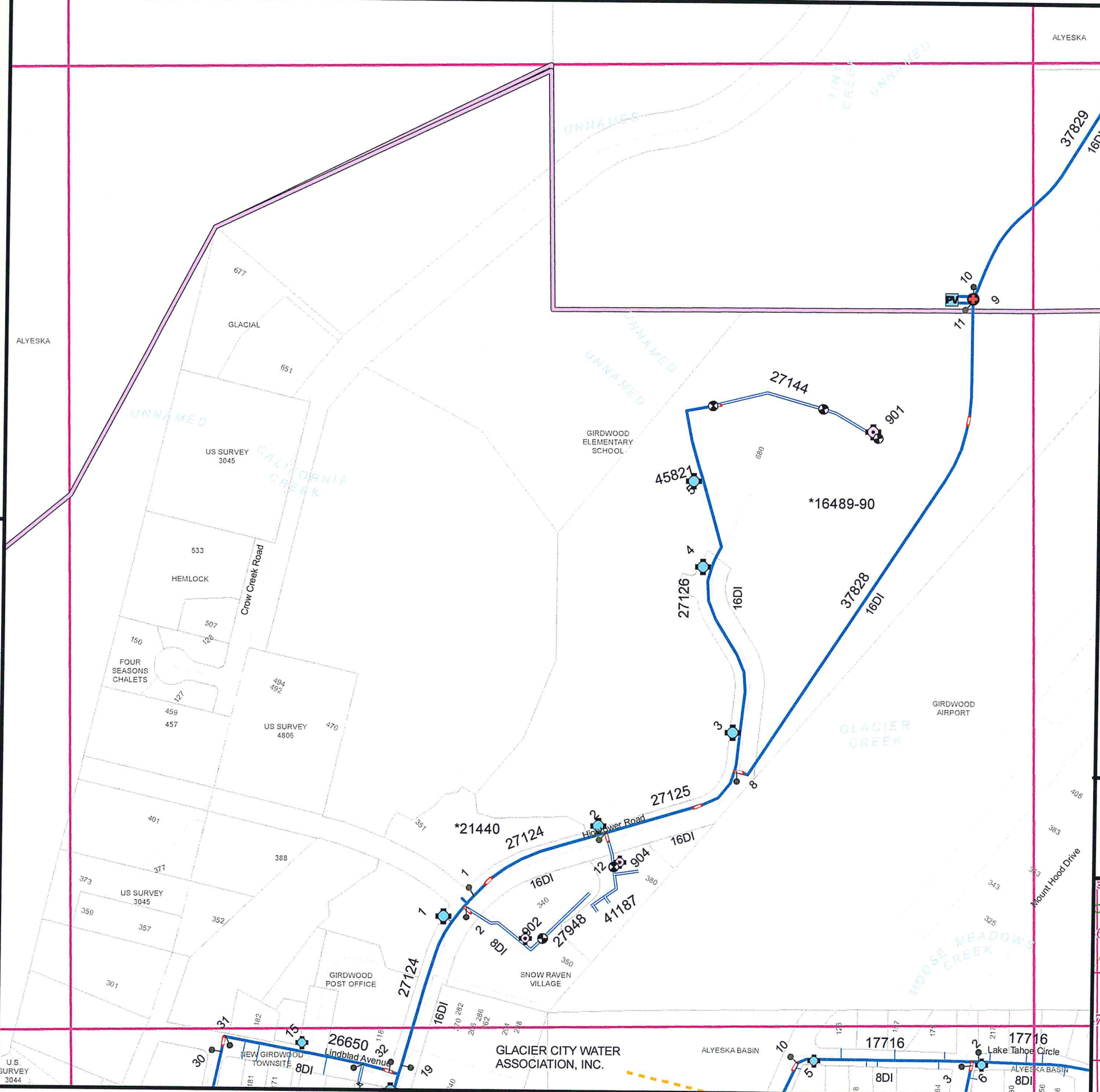
Legend

- All Status (Constructed, Design, Standby)
- Gate or Butterfly, AWWU
 - Gate or Butterfly, nonAWWU
 - All Valve Types, All Owners, Closed
 - Single, AWWU
 - Single, Private
 - Single, Unknown Owner
 - Double, AWWU
 - Double, Private
 - Flushing, AWWU
 - All Hydrant, Other Utility
 - Well, AWWU
 - Well, Private
- Construction Facilities
- Administration Facility
 - Air Vacuum Facility
 - Booster Station Facility
 - Pressure Reducing Facility
 - Reservoir Facility
 - Valve Facility
 - Well Facility
 - Water Treatment Facility
- Constructed Pipe
- Main, AWWU
 - Service Line, Private
 - Main, Other Utility
 - Casing and Outer Pipe, All Owners
- Design Pipe
- Main, AWWU
 - Service Line, Private
- Retired Pipe
- Abandoned in Place, All Owners
- Other
- Record Drawing Limit
 - Other Regulated Utility
 - Pressure Zone
 - MOA Grid

INFORMATION AND DATA CONTAINED ON THIS DOCUMENT IS NOT TO BE CONSIDERED ACCURATE AND THE MUNICIPALITY OF ANCHORAGE ASSUMES NO LIABILITY FOR DAMAGES OCCURRING AS A RESULT OF USING THIS DOCUMENT. FOR THE LATEST AND MOST UP TO DATE INFORMATION YOU ARE URGED TO CALL THE ANCHORAGE WATER & WASTEWATER UTILITY AT (907) 564-2725 BEFORE STARTING OPERATIONS.

SE4715 Grid Number

Water Distribution System



GLACIER CITY CENTER SITE DRAWINGS

Girdwood, Alaska



G I R D W O O D A L A S K A
B O X 8 4 2 9 9 5 8 7
P H 9 0 7 . 7 8 3 . 1 0 9 0
F A X 9 0 7 . 7 8 3 . 1 0 9 5
Z A R C H I T E C T S @ A L A S K A . N E T
□ □ □ □ □ □ □ □ □ □

- Civil
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Phone: 907.274.5257
www.bellalaska.com
- Landscape Architecture
Earthscape
329 F St.
Ste. 222
Anchorage AK 99501
Phone: 907.279.2688
earthscapealaska.com
- Structural
BBFM Engineers Inc.
845 K St.
Anchorage AK 99501
Phone: 907.274.2236
www.bbfm.com
- Mechanical
Spurlock & Associates, Inc.
1150 S Colony Way
Ste PMB 370
Palmer AK 99645
Phone: 907.344.8222
tom@saialaska.com
- Electrical
EIC Engineers
6927 Old Seward Hwy
Suite 200
Anchorage AK 99518
Phone: 907.349.9712
eric@eiceng.com

DRAWING INDEX

T1.1 COVER SHEET

CIVIL

C1.0 EXISTING SITE PLAN
C2.0 PROPOSED SITE PLAN
C2.0 BASE FLOOD ELEVATION DATUM

LANDSCAPE

L1.0 1"=20' SITE PLAN, REV. F

ARCHITECTURAL

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A0.2 LOCATION MAP
A0.3 VICINITY MAP
A0.4 FIRE ACCESS SITE PLAN
A0.5 COMMUNITY PARKING ACCESS PLAN
A0.6 SITE RENDERINGS
A0.7 SITE RENDERINGS

DRAWING SUBSETS

GCC BUILDING A – COFFEE & SANDWICH SHOPS, DISTILLERY
GCC TOWNHOUSES – SOUTH UNITS
GCC TOWNHOUSES – NORTH UNITS



GLACIER CITY CENTER
SITE DRAWINGS
GIRDWOOD, ALASKA

CONCEPT DESIGN

COVER SHEET

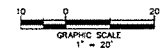
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SITE PLANS

T1.1

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FULL SIZE DRAWING SHEETS ARE 24"x36". SCALES ON
OTHER SHEET SIZES SHOULD BE ADJUSTED ACCORDINGLY



LOT 2

GIRDWOOD ELEMENTARY
SCHOOL SUBDIVISION

EXISTING FOOTPATH/TRAIL

ORDINARY HIGH WATER LINE AS
MARKED BY MOA WATERSHED
MANAGEMENT AUGUST 2021

TRACT E5

10' REAR SETBACK
20' T&E EASEMENT
(PLAT 95-35)
25' BUILDING
SETBACK EASEMENT
(PLAT 95-35)

25' CREEK SETBACK (2021)

CALIFORNIA CREEK WAY

TRACT E3
65,512 S.F.
1.504 AC.

10' SIDE SETBACK

TRACT A2

8' SECONDARY FRONT SETBACK

20' FRONT SETBACK (MAX)

16' FRONT SETBACK (MIN)

HIGHTOWER ROAD

10' T&E EASEMENT (PLAT 95-35)

15' CEA EASEMENT (BK 3848 PG 494)

40' ROW

30' ROW

40' ROW

NEW GIRDWOOD TOWNSITE
TRACT B

SNOW RAVEN VILLAGE
TRACT 1

LEGEND

- SS SANITARY SEWER MANHOLE
- SD STORM DRAIN MANHOLE
- CB STORM DRAIN CATCH BASIN
- FD FIRE HYDRANT
- ET ELECTRIC TRANSFORMER
- LP LIGHT POLE
- SIGN
- BOLLARD
- PROPERTY LINE
- ADJACENT PROPERTY LINE
- T&E EASEMENT
- BUILDING SETBACK
- ORDINARY HIGH WATER (OHW)
- SETBACK FROM OHW
- UNDERGROUND ELECTRIC LINE
- UNDERGROUND GAS LINE
- SEWER LINE
- WATER LINE
- STORM DRAIN LINE
- DITCHLINE
- VEGETATION/TREE LINE
- PAVEMENT
- GRAVEL

CURVE TABLE						
CURVE #	LENGTH	RADIUS	DELTA	CHORD	CHORD BEARING	TANGENT
C1	19.73'	955.95'	110°58'	19.73'	N65°22'02"W	9.87'
C2	170.49'	621.54'	15°42'59"	169.95'	N56°49'24"W	85.78'
C3	29.66'	20.00'	84°57'51"	27.01'	N6°11'35"W	18.32'
C4	159.79'	460.00'	19°54'09"	158.99'	S26°38'31"W	80.71'



GLACIER CITY CENTER
SITE DRAWINGS
GIRDWOOD, ALASKA

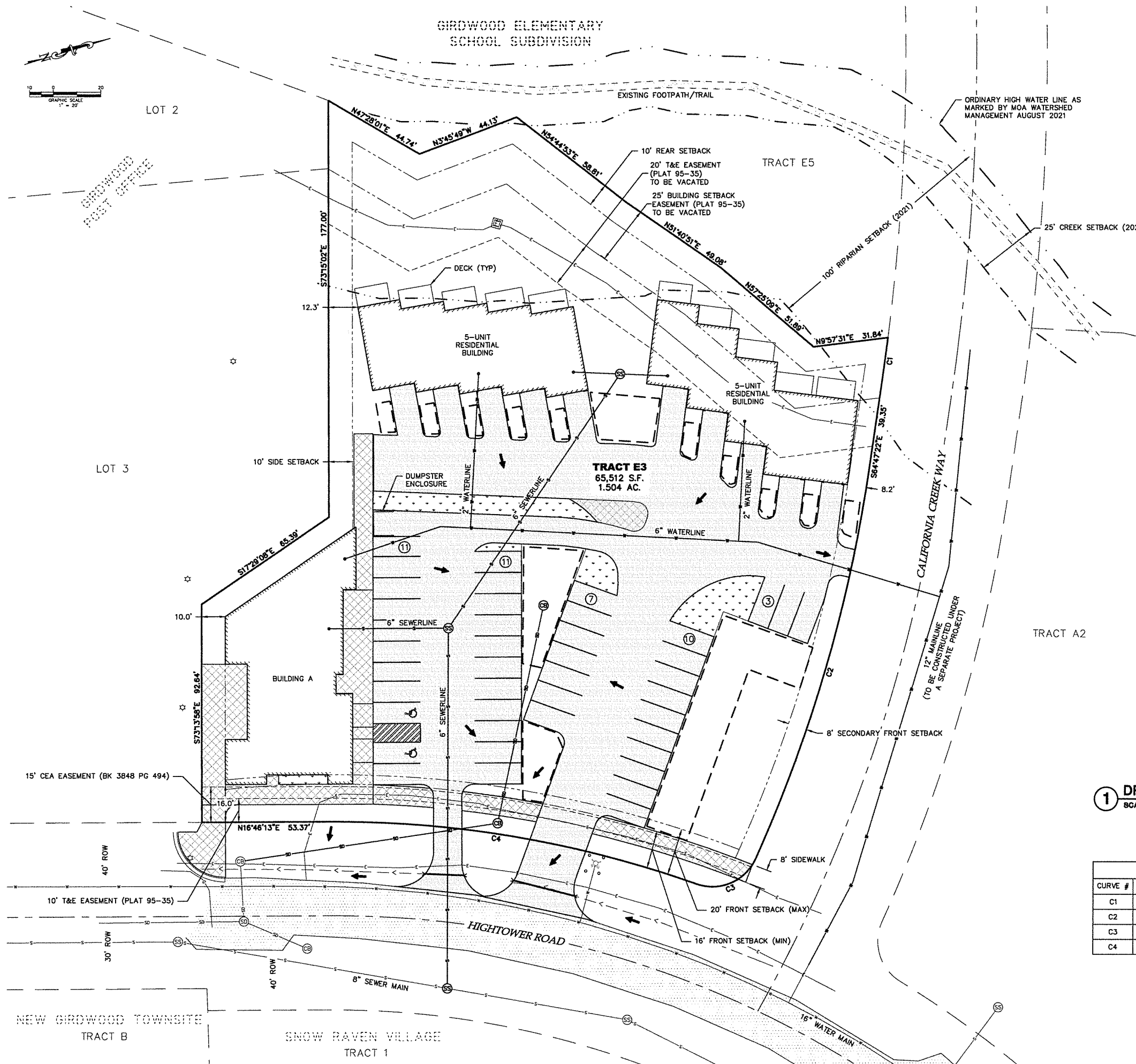
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EXISTING
SITE PLAN

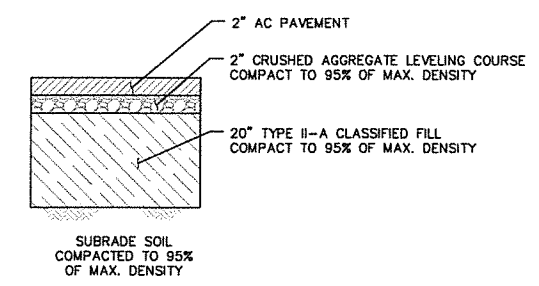
C1.0

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LEGEND

- | EXISTING | PROPOSED | |
|----------|----------|---------------------------|
| SS | SS | SANITARY SEWER MANHOLE |
| SD | SD | STORM DRAIN MANHOLE |
| CB | CB | STORM DRAIN CATCH BASIN |
| ⊕ | | FIRE HYDRANT |
| ⊞ | | ELECTRIC TRANSFORMER |
| ☆ | | LIGHT POLE |
| • | | BOLLARD |
| --- | --- | PROPERTY LINE |
| --- | --- | ADJACENT PROPERTY LINE |
| --- | --- | T&E EASEMENT |
| --- | --- | BUILDING SETBACK |
| --- | --- | ORDINARY HIGH WATER (OHW) |
| --- | --- | SETBACK FROM OHW |
| --- | --- | UNDERGROUND ELECTRIC LINE |
| --- | --- | UNDERGROUND GAS LINE |
| --- | --- | SEWER LINE |
| --- | --- | WATER LINE |
| --- | --- | STORM DRAIN LINE |
| --- | --- | DITCHLINE |
| --- | --- | CULVERT |
| --- | --- | FLOW DIRECTION |
| --- | --- | PAVEMENT |
| --- | --- | GRAVEL |
| --- | --- | CONCRETE |
| --- | --- | LANDSCAPING |
| --- | --- | SNOW STORAGE |



1 DRIVEWAY/PAVEMENT SECTION
SCALE: NTS

CURVE TABLE						
CURVE #	LENGTH	RADIUS	DELTA	CHORD	CHORD BEARING	TANGENT
C1	19.73'	955.95'	1°10'58"	19.73'	N65°22'02"W	9.87'
C2	170.49'	621.54'	15°42'59"	169.95'	N56°49'24"W	85.78'
C3	29.66'	20.00'	84°57'51"	27.01'	N6°11'35"W	18.32'
C4	159.79'	460.00'	19°54'09"	158.99'	S26°38'31"W	80.71'

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ANCHORAGE, AK 99503
(907) 274-5257
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BIRDWOOD, ALASKA

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SITE PLAN

C2.0



LEGEND

- BASE FLOOD ELEVATION (MOA FLOODPLAIN MAP)
- BASE FLOOD ELEVATION (INTERPOLATED)



LANDSCAPING REQUIREMENTS (Title 21)

Zoning: GC8

Adjacent zoning:



21.09 Girdwood

21.09.070 E Landscaping, Vegetation, and Tree Retention

4. Minimum vegetation coverage by use (Table 21.09-10)

Natural Vegetation 20% Coverage
Total Permeable Surface 40%
See Zoning Information, Sheet A0.1

6. Setback Area Vegetation

a. Frontage Roads - Area along Hightower Road to be planted as shown with mix of deciduous and evergreen trees with continuous planting bed of natural plantings. Over 50% are evergreen, plant spacing of trees is at average interval of 15' o.c.
c. Buffering Non-Similar Uses
Planting beds are used to separate commercial from residential uses. The Woonerf concept enhances and defines the residential space.

7. Tree Protection During Construction

b. Sturdy fencing will be used to protect existing vegetation to be saved. Fencing to be shown on Site Demo or Grading sheets.

8. Revegetation of Disturbed Areas
All disturbed areas will be revegetated using native or non-invasive species.

21.07.080 Landscaping, Screening, and Fences

GCB adjacent to GCB does not require landscaping.

Arterial Landscaping

L1 Visual Enhancement Landscaping required along Hightower, and California Creek Way, see above.

21.07.080 E2. Parking Lot Landscaping

Parking Lot Perimeter Landscaping - Parking lots with over 10 spaces. Requires landscaping standard L1 Visual Enhancement where the parking lot abuts the property line.

Parking Lot Interior Landscaping - Parking lots with over 40 spaces requires 5% area to be landscaped with 1 tree+6 shrubs/150sf; min bed size 165sf.

21.07.110.C.10.b. Residential Design Standards

Front Driveway Separations

Create planting area from facade or front entry landing to driveway or street, min. 4' wide x 8' long, planted with 1 tree and 5 shrubs/unit where distance to street is 20' (less than 8' distance use 1 tree/5 shrubs/2 units)

L1 Visual Enhancement Landscaping

Applicable Standards:

Min average planting bed = 8' (min width 5', max 50' run)
1 tree + 6 shrubs/20 lf

Natural vegetation:

Average planting interval 15' o.c.; max. 50% deciduous; 3 shrubs/tree

Landscape Summary

North Property Line at r.o.w = 214 l.f.
Requires 15 trees and 43 shrubs
Shown: 15 trees, 43 shrubs

East Property Line = 174 l.f. along arterial.
Requires 12 trees, 35 shrubs
Shown: 12 trees, +100 shrubs

South Property Line:
Landscaping not required

West Property Line

Natural vegetation being saved. Existing vegetation includes spruce, cottonwood, alder, willow and mix of groundcovers and shrubs that exceeds minimum requirements.

Parking Lot Interior Landscaping - Parking lot interior = 16,680 sf paved area = 834 sf landscape beds. Requires 6 trees, 34 shrubs
Shown: 8 trees, +100 shrubs (includes perennials = 3/shrub)

Front Driveway Separations

Planting area from front entry landing to driveway. Requires 1 tree and 5 shrubs/unit

Shown: 5 perennials/unit



Do not disturb native vegetation

Snow storage areas, lawn mix or no mow grass mix

RAP with granite mix at driveways and 8' walkway

Native Groundcovers (12" - 24" tall):

Iris
Nuuk
Labrador tea
Meadow Rue
Grasses

Asphalt

Evergreen trees:
Sitka Spruce and Hemlock

Gabion basket curb and retention basin

Native meadow mix :

Yarrow
Iris
Rye
Calamagrostis

Native Shrubs (5' - 15' + tall):

Alder
Willow
Rose

OVERALL LANDSCAPE PLAN

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NOT FOR CONSTRUCTION

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LANDSCAPE DRAWINGS

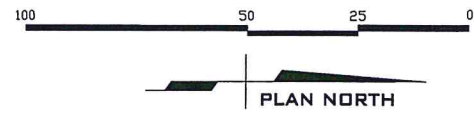
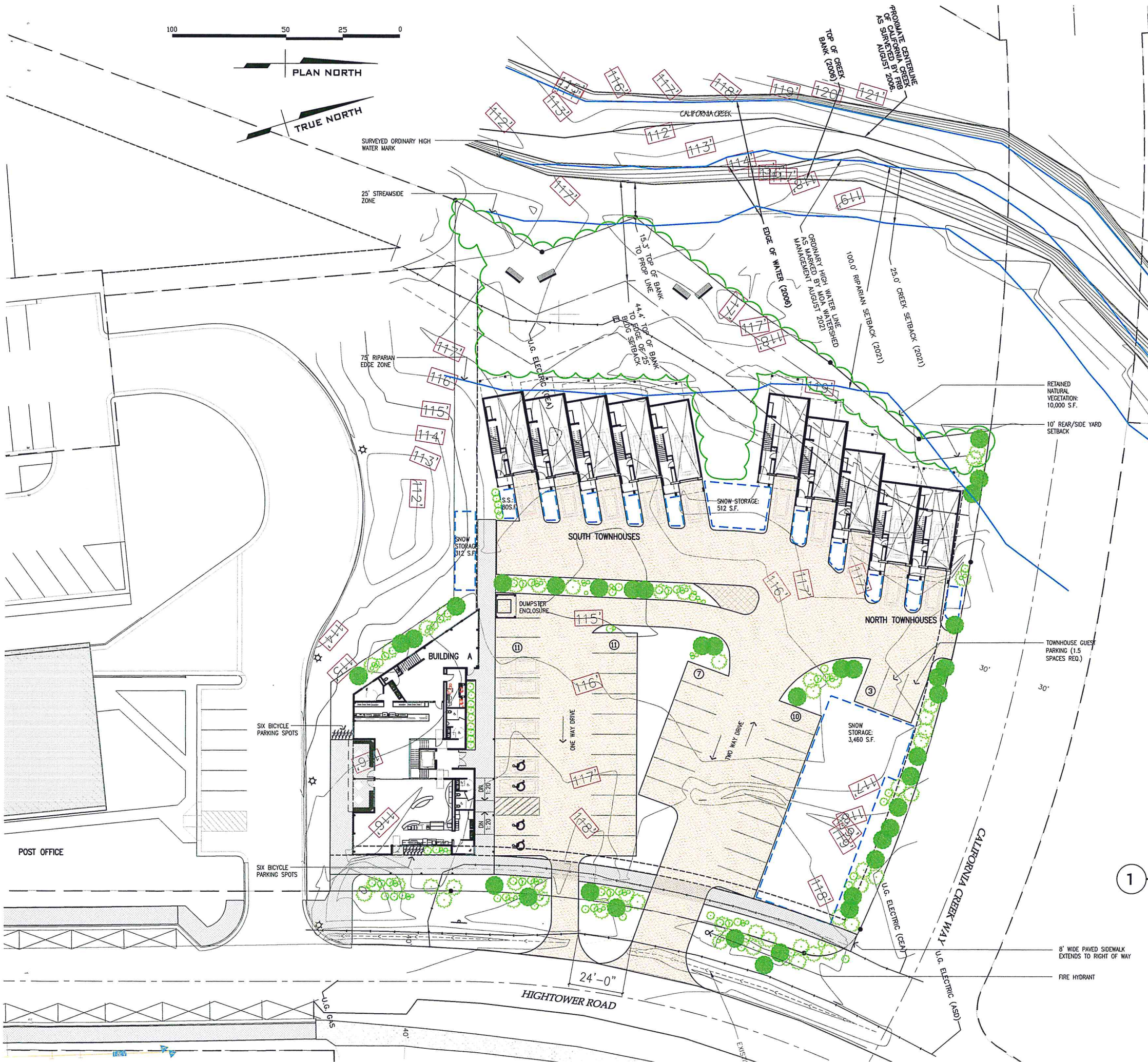
CONCEPT DESIGN

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LANDSCAPE PLAN

L1.0



ZONING ANALYSIS SUMMARY	
GIRDWOOD ZONING DISTRICT:	gC-8
PARKING REQUIRED PER TABLE 21.07-4: SEE GLACIER CITY CENTER PARKING ANALYSIS:	
PARKING ANALYSIS SYNOPSIS:	
PARKING REQUIRED FOR BUILDING A:	79 PARKING SPACES
PARKING REQUIRED FOR TOWNHOUSES:	17 PARKING SPACES
TOTAL ON SITE PARKING REQUIRED (NOT ACCOUNTING FOR POTENTIAL INTERNAL SHARED USE AGREEMENT OR ALLOWED PARKING REDUCTIONS)	96 PARKING SPACES
PARKING PROVIDED:	
COMMERCIAL - ON SITE:	40 PARKING SPACES
RESIDENTIAL - ON SITE:	22 PARKING SPACES
COMMUNITY PARKING LOCATED OFF SITE PER AMC 21.09.070L2:	34 PARKING SPACES
TOTAL PARKING PROVIDED	96 PARKING SPACES
TOTAL COMMUNITY PARKING WITHIN 600 FEET WALKING DISTANCE: 70 PARKING SPACES	
FRONT SETBACK PER TABLE 21.09.060-6: 16 FEET MIN, 20 FEET MAXIMUM	
SIDE SETBACK PER TABLE 21.09.060-6: 10 FEET	
REAR SETBACK PER TABLE 21.09.060-6: 10 FEET	
LOT SIZE:	65,512 S.F.
MAXIMUM LOT COVERAGE BY BUILDING PER TABLE 21.09.060-6	60%
ACTUAL LOT COVERAGE BY BUILDING IS 23,777 S.F.	35%
MAXIMUM FLOOR AREA RATIO:	N/A
ACTUAL FLOOR AREA RATIO:	N/A
MAXIMUM DWELLING UNITS PER ACRE:	20
ACTUAL DWELLING UNITS PER ACRE:	7
REQUIRED NATURAL VEGETATION AREA RETAINED PER TABLE 21.09.070-10 (10% OF LOT SIZE): 6,551 S.F.	
NATURAL VEGETATION AREA RETAINED	10,000 S.F.+
REQUIRED PERMEABLE SURFACE AREA PER TABLE 21.09.070-10 (20% OF LOT SIZE): 13,102 S.F.	
PERMEABLE SURFACE AREA PROVIDED:	LOT SIZE: 65,512 S.F. BUILDING FOOT PRINT: -10,765 S.F. PAVED DRIVEWAYS AND PATHS: -23,863 S.F. TOTAL PERMEABLE SURFACE PROVIDED: 30,884 S.F.
REQUIRED SNOW STORAGE PER 21.09.070.K.1.b: 20% OF PLOWED AREA	
PLOWED DRIVEWAY, RAMP AND WALKWAY AREA:	23,863 S.F.
REQUIRED SNOW STORAGE AREA:	4,773 S.F.
SNOW STORAGE AREA PROVIDED:	5,084 S.F.

1 SITE PLAN
SCALE: 1" = 20'

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SITE DRAWINGS
GIRDWOOD, ALASKA

CONCEPT DESIGN

1"=20' SITE PLAN

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1

LOCATION MAP
SCALE: NO SCALE



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GIRDWOOD, ALASKA
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LOCATION MAP

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100 50 25 0

PLAN NORTH

TRUE NORTH

SURVEYED ORDINARY HIGH WATER MARK

25' STREAMSIDE ZONE

75' RIPARIAN EDGE ZONE

100' HOSE REACH

BUILDING A

SOUTH TOWNHOUSES

NORTH TOWNHOUSES

10' REAR/SIDE YARD SETBACK

FIRE APPARATUS LANE

FIRE HYDRANT

POST OFFICE

40'

HIGHTOWER ROAD

CALIFORNIA CREEK WAY

1 FIRE ACCESS SITE PLAN
SCALE: 1" = 20'



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FIRE ACCESS SITE PLAN

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1 FROM EAST
SCALE: NO SCALE

2 FROM SOUTH-WEST
SCALE: NO SCALE



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1 FROM NORTH
SCALE: NO SCALE

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GLACIER CITY CENTER COFFEE SHOP AND DISTILLERY

Girdwood, Alaska



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PH 907.783.1090
FAX 907.783.1095
ZARCHITECTS@ALASKA.NET
□ □ □ □ □ □ □ □ □ □

UNIT AREA (SQUARE FEET)

MEZZANINE	685
SECOND FLOOR	3,916
FIRST FLOOR	4,285
TOTAL GROSS AREA	8,886

FULL SIZE DRAWING SHEETS ARE 24"x36". SCALES ON
OTHER SHEET SIZES SHOULD BE ADJUSTED ACCORDINGLY

DRAWING INDEX

Ta1.1 COVER SHEET
Ra1.1 RENDERINGS

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Aa1.2 SECOND FLOOR PLAN
Aa1.3 MEZZANINE PLAN
Aa1.5 SECOND FLOOR REFLECTED CEILING PLAN
Aa2.1 ELEVATIONS
Aa2.2 ELEVATIONS
Aa3.1 SECTIONS
Aa3.2 SECTIONS

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BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY
GIRDWOOD, ALASKA
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COVER SHEET

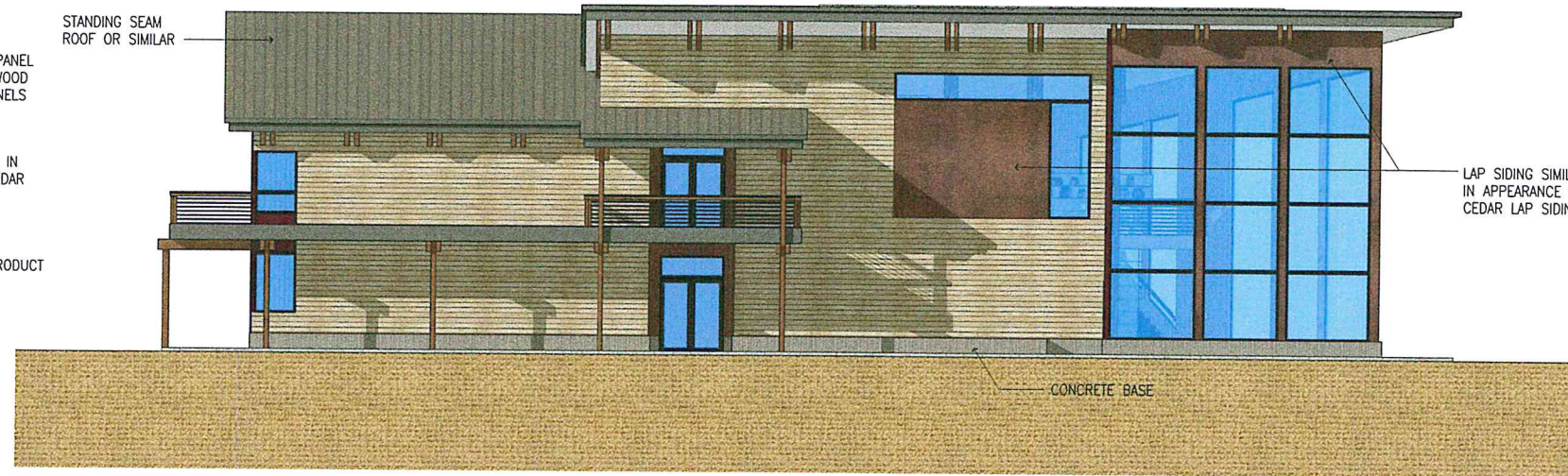
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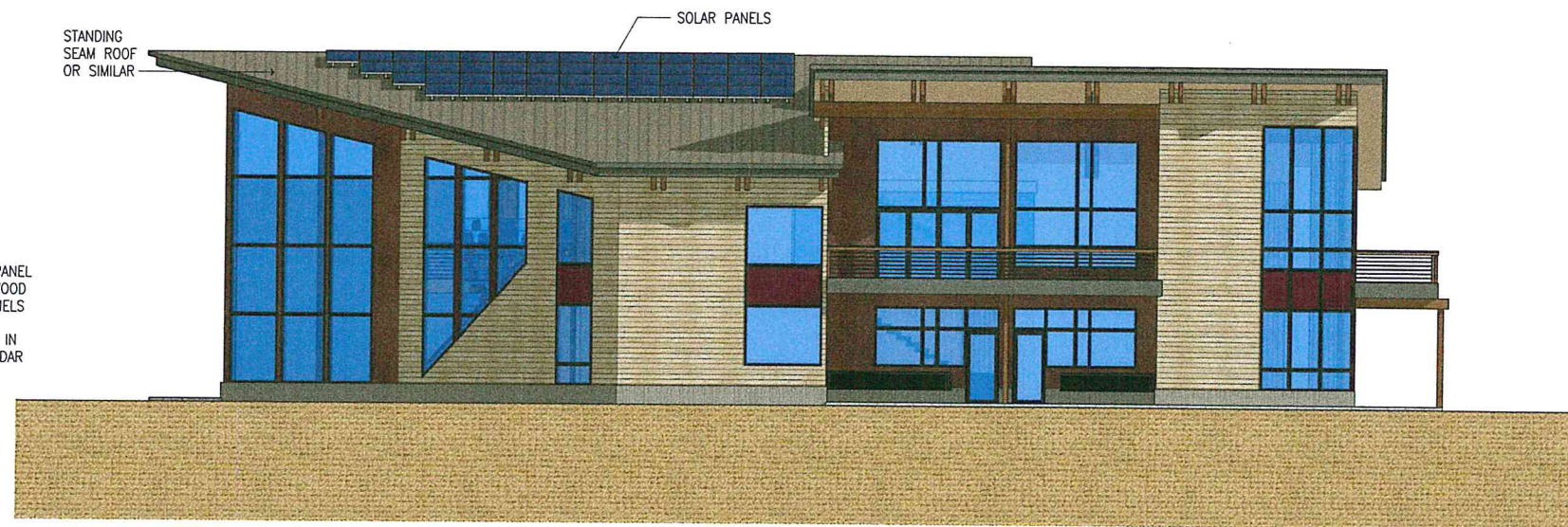
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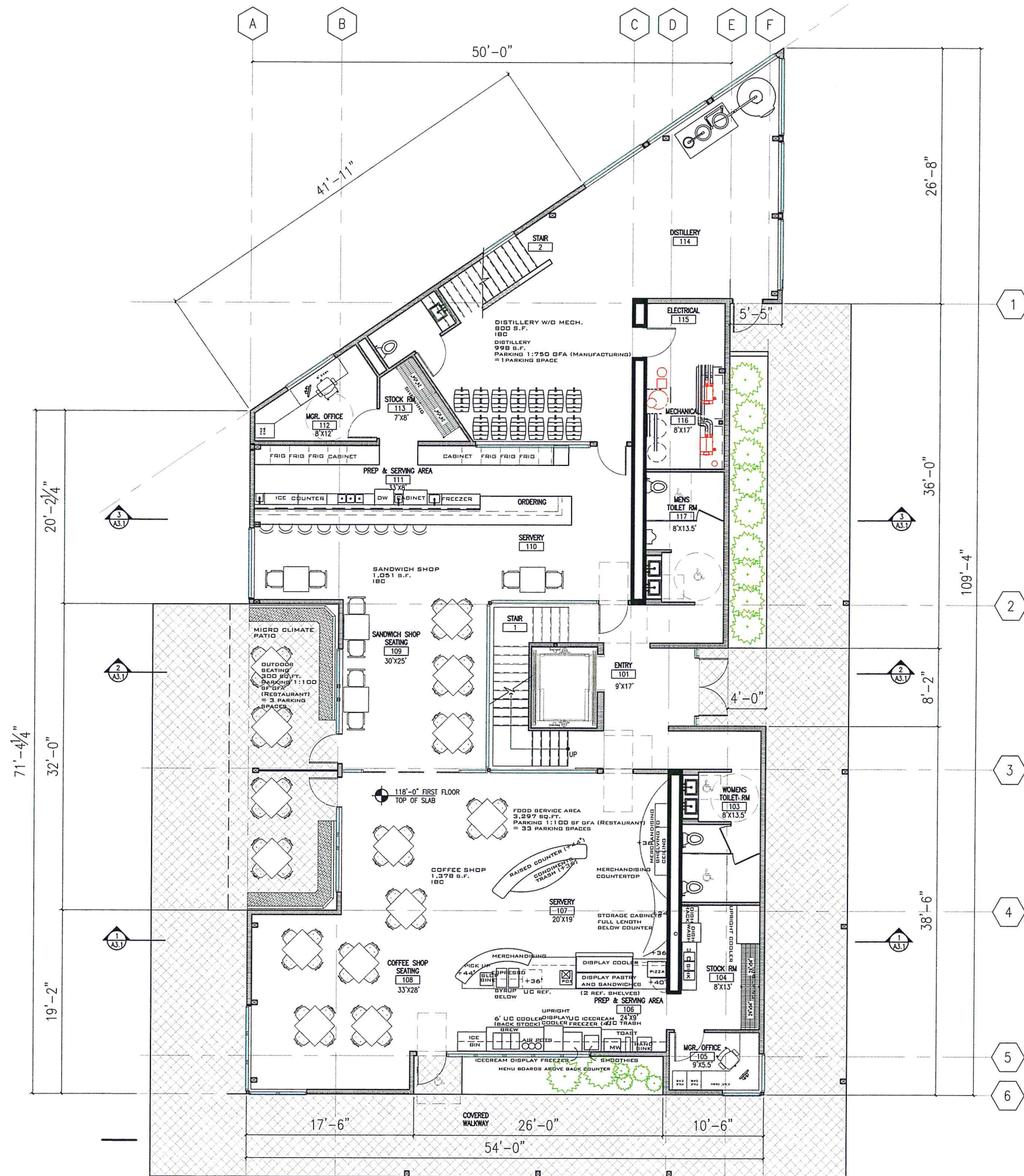
NORTH




WEST



SOUTH



1 FIRST FLOOR PLAN
SCALE: 3/16" = 1'-0"



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BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY**
GIRDWOOD, ALASKA

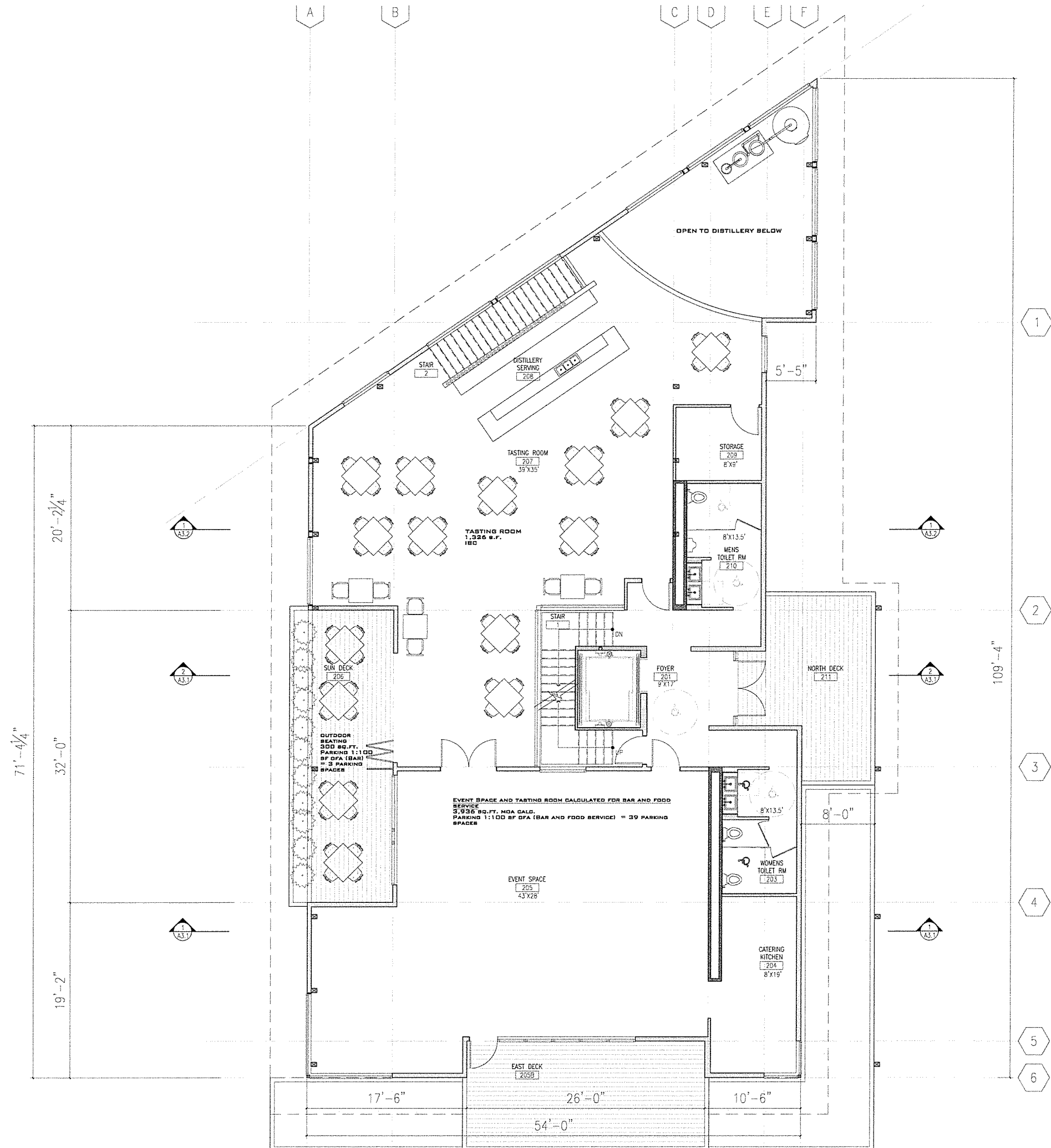
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1 SECOND FLOOR PLAN
SCALE: 3/16" = 1'-0"

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GLACIER CITY CENTER
BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY
BIRDWOOD, ALASKA
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SECOND FLOOR PLAN

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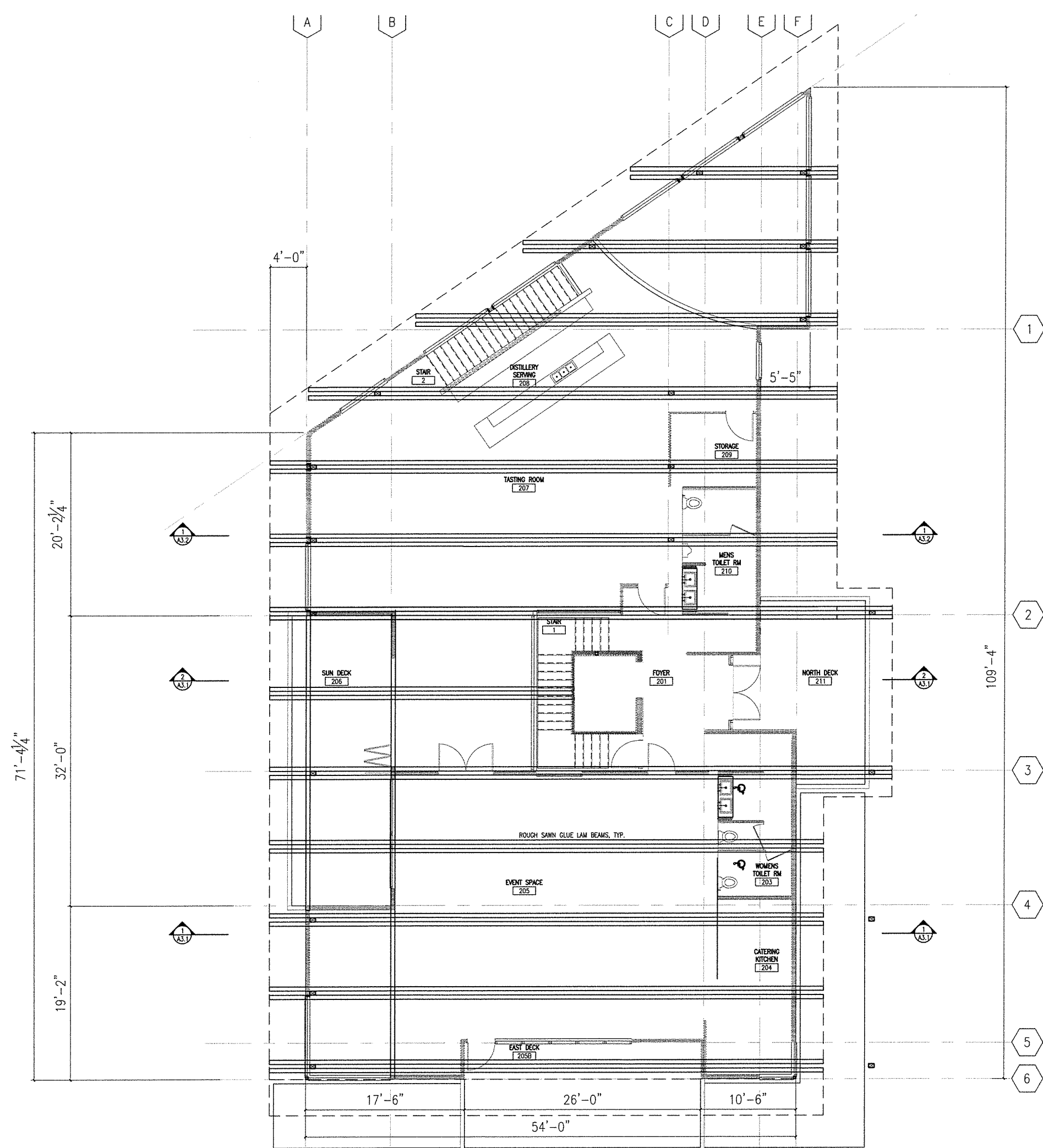
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BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY**
GIRDWOOD, ALASKA
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MECHANICAL MEZZANINE PLAN

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1 SECOND FLOOR RC PLAN
SCALE: 3/16" = 1'-0"

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GLACIER CITY CENTER
BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY
BIRDWOOD, ALASKA
CONCEPT DESIGN
SECOND FLOOR REFLECTED CEILING PLAN

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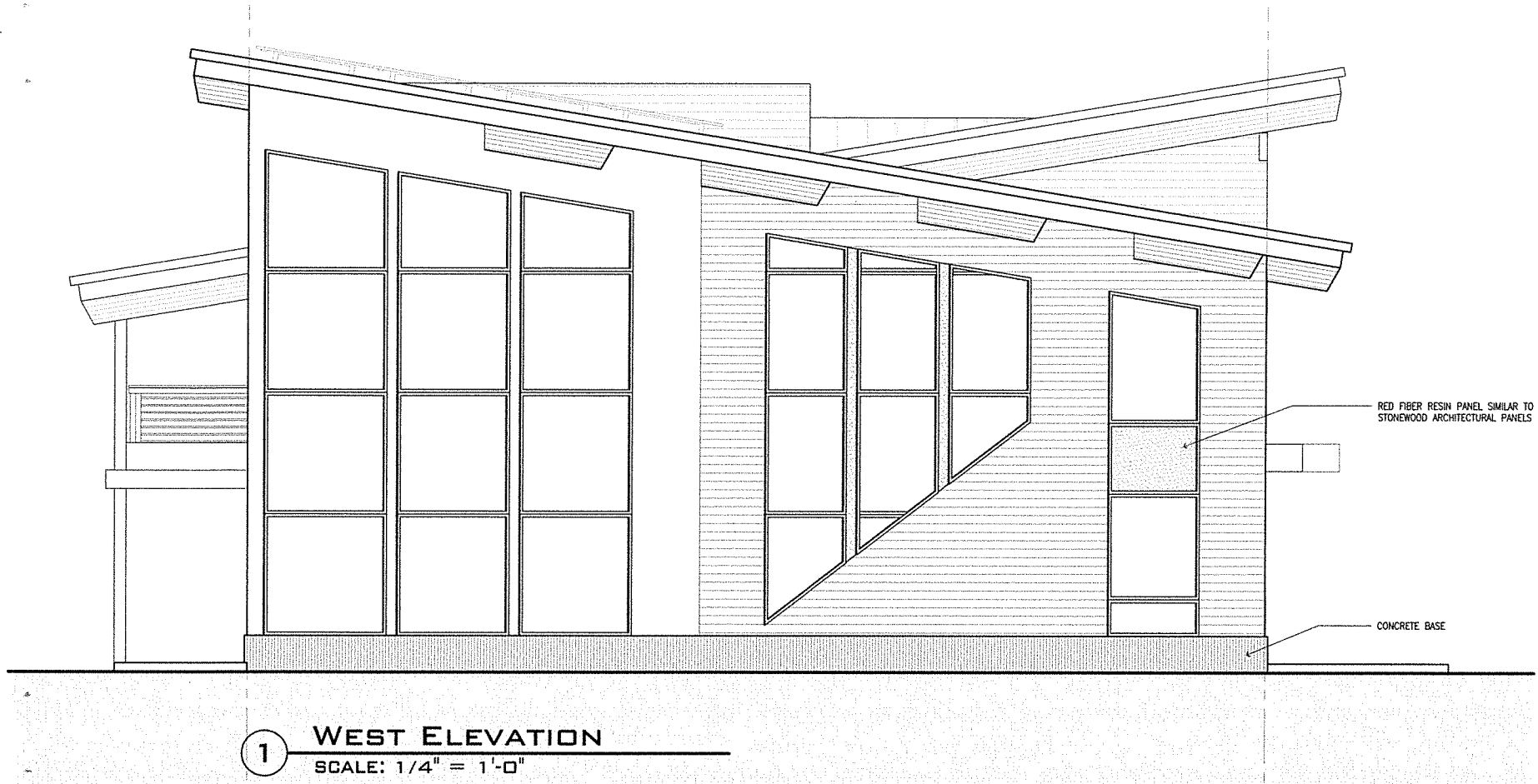
3 DISTILLERY ROOM
SCALE: NTS



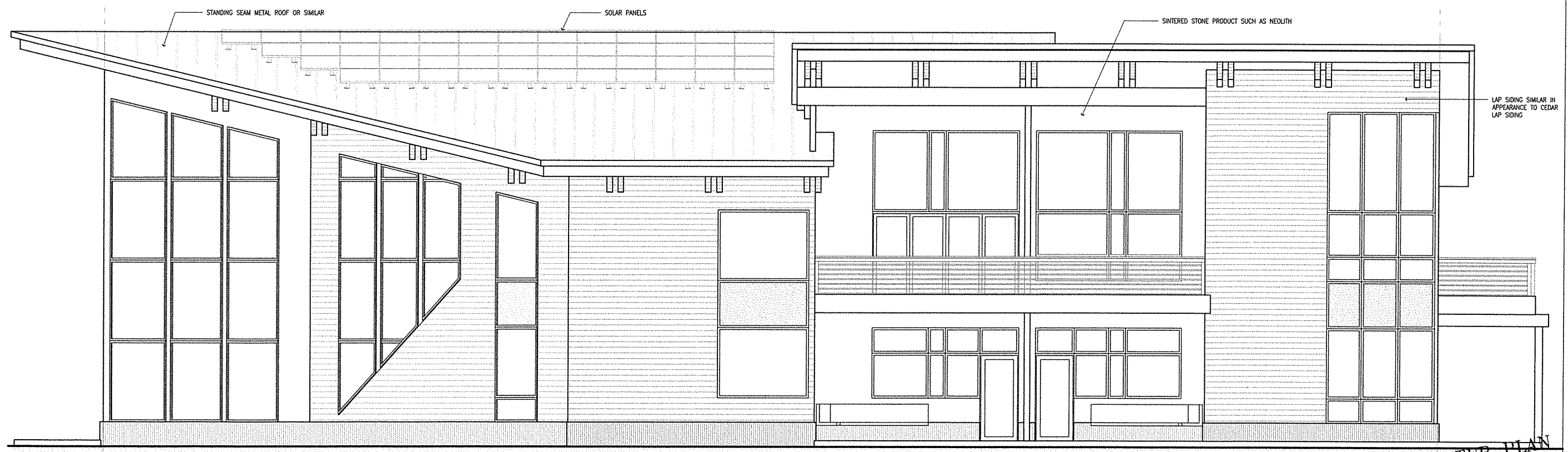
1 EAST ELEVATION
SCALE: 1/4" = 1'-0"



2 NORTH ELEVATION
SCALE: 1/4" = 1'-0"



1 WEST ELEVATION
SCALE: 1/4" = 1'-0"



2 SOUTH ELEVATION
SCALE: 1/4" = 1'-0"

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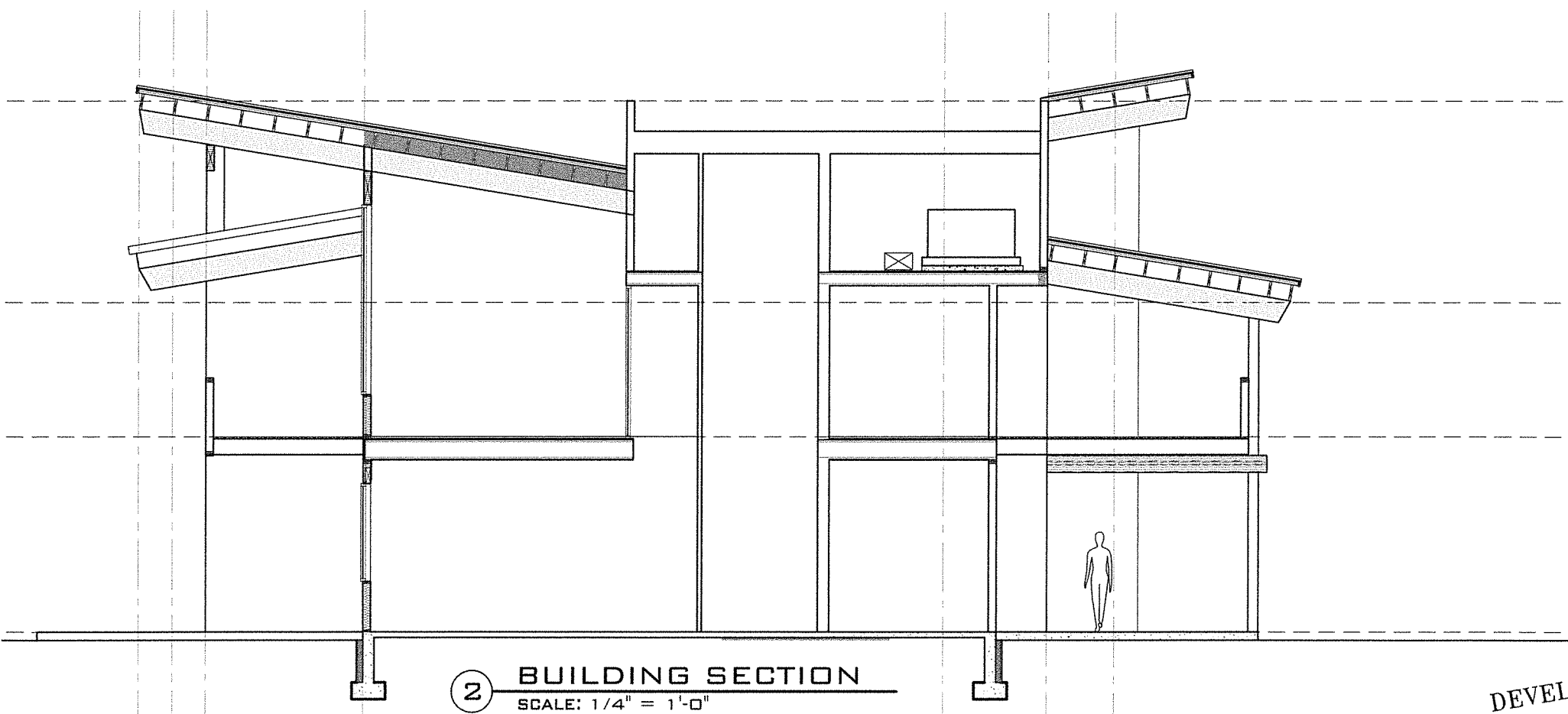
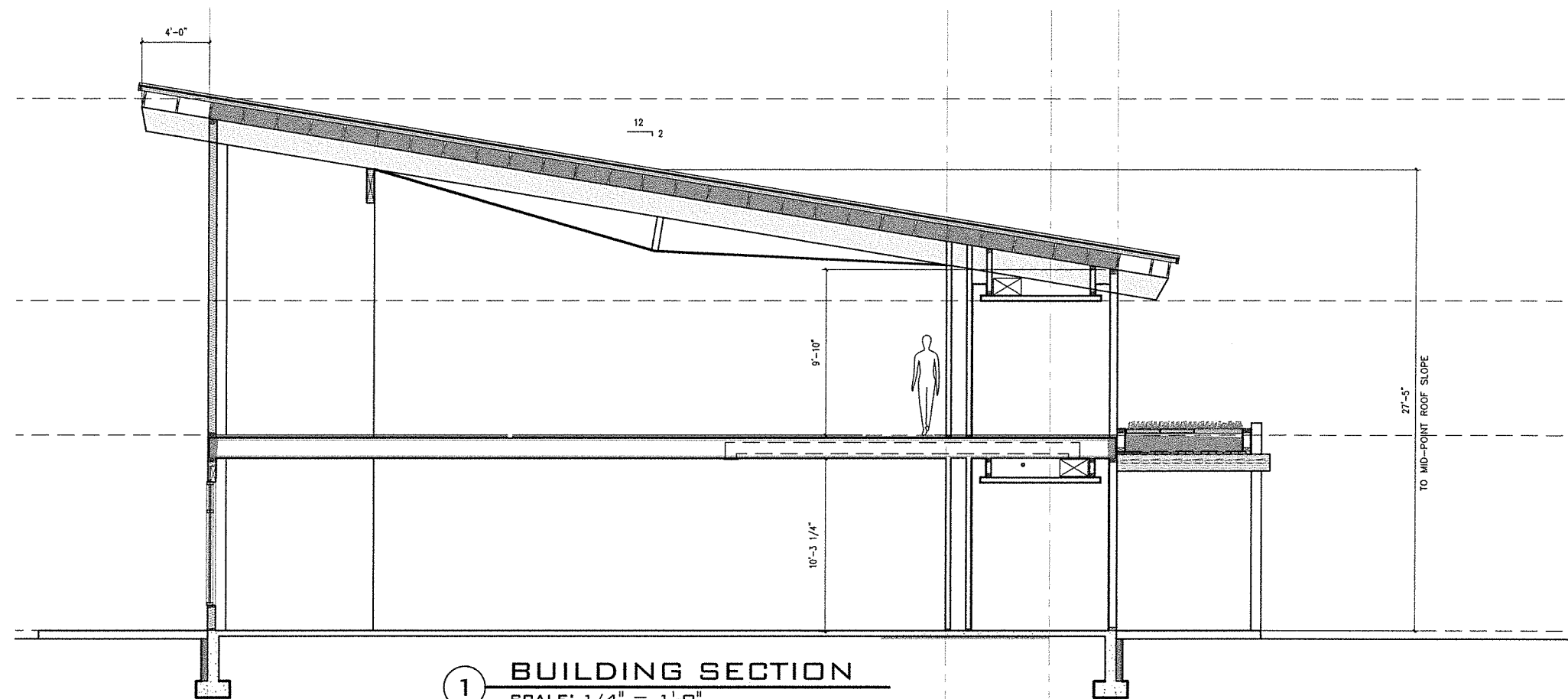
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CONCEPT DESIGN

BUILDING ELEVATIONS

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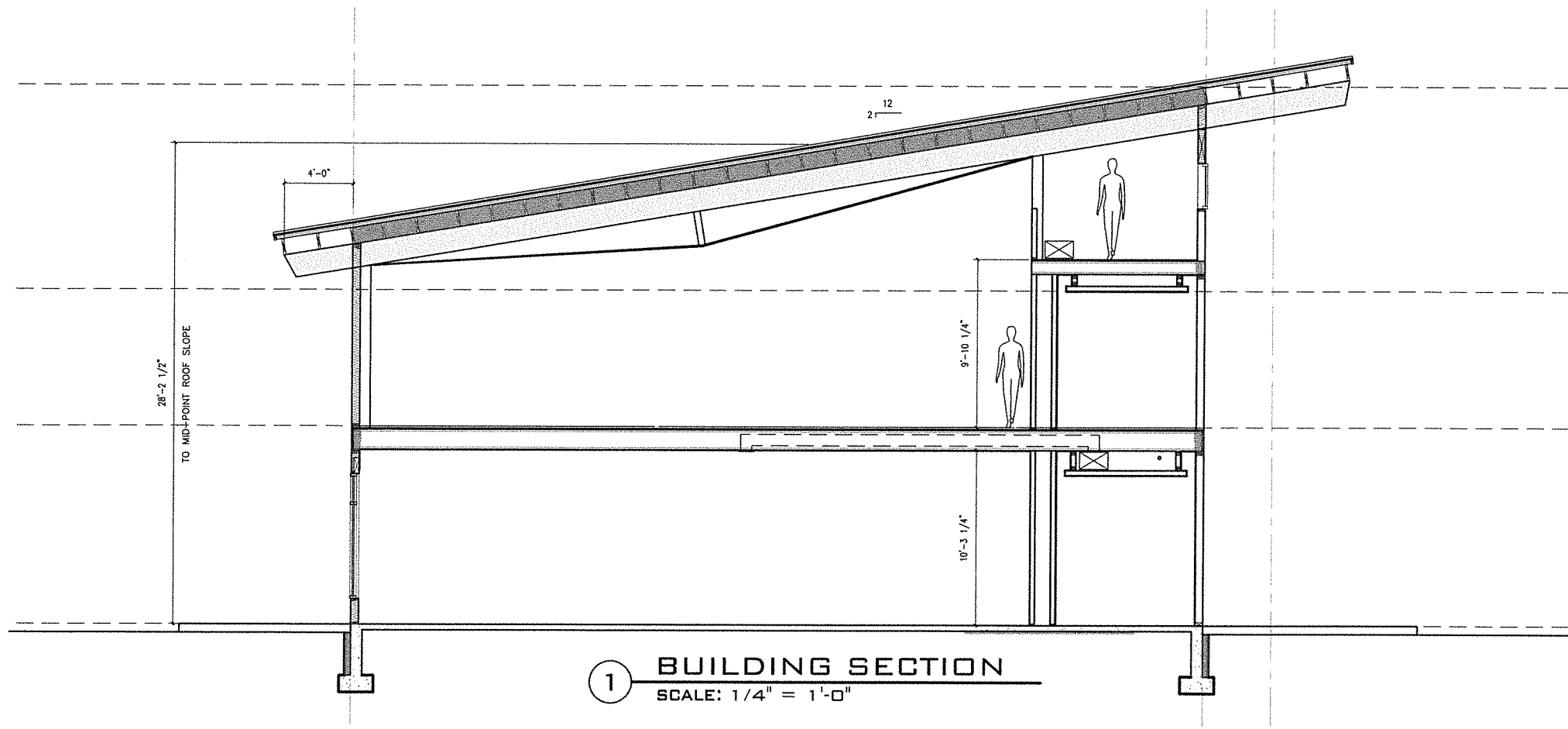
GLACIER CITY CENTER
BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY
BIRDWOOD, ALASKA
CONCEPT DESIGN

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BUILDING A - COFFEE & SANDWICH SHOPS, DISTILLERY
GIRDWOOD, ALASKA
CONCEPT DESIGN

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GLACIER CITY CENTER TOWNHOUSES

SOUTH UNITS

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6927 Old Seward Hwy
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Anchorage AK 99518
Phone: 907.349.9712
eric@eiceng.com



GLACIER CITY CENTER TOWNHOUSES
GIRDWOOD, ALASKA

COVER SHEET - SOUTH UNITS

SUBMITTAL: 12.13.21
DRAWN BY: ET
CHECKED BY: MAZ
REVISIONS:

JOB NUMBER 21-25
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Ts 1.1

UNIT AREA (SQUARE FEET)

	GARAGE SPACE UNIT 1	LIVING SPACE UNIT 1	TOTAL UNIT 1
THIRD FLOOR	N.A.	648	648
SECOND FLOOR	N.A.	648	648
FIRST FLOOR	506	142	648
TOTAL GROSS AREA	506	1,438	1,944

5 UNIT BUILDING AREA (SQUARE FEET)

	TWO BEDROOM UNITS (5)
THIRD FLOOR	3,240
SECOND FLOOR	3,240
FIRST FLOOR (LIVING SPACE AND GARAGE)	3,240
TOTAL GROSS BUILDING AREA	9,720

DRAWING INDEX

Ts1.1 COVER SHEET
Rs1.1 RENDERINGS
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As1.2 SECOND FLOOR PLAN - SOUTH UNITS
As1.3 THIRD FLOOR PLAN - SOUTH UNITS
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As2.1 ELEVATIONS - SOUTH UNITS
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STRUCTURAL - SOUTH UNITS

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Ss1.2 SECOND FLOOR FRAMING - SOUTH UNITS
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NOT INCLUDED IN
DEVELOPMENT MASTER PLAN
SUBMITTAL

DEVELOPMENT MASTER PLAN
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12-21-21

FULL SIZE DRAWING SHEETS ARE 24"x36". SCALES ON
OTHER SHEET SIZES SHOULD BE ADJUSTED ACCORDINGLY



EAST



NORTH



WEST



SOUTH



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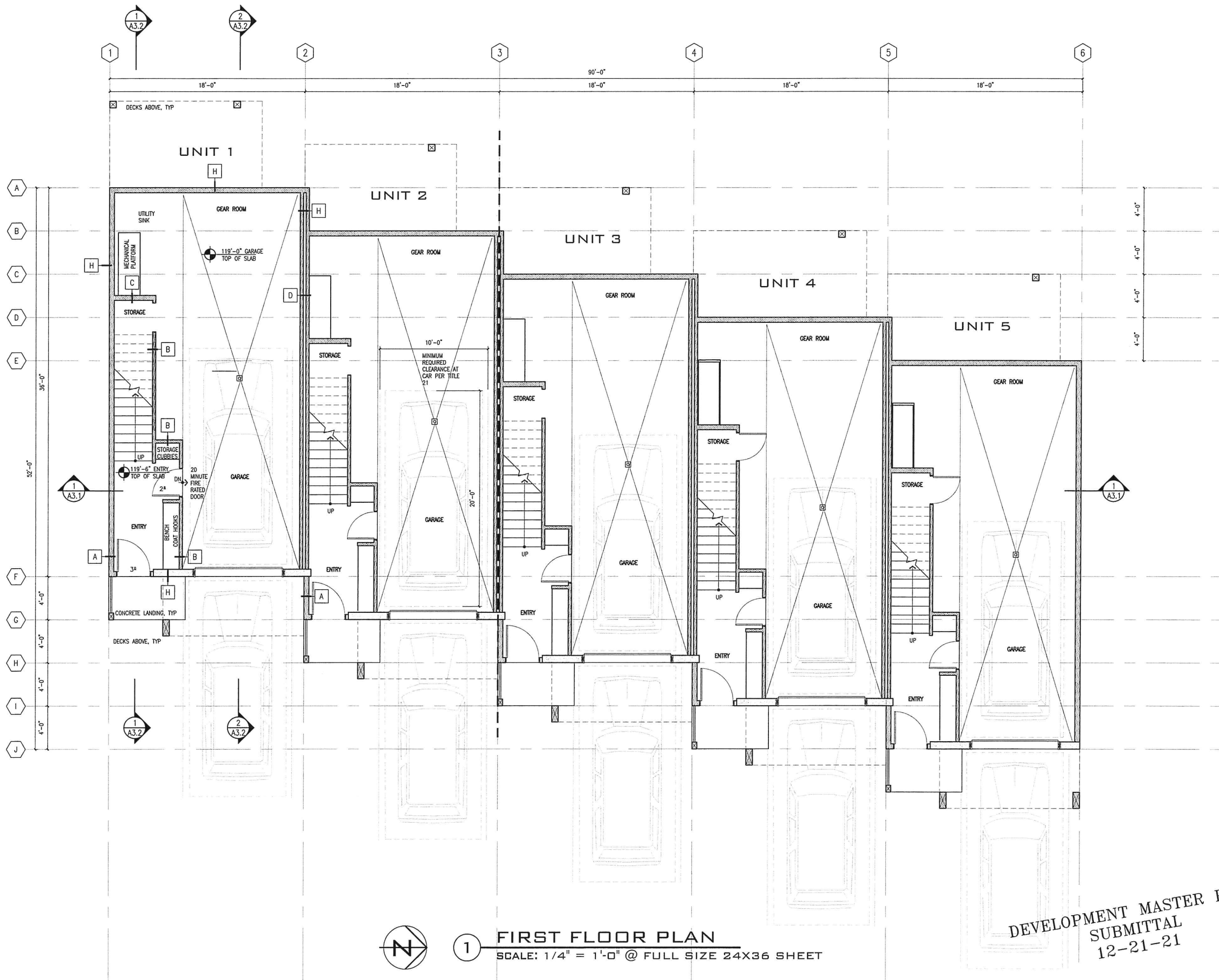
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BIRDWOOD, ALASKA

FIRST FLOOR PLAN - SOUTH UNITS

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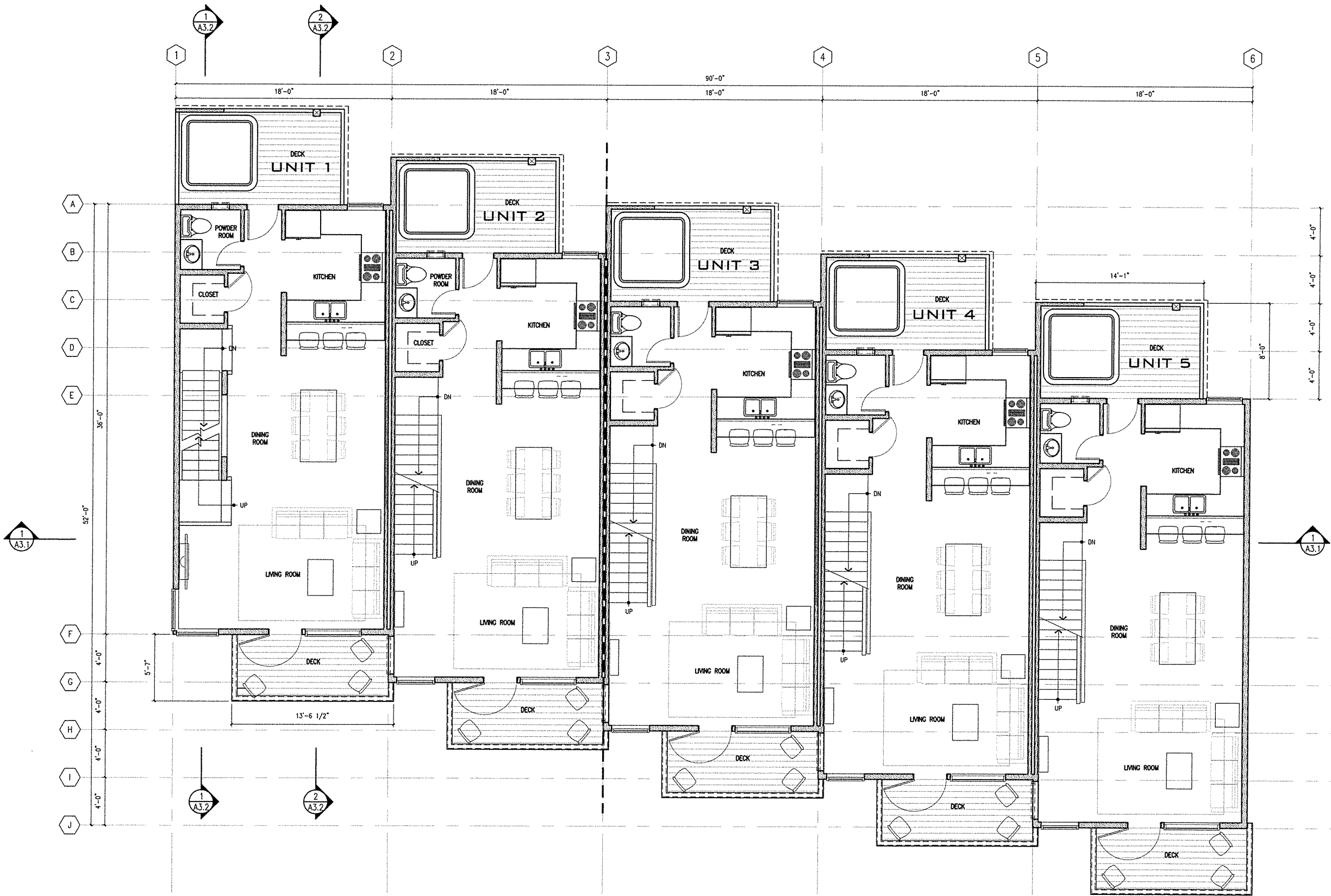
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As1.1



1 FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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1 SECOND FLOOR PLAN
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



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BIRDWOOD, ALASKA

SECOND FLOOR PLAN - SOUTH UNITS

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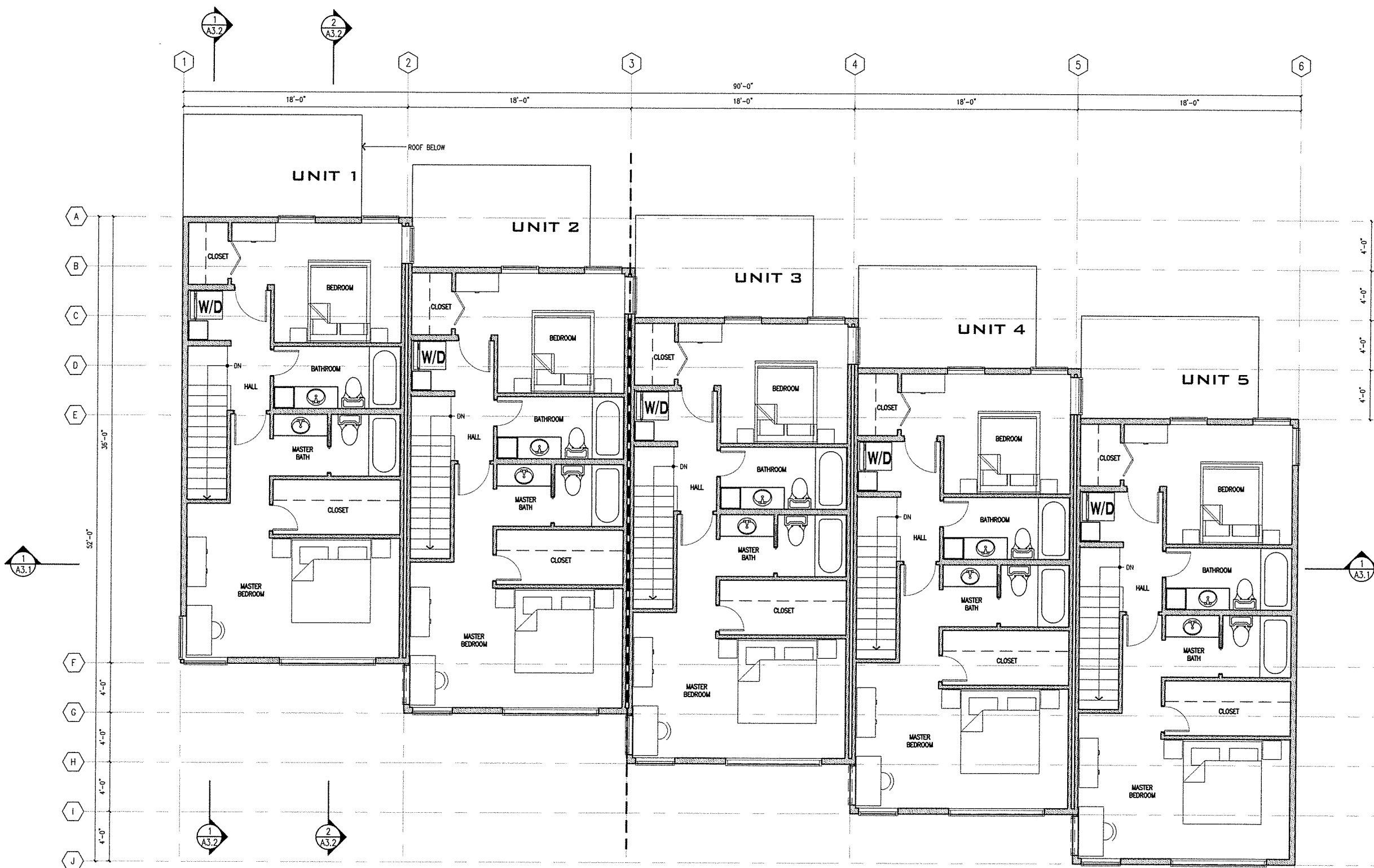
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GIRDWOOD, ALASKA

THIRD FLOOR PLAN - SOUTH UNITS

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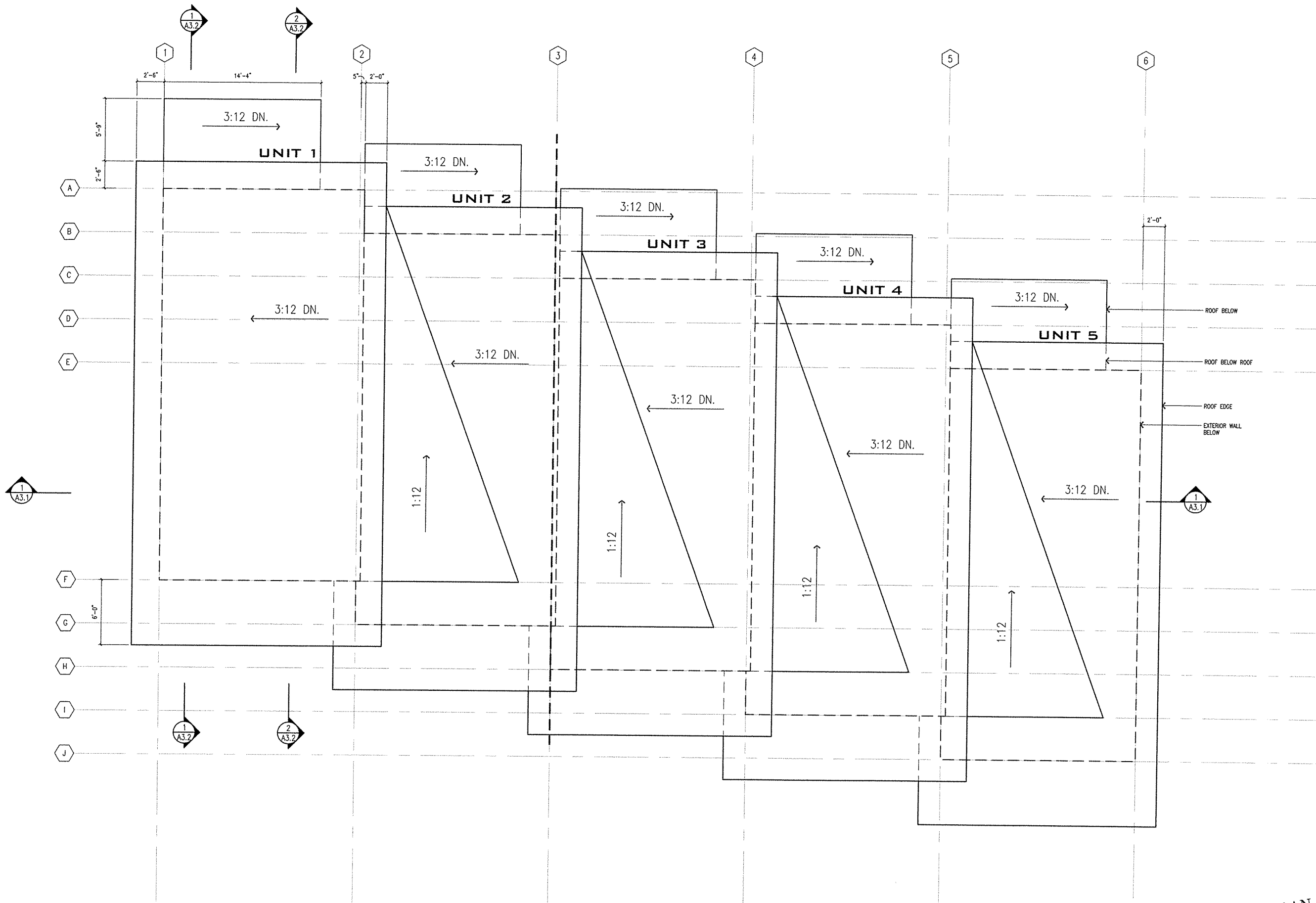


1

THIRD FLOOR PLAN

SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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1 ROOF PLAN
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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GIRDWOOD, ALASKA

ROOF PLAN - SOUTH UNITS

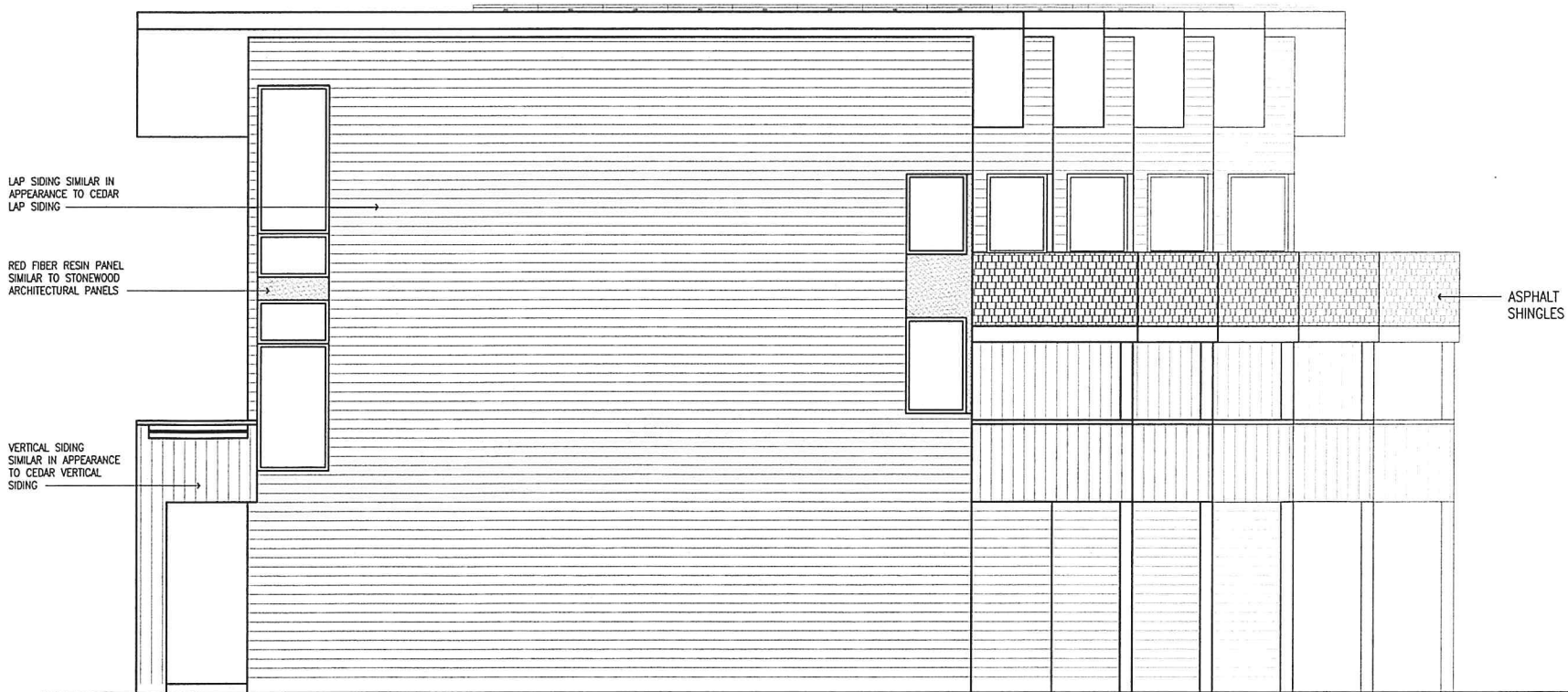
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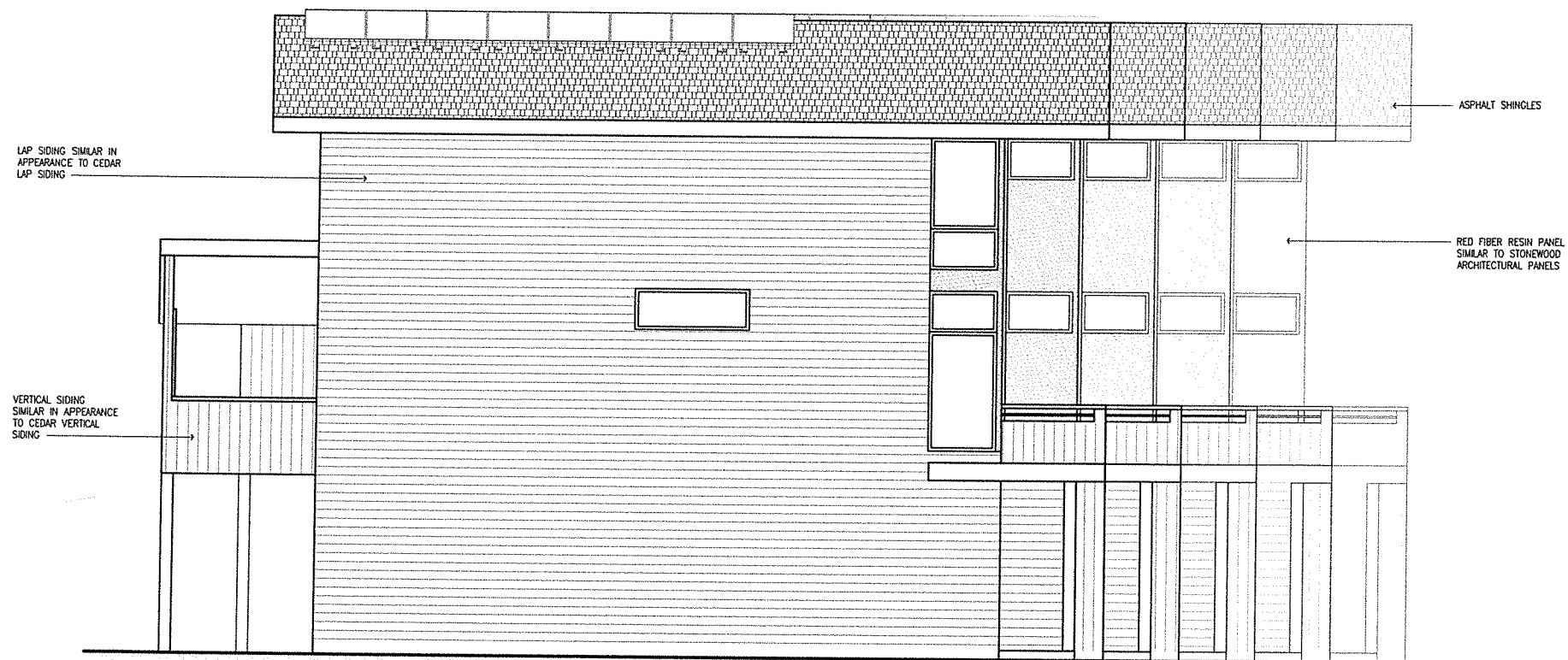
1 **EAST ELEVATION**
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



2 **SOUTH ELEVATION**
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

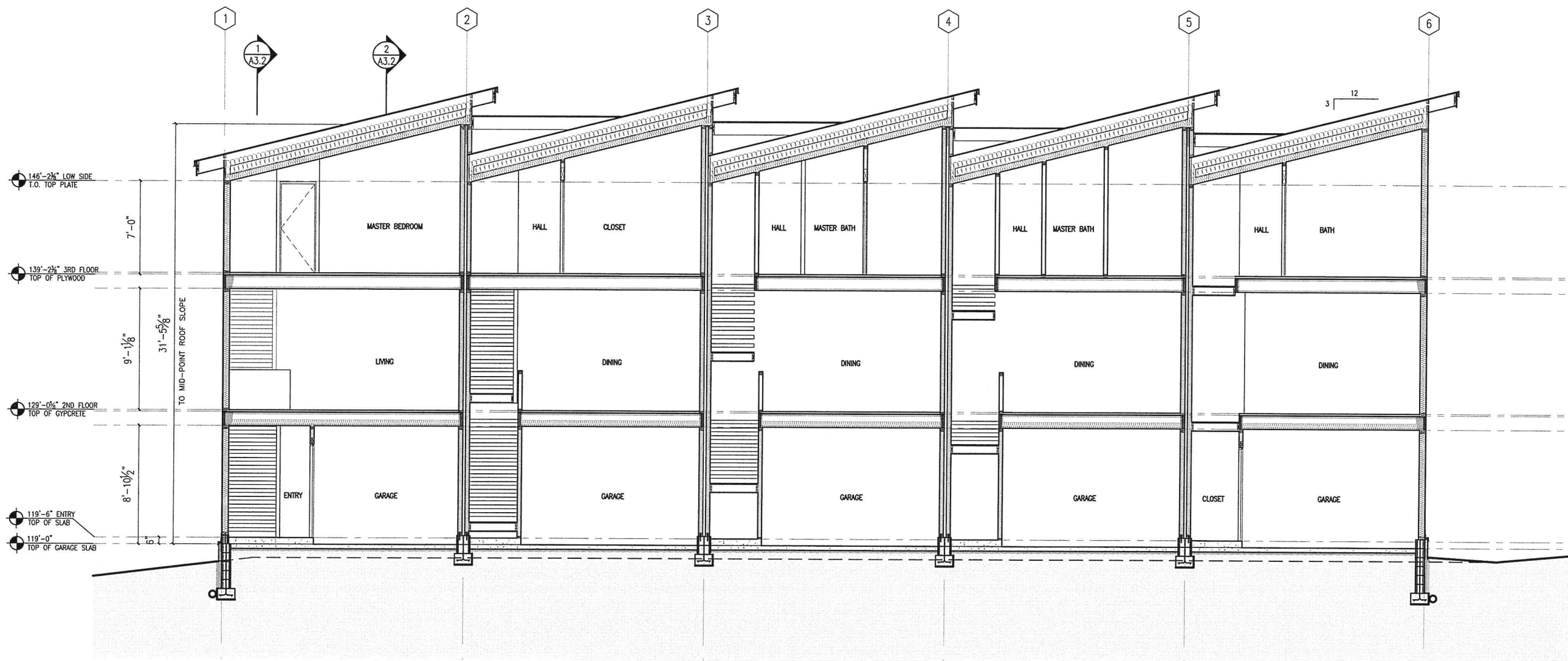


1 WEST ELEVATION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



2 NORTH ELEVATION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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1 SECTION
 SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



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 GIRDWOOD, ALASKA

BUILDING SECTIONS - SOUTH UNITS

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As3.1



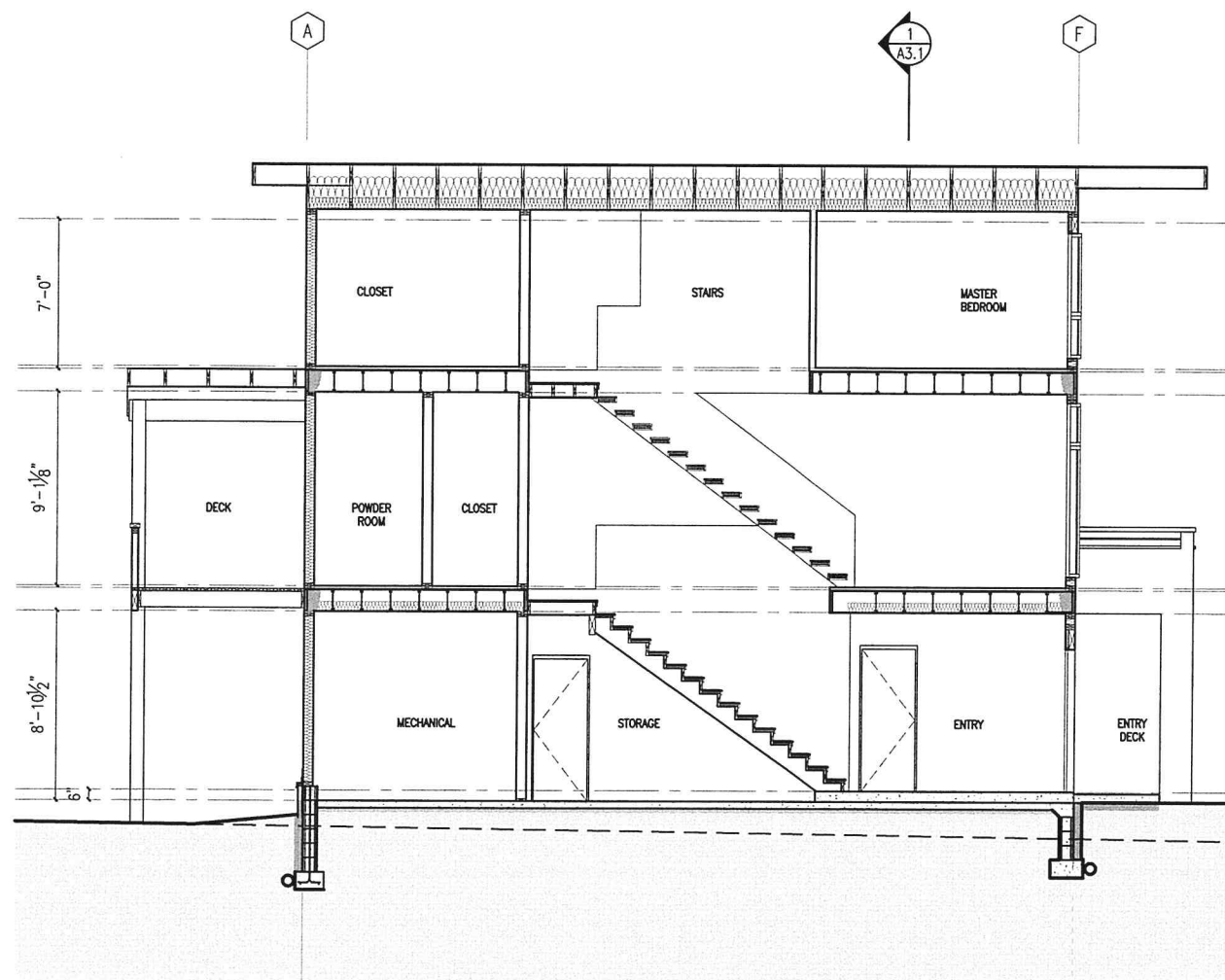
GLACIER CITY CENTER TOWNHOUSES
BIRDWOOD, ALASKA

BUILDING SECTIONS - SOUTH UNITS

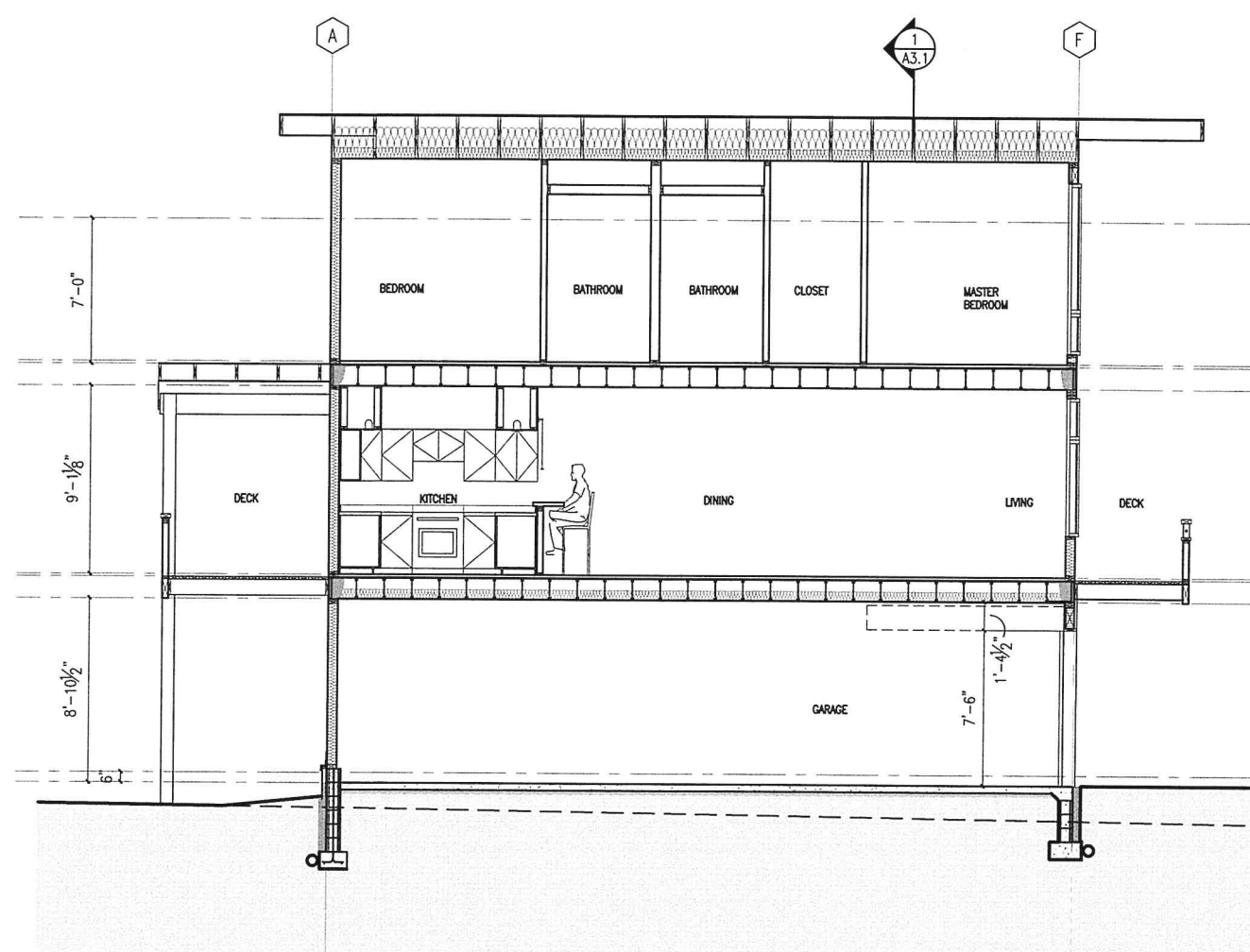
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1 SECTION
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2 SECTION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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NORTH UNITS

Girdwood, Alaska



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www.bellalaska.com
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Phone: 907.279.2688
earthscapealaska.com
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www.bbfm.com
- **Mechanical**
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eric@eiceng.com



GLACIER CITY CENTER TOWNHOUSES
GIRDWOOD, ALASKA

COVER SHEET - NORTH UNITS

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DRAWING INDEX

Tn1.1 COVER SHEET

ARCHITECTURAL – NORTH UNITS

- | | |
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| An1.2 | SECOND FLOOR PLAN |
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| An2.1 | ELEVATIONS |
| An2.2 | ELEVATIONS |
| An3.1 | BUILDING SECTIONS |
| An3.2 | BUILDING SECTIONS |

NOT INCLUDED IN
DEVELOPMENT MASTER PLAN
SUBMITTAL. SECTIONS ARE THE SAME
AS SOUTH UNITS.

~~STRUCTURAL - NORTH UNITS~~

- ~~Sn1.1 FOUNDATION PLAN - NORTH UNITS
Sn1.2 SECOND FLOOR FRAMING - NORTH UNITS
Sn1.3 THIRD FLOOR FRAMING - NORTH UNITS
Sn1.4 ROOF FRAMING - NORTH UNITS~~

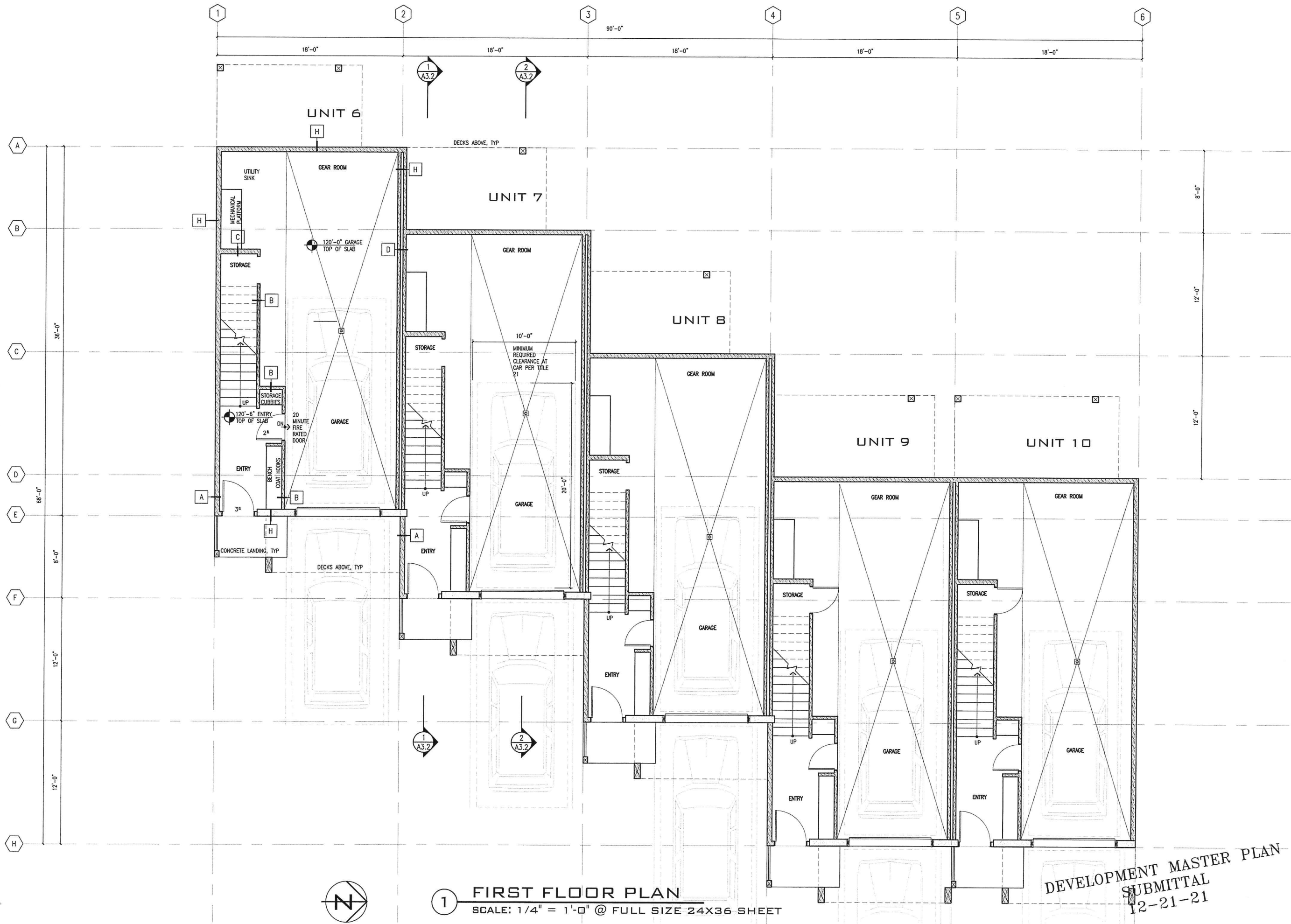
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SUBMITTAL

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1

FIRST FLOOR PLAN

SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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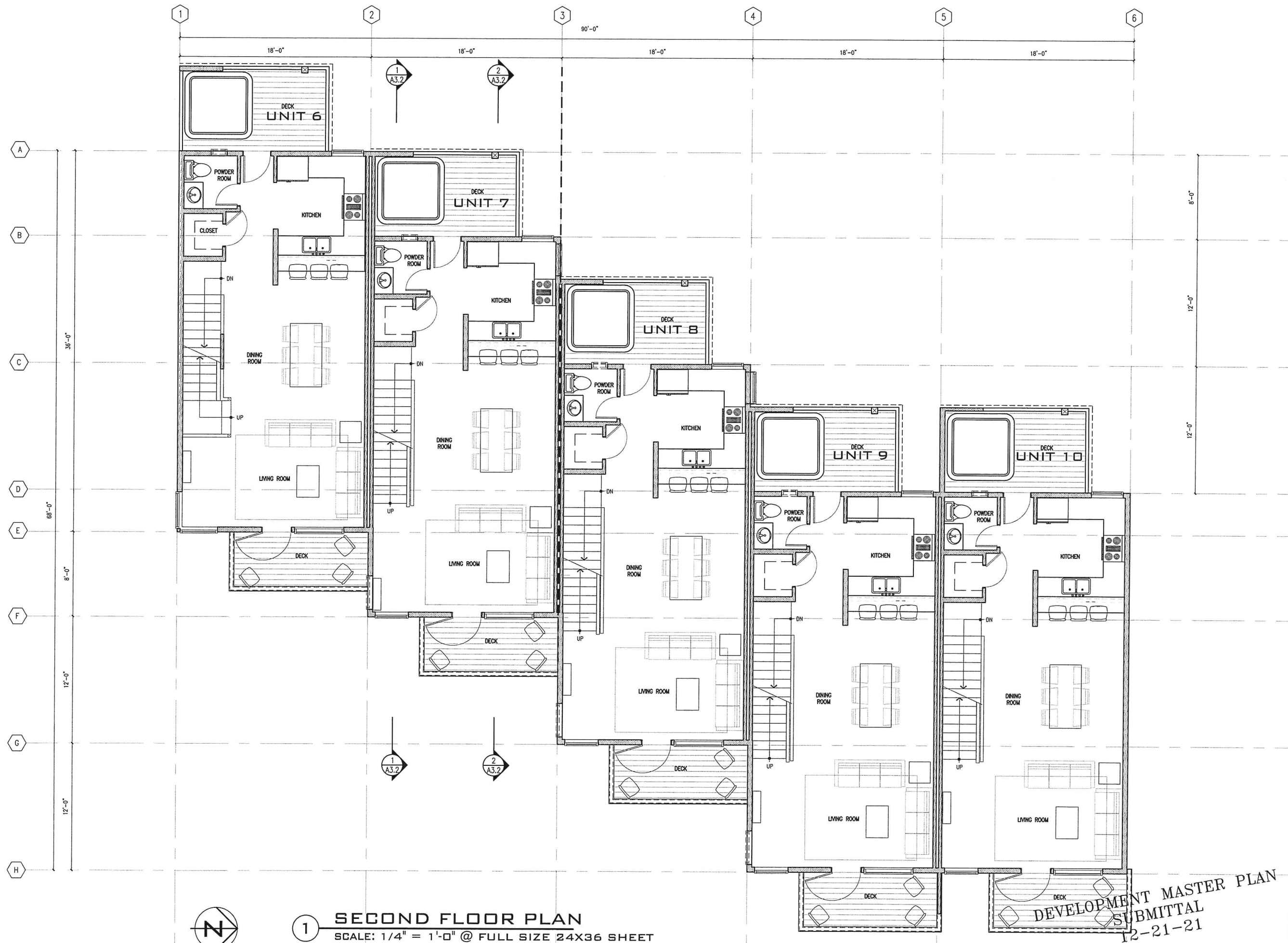
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BIRDMOOD, ALASKA

FIRST FLOOR PLAN - NORTH UNITS

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1

SECOND FLOOR PLAN

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BIRDWOOD, ALASKA

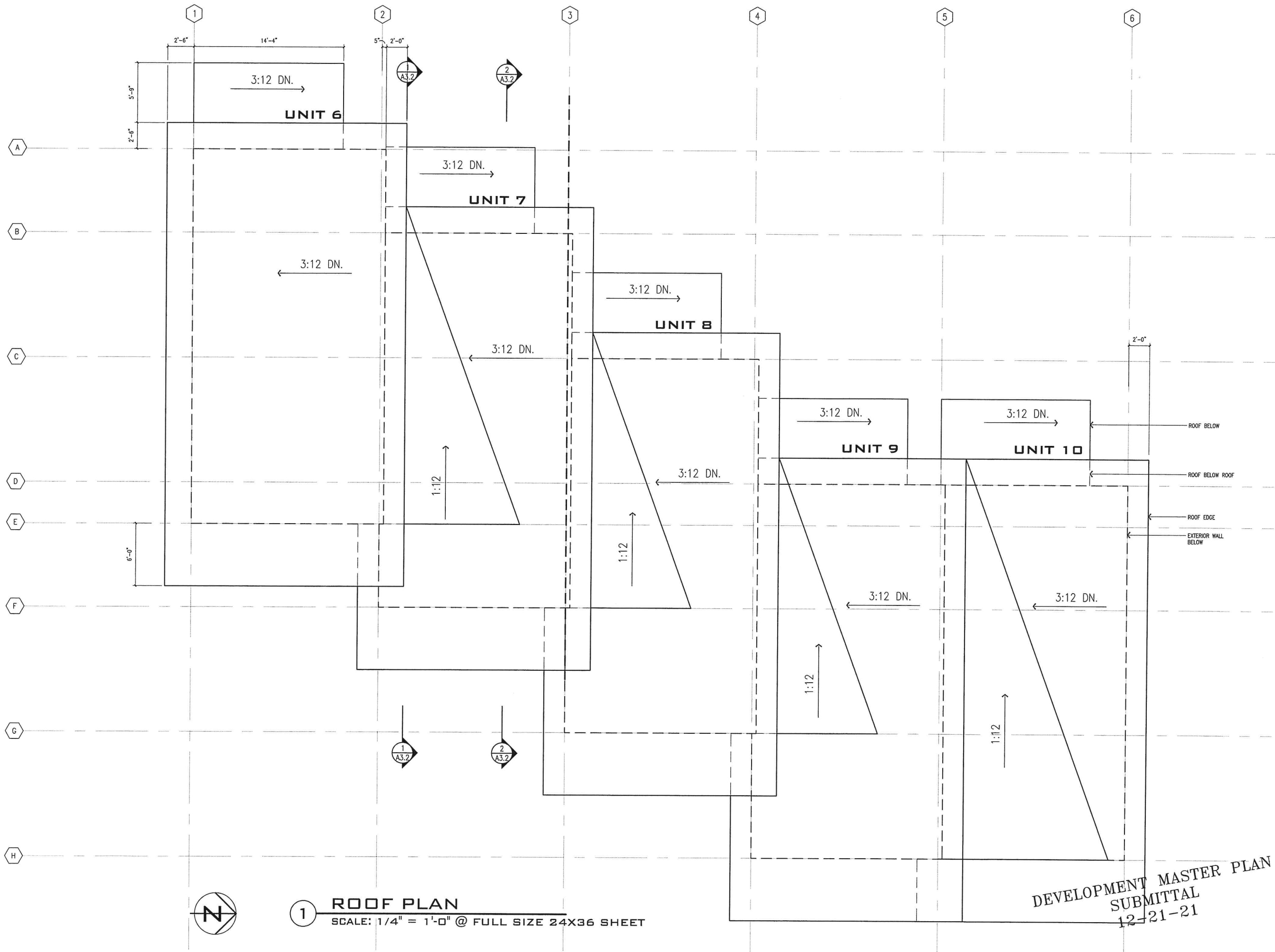
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GIRDWOOD, ALASKA

ROOF PLAN - NORTH UNITS

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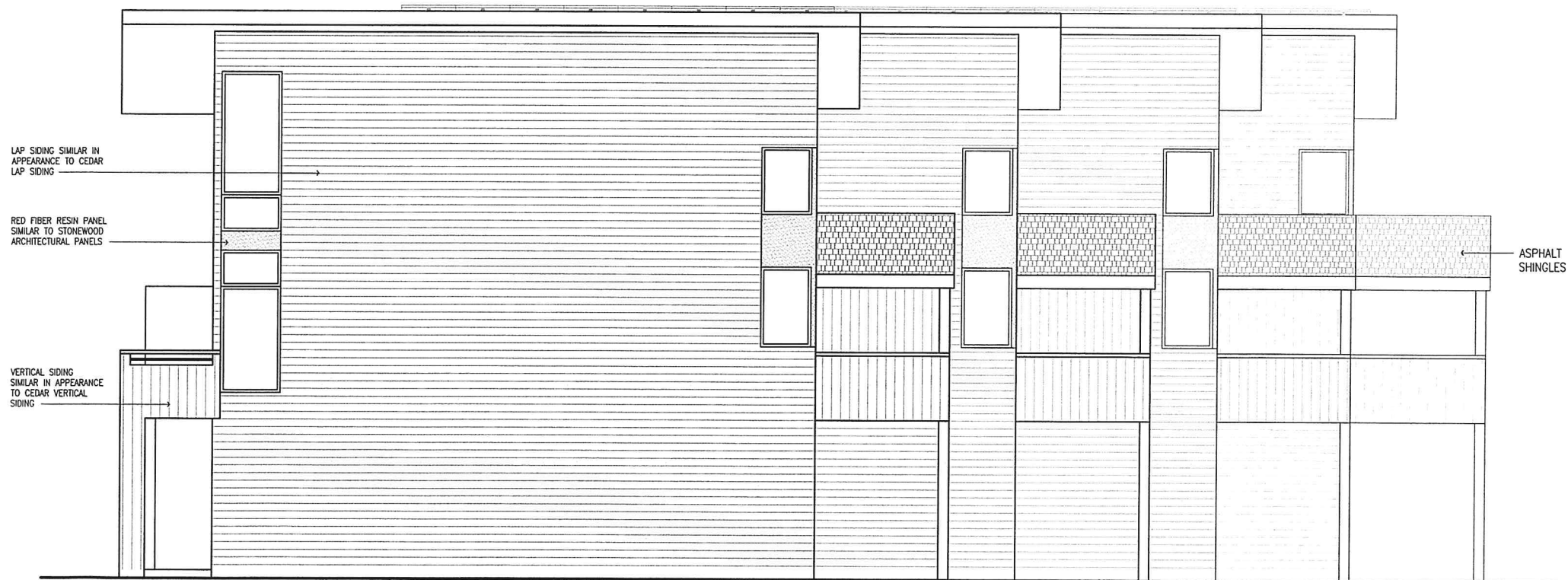
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AN1.4

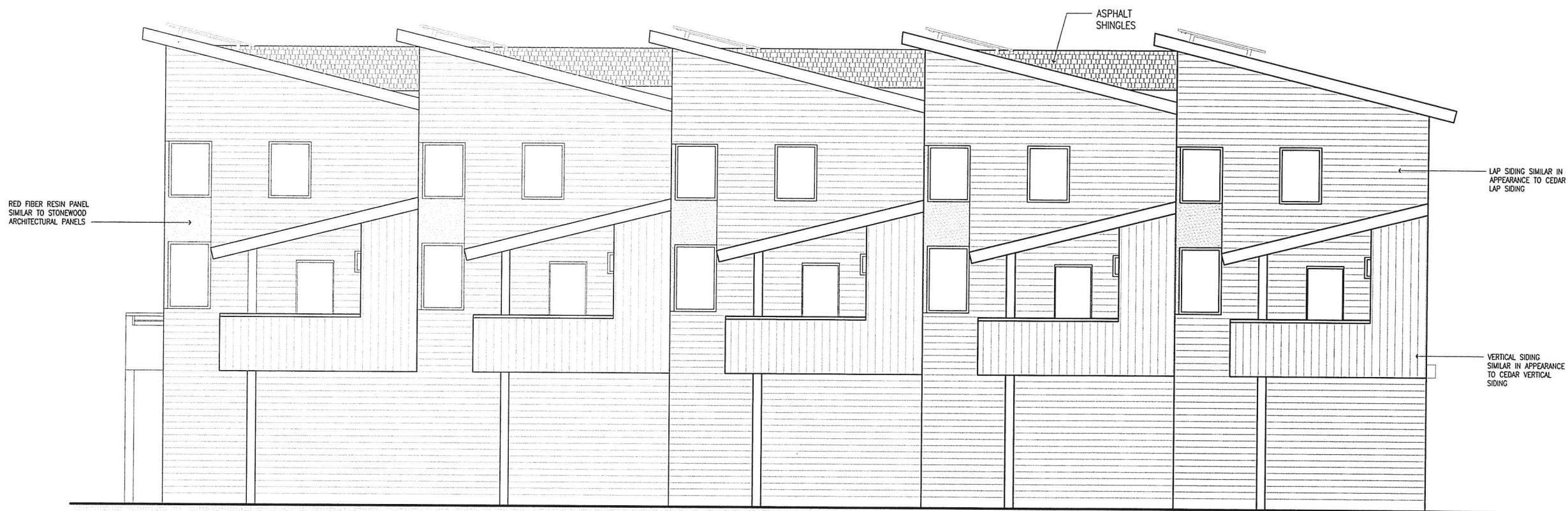
1 ROOF PLAN
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



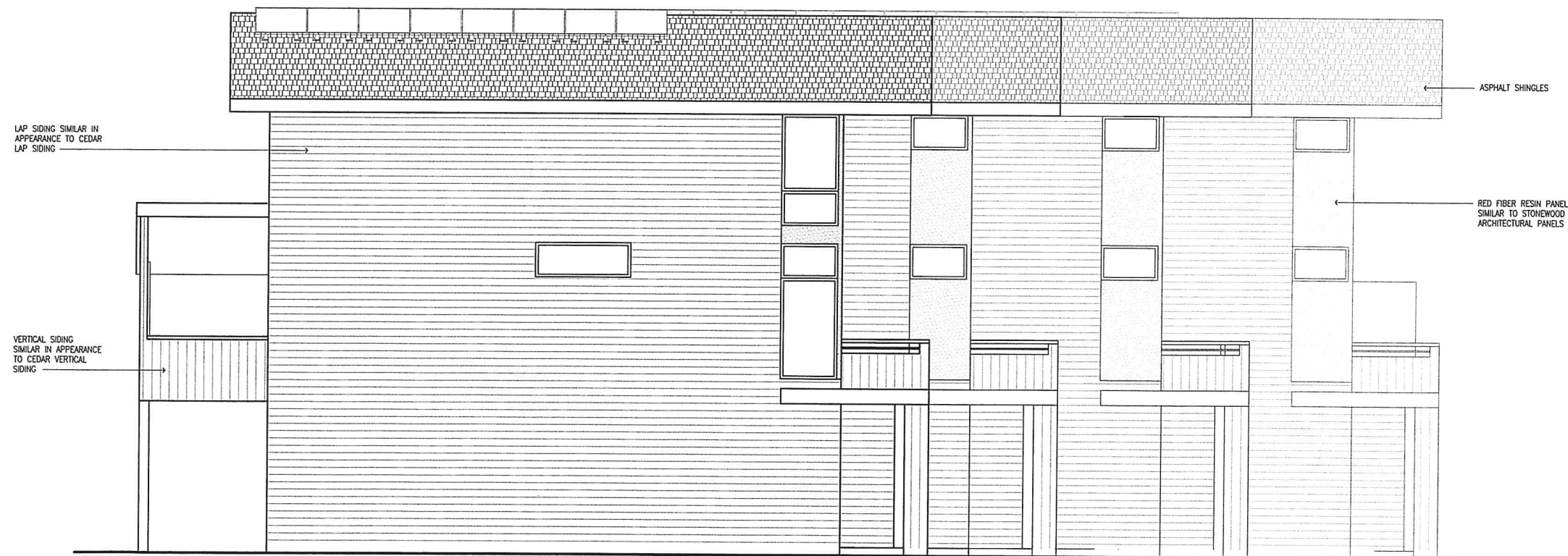
1 EAST ELEVATION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



2 SOUTH ELEVATION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



1 WEST ELEVATION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET



2 NORTH ELEVATION
SCALE: 1/4" = 1'-0" @ FULL SIZE 24X36 SHEET

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REF	NOTE
001	VIA PHOTOCELL CONTROLLED CONTACTOR.


- A. SITE LIGHTING WILL BE DESIGNED TO ACHIEVE 1 FOOTCANDLE AVERAGE ON THE WALKING PATHS PER THE GIRDWOOD LAND USE REGULATIONS, CHAPTER 21.03.070.G. ALL SITE LIGHTING FIXTURES WILL UTILIZE FULL CUTOFF DISTRIBUTION TO MINIMIZE GLARE AND LIGHT TRESPASS AND TO ENSURE THE LIGHT SOURCE IS NOT VISIBLE FROM THE PROPERTY LINE. WALKWAY LIGHTING MAY UTILIZE A VISIBLE LIGHT SOURCE PROVIDED THEY ARE DIFFUSED WITH A TRANSLUCENT COVER, FINAL LOCATION, QUANTITY, AND TYPE OF SITE LIGHTING FIXTURES IS TO BE DETERMINED.
- B. EMERGENCY EGRESS LIGHTING WILL BE PROVIDED VIA BUILDING MOUNTED LIGHTINGS WHERE REQUIRED BY THE IBC. THE BUILDING MOUNTED LIGHTING WILL BE SHOWN ON SEPARATE DRAWINGS AT A FUTURE DATE.
- C. FINAL SIZING AND LOCATION OF ELECTRICAL DISTRIBUTION EQUIPMENT IS TO BE DETERMINED.

LUMINAIRE SCHEDULE							
NOTES							
(K.	{x}	DENOTES A GENERAL, NON-REFERENCED, NOTE. NUMBERED NOTES ARE REFERENCED IN THE...					
(A)	CATALOG NUMBERS ARE FOR GENERAL REFERENCE AND ARE NOT INCLUSIVE OF ALL...						
(B)	REFER TO ARCHITECTURAL DRAWINGS FOR EXACT LOCATION AND PROVIDE MOUNTING...						
(C)	PROVIDE UNIVERSAL OR MULTI-VOLTAGE LIGHT DRIVERS WHEN AVAILABLE. COORDINATE EXACT...						
(D)	LIGHT SOURCE COLOR TEMPERATURE, UNLESS OTHERWISE NOTED: 3500K (SELECT NEAREST AVAILABLE...						
(E)	COLOR FINISH FOR ALL EXTERIOR LUMINAIRES TO BE DARK BRONZE UNON.						
(F)	ALL POLES TO BE SQUARE STEEL WITH VIBRATION DAMPER RATED FOR 100MPH WIND WITH 1.3 GUST...						
(G)	PROVIDE DRIVEN STEEL PILE LIGHT POLE FOUNDATION PER M.A.S.S. DETAIL 80-13. EXTEND PILE 3'-0" ABOVE GRADE AND PROVIDE 3'-0" CONCRETE PROTECTION BASE UNON FOR AN OVERALL MOUNTING HEIGHT AS...						
1 [NOT USED.							
SCHEDULE							
TYPE	DESCRIPTION	WATTS	LAMPS	MOUNTIN	MANUFACTURER	MODEL	NOTES
ZP6	DUAL HEAD POLE MOUNTED AREA LIGHT	100 W	5860 LM LED	POLE +14'-0"	LITONIA	P2 40K	



**GLACIAR CITY CENTER
TOWNHOUSES**
HIGHTOWER RD & STUMPY WY
GIRDWOOD, ALASKA

REVISIONS	
NUM	DESCRIPTION



JOB NO.	E21-3747
DATE	12/21/2021
DRAWN	LDW
REVIEWED	EDC

ELECTRICAL SITE PLAN

SHEET NO.

E1.2