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Cross, and Dunbar  
Prepared by: Planning Department  
Reviewed by: Department of Law and  
Assembly Counsel's Office  
For reading: November 22, 2022

**ANCHORAGE, ALASKA  
AO No. 2022-80(S)**

1 **AN ORDINANCE OF THE ANCHORAGE MUNICIPAL ASSEMBLY AMENDING**  
2 **ANCHORAGE MUNICIPAL CODE TITLE 21 CHAPTERS 21.04, ZONING**  
3 **DISTRICTS; 21.07, DEVELOPMENT AND DESIGN STANDARDS; 21.11,**  
4 **DOWNTOWN; 21.15, RULES OF CONSTRUCTION AND DEFINITIONS, AND**  
5 **AMENDING ANCHORAGE MUNICIPAL CODE OF REGULATIONS SECTION**  
6 **21.20.007, SCHEDULE OF FEES, IN ORDER TO AMEND THE OFF-STREET**  
7 **PARKING AND SITE ACCESS DEVELOPMENT STANDARDS AND ACHIEVE**  
8 **COMPREHENSIVE PLAN GOALS FOR INFILL AND REDEVELOPMENT,**  
9 **HOUSING, AND ACCESSIBLE LAND USE.**

10  
11 (Planning and Zoning Commission Case No. 2022-0026)

12  
13 **WHEREAS, a thriving city is a place full of people, that has a strong**  
14 **economy, a diverse transportation network everyone can safely use, and an**  
15 **attractive environment of buildings, public spaces, and natural features; and**

16  
17 **WHEREAS, surface parking lots have become a large portion of our city's**  
18 **total land area as the city's footprint continues to grow, leaving more of our**  
19 **land covered in asphalt and reducing available area for housing,**  
20 **development, and green spaces; and**

21  
22 **WHEREAS,** the *Anchorage 2040 Land Use Plan (2040 Plan)* assesses the  
23 housing and employment needs of current and future Anchorage residents and  
24 includes **a vision,** goals, policies, and actions to address these needs; and

25  
26 **WHEREAS,** Goal 3 of the *2040 Plan* establishes Anchorage's commercial centers  
27 and corridors as the places to accommodate new business growth and mixed-use  
28 housing opportunities through infill and redevelopment in a more efficiently-  
29 **developed,** walkable **community[development pattern];** and

30  
31 **WHEREAS,** Goal 4 of the *2040 Plan* establishes Anchorage's neighborhoods as  
32 the places to provide a range of additional housing opportunities, meeting the  
33 housing needs of residents of all income levels and household types; and

34  
35 **WHEREAS,** off-street parking **minimums** and driveway access standards are  
36 often the costliest and most land-consuming zoning requirements for multi-unit  
37 housing, mixed-use, and business developments; and

38  
39 **WHEREAS,** minimum parking requirements[, ~~where they exceed forecast~~  
40 ~~parking utilization,~~] are a barrier to the development of a range of housing types  
41 and businesses **and hinder adaptive re-use of existing buildings;** and

1  
2 **WHEREAS, [wider than necessary site access driveway and vehicle**  
3 **circulation aisle requirements combined with i]** inadequate treatment of  
4 pedestrian and bicyclist site access needs also impact the cost and range of  
5 housing types and business developments, and choices for travel mode; and

6  
7 **WHEREAS,** Action 4-3 of the *2040 Plan* calls for amendments to Title 21 to allow  
8 parking reductions by right for residential uses and to offer greater parking  
9 reductions in key development areas; and

10  
11 ~~**[WHEREAS, Action 4-6 of the 2040 Plan calls for amendments to Title 21 for**~~  
12 ~~**internal site circulation for vehicles and private lanes for compact infill**~~  
13 ~~**housing; and]**~~

14  
15 ~~**[WHEREAS, the 2040 Plan as well as neighborhood and district plans**~~  
16 ~~**recognize traditional urban neighborhoods and transit-supportive**~~  
17 ~~**development corridors in certain parts of the Anchorage Bowl as having**~~  
18 ~~**stronger street grid patterns, greater access to public transit and sidewalks,**~~  
19 ~~**lower rates of car ownership and parking utilization, and shorter distances**~~  
20 ~~**between trip destinations, which merit alternative parking, driveway, and**~~  
21 ~~**form-based regulations tailored to their urban contexts; and]**~~

22  
23 **WHEREAS,** the *Metropolitan Area Transportation Solutions (AMATS) 2040*  
24 *Metropolitan Transportation Plan* sets forth policies and actions under Goal 3 to  
25 develop an efficient multi-modal transportation system to reduce congestion,  
26 promote accessibility, and improve system reliability, including Action 3E-5 to  
27 pursue parking strategies that support multi-modal transportation options; and

28  
29 **WHEREAS,** the *AMATS Non-Motorized Plan (2022)* calls for short-term and long-  
30 term bicycle parking spaces and provides recommendations for their design and  
31 location; and

32  
33 **WHEREAS,** neighborhood and district plans support amending Title 21 parking  
34 and site access regulations, including but not limited to the *Spenard Corridor Plan*  
35 Policies 5.28, 5.29, 5.30, and 5.32 for flexible parking requirements, compact  
36 parking design, parking management solutions, and shared parking to create  
37 efficiencies to further redevelopment and investment goals; and the *Fairview*  
38 *Neighborhood Plan* Strategy 3.2 to reduce required on-site parking; and

39  
40 **WHEREAS,** amending Title 21 to remove~~[streamline approvals for~~  
41 ~~administrative parking reductions from]~~ the requirement to provide a  
42 minimum number of parking [required -] spaces [and expand the menu of  
43 available parking reduction strategies,] will provide [more -]flexibility, allow  
44 builders to determine parking needs based on market demand, reduce costs,  
45 and make it easier to build~~[facilitate -]~~developments with efficient, compact site  
46 plans that balance site access for motorists, pedestrians, and bicyclists in context  
47 with the surrounding neighborhood; and

48  
49 ~~**[WHEREAS, amending Title 21 to replace five existing area-specific**~~

~~administrative parking reductions with by-right lower minimum parking requirements in urban neighborhood contexts near Downtown and transit-supportive development corridors will allow for more efficient land use, more flexibility to meet market demand, and more choice for parking management strategies in these key areas; and]~~

~~[WHEREAS, amending multifamily residential driveway access and circulation requirements will facilitate development within urban contexts that is true to neighborhood character and desired outcomes; and]~~

~~[WHEREAS, the Planning Department received and incorporated public comments on the policy direction before writing the proposed Title 21 text amendments, received and incorporated comments on the drafts of the proposed Title 21 amendments, maintained a webpage covering the proposed Title 21 amendments that included an online questionnaire, and project information and presentations; and]~~

WHEREAS, the Planning Department received and incorporated feedback from dozens of consultations and public meetings with the general public, private-sector development experts, design/architectural professionals, business organizations, neighborhoods, public agencies, and municipal committees and commissions; and

WHEREAS, this Substitute (S) version of AO 2022-80 is the result of a working group process initiated by Assembly members and including the municipal Planning Department in careful consideration of public, agency, and developer concerns regarding the draft ordinance; now, therefore,

**THE ANCHORAGE ASSEMBLY ORDAINS:**

**Section 1.** Anchorage Municipal Code Chapter 21.04, Zoning Districts, is hereby amended to read as follows (*the remainder of the chapter is not affected and therefore not set out*):

**21.04.020 Residential Districts**

\*\*\* \*\*

H. *R-3A: Residential Mixed-Use District.*

\*\*\* \*\*

2. District-Specific Standards

\*\*\* \*\*

[E. *REDUCED PARKING RATIOS.* DEVELOPMENT IN THE R-3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION 21.07.090F.6.]

e[F]. *Enhanced [street] sidewalk option[OPTION].* An enhanced [street-]sidewalk environment[ENVIRONMENT] may be provided in lieu of required sidewalks and side perimeter landscaping, as provided in 21.07.060F.16[G.19][F.17].

f[G]. *Building height increases.*  
\*\*\*      \*\*\*      \*\*\*

g[H]. *Neighborhood protections.*  
\*\*\*      \*\*\*      \*\*\*

J. *R-4A: Residential Mixed-Use District.*  
\*\*\*      \*\*\*      \*\*\*

2. District-Specific Standards  
\*\*\*      \*\*\*      \*\*\*

[D. *REDUCED PARKING RATIOS. DEVELOPMENT IN THE R-4A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION 21.07.090F.6.]*

d[E]. *Building height increase.*  
\*\*\*      \*\*\*      \*\*\*

(AO 2012-124(S), 2-26-13; AO No. 2015-100, § 1, 10-13-15; AO No. 2017-176, § 3, 1-9-18; AO No. 2019-58, § 2, 5-7-19)

**21.04.030 Commercial districts.**  
\*\*\*      \*\*\*      \*\*\*

G. *Standards for Mixed-Use Development in the B-1A and B-1B Districts.*  
\*\*\*      \*\*\*      \*\*\*

[5. *REDUCED PARKING RATIOS. THE DEVELOPMENT IS ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT OF THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION 21.07.090F.6.]*

5[6]. *Enhanced ~~[street]~~ sidewalk option[OPTION]. An enhanced ~~[street]~~ sidewalk environment[ENVIRONMENT] may be provided in lieu of required sidewalks and side perimeter landscaping, as provided in 21.07.060F.16[G-19][F.17].*

*(Note to Code Revisor: Subsection 7 below, which was shown as deleted in AO 2022-80, is being restored to its current code language and re-numbered to Subsection 6 in the S-Version.)*

**6. Building placement and orientation. Buildings should be placed and oriented to the street, lining sidewalks and public spaces with frequent shops, entrances, windows with interior views, and articulated ground-level facades. The following standards apply:**

**a. Buildings shall have visual access windows and/or primary entrances on street-facing building elevations (up to a maximum of two elevations) for at least 15 percent of the non-residential ground floor wall area. Qualifying**

windows shall be no more than four feet above finished grade. Windows shall comprise at least ten percent of the wall area of the upper floor building elevation (above the ground floor). Exterior wall areas of building mechanical rooms are exempt. An elevation that is more than 150 feet away from the facing street shall be exempt, unless it is the only applicable elevation.

**b. Either:**

**i. Provide at least one primary entrance within 60 feet of a street sidewalk, or 90 feet for buildings over 25,000 square feet of gross floor area, and connected to the street by a clear and direct walkway; or**

**ii. Provide a prominent and inviting primary entrance that is visible from the street, connected by a direct walkway to the street, and highlighted by two of the following:**

**(A) Portico, overhang, canopy, or similar permanent feature projecting from the wall;**

**(B) Recessed and/or projected entrance that covers at least 80 square feet;**

**(C) Arches, peaked roof forms, terracing parapets, or other change of building roofline;**

**(D) Changes in siding material, or detail features such as tilework, to signify the entrance; or**

**(E) Entrance plaza, patio, or similar common private open space.**

**c. Buildings shall comply with the maximum setbacks established in Section 21.06.020, Tables of Dimensional Standards, and subsection 21.06.030C.5. The maximum setback may be reduced or eliminated with the concurrence of the director.**

[7. *BUILDING PLACEMENT AND ORIENTATION.* BUILDINGS SHOULD BE PLACED AND ORIENTED TO THE STREET, LINING SIDEWALKS AND PUBLIC SPACES WITH FREQUENT SHOPS, ENTRANCES, WINDOWS WITH INTERIOR VIEWS, AND ARTICULATED GROUND-LEVEL FACADES. THE FOLLOWING STANDARDS APPLY:

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A. BUILDINGS SHALL HAVE VISUAL ACCESS WINDOWS AND/OR PRIMARY ENTRANCES ON STREET-FACING BUILDING ELEVATIONS (UP TO A MAXIMUM OF TWO ELEVATIONS) FOR AT LEAST 15 PERCENT OF THE NON-RESIDENTIAL GROUND FLOOR WALL AREA. QUALIFYING WINDOWS SHALL BE NO MORE THAN FOUR FEET ABOVE FINISHED GRADE. WINDOWS SHALL COMPRISE AT LEAST TEN PERCENT OF THE WALL AREA OF THE UPPER FLOOR BUILDING ELEVATION (ABOVE THE GROUND FLOOR). EXTERIOR WALL AREAS OF BUILDING MECHANICAL ROOMS ARE EXEMPT. AN ELEVATION THAT IS MORE THAN 150 FEET AWAY FROM THE FACING STREET SHALL BE EXEMPT, UNLESS IT IS THE ONLY APPLICABLE ELEVATION.

B. EITHER:

I. PROVIDE AT LEAST ONE PRIMARY ENTRANCE WITHIN 60 FEET OF A STREET SIDEWALK, OR 90 FEET FOR BUILDINGS OVER 25,000 SQUARE FEET OF GROSS FLOOR AREA, AND CONNECTED TO THE STREET BY A CLEAR AND DIRECT WALKWAY; OR

II. PROVIDE A PROMINENT AND INVITING PRIMARY ENTRANCE THAT IS VISIBLE FROM THE STREET, CONNECTED BY A DIRECT WALKWAY TO THE STREET, AND HIGHLIGHTED BY TWO OF THE FOLLOWING:

(A) PORTICO, OVERHANG, CANOPY, OR SIMILAR PERMANENT FEATURE PROJECTING FROM THE WALL;

(B) RECESSED AND/OR PROJECTED ENTRANCE THAT COVERS AT LEAST 80 SQUARE FEET;

(C) ARCHES, PEAKED ROOF FORMS, TERRACING PARAPETS, OR OTHER CHANGE OF BUILDING ROOFLINE;

(D) CHANGES IN SIDING MATERIAL, OR DETAIL FEATURES SUCH AS TILEWORK, TO SIGNIFY THE ENTRANCE; OR

(E) ENTRANCE PLAZA, PATIO, OR  
SIMILAR COMMON PRIVATE OPEN  
SPACE.

C. BUILDINGS SHALL COMPLY WITH THE MAXIMUM  
SETBACKS ESTABLISHED IN SECTION 21.06.020,  
TABLES OF DIMENSIONAL STANDARDS, AND  
SUBSECTION 21.06.030C.5. THE MAXIMUM  
SETBACK MAY BE REDUCED OR ELIMINATED  
WITH THE CONCURRENCE OF THE DIRECTOR.]

H. *Standards for Mixed-Use Development in the B-3 District.*

\*\*\* \*\*\*

**3. Building Placement and Orientation. Buildings are subject to the building placement and orientation standards for mixed-use development in the B-1A/B-1B districts in subsection 21.04.030G.6. above.**

[3. BUILDING PLACEMENT AND ORIENTATION. BUILDINGS ARE SUBJECT TO THE BUILDING PLACEMENT AND ORIENTATION STANDARDS FOR MIXED-USE DEVELOPMENT IN THE B-1A/B-1B DISTRICTS IN SUBSECTION 21.04.030G.7. ABOVE.]

[4. REDUCED PARKING RATIOS. THE DEVELOPMENT IS ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT OF THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION 21.07.090F.6.]

4[5]. *Enhanced [street] sidewalk option*[OPTION]. An enhanced ~~[street]~~sidewalk *environment*[ENVIRONMENT] may be provided in lieu of required sidewalks and side perimeter landscaping, as provided in 21.07.060F.16[G.19][F.17].

\*\*\* \*\*\*

(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO No. 2017-58, § 1, 4-11-17; AO No. 2019-11, § 2, 2-12-19)

**21.04.070 Overlay Zoning Districts.**

\*\*\* \*\*\*

C. *Commercial Center Overlay.*

\*\*\* \*\*\*

[4. REDUCED PARKING RATIOS. INSTEAD OF THE FIVE PERCENT REDUCTION PROVIDED IN SUBSECTIONS 21.04.030G. AND H., ANY USE IS ELIGIBLE FOR A REDUCTION OF UP TO 10 PERCENT OF THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION 21.07.090F.6.]

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1           4[5]. *Uses prohibited.*  
2           \*\*\*       \*\*\*       \*\*\*

3           5[6]. *Dimensional standards.*

4  
5           a.     The minimum front setback is reduced to five feet  
6           for building elevations that comply with  
7           21.04.030G.7., *Building Placement and*  
8           *Orientation.*[A. THE MINIMUM FRONT  
9           SETBACK IS REDUCED TO FIVE FEET FOR  
10          BUILDING ELEVATIONS THAT COMPLY WITH  
11          21.04.030G.7., BUILDING PLACEMENT AND  
12          ORIENTATION.]

13  
14          b.[B.] Uses in the B-1B district are exempt from the gross  
15          floor area limitations of subsection 21.04.030C.2.b.,  
16          provided that an individual use occupies no more than  
17          60,000 square feet of gross floor area.

18  
19          6[7]. *Building entrances.*  
20          \*\*\*       \*\*\*       \*\*\*

21          7[8]. *Sidewalks and walkways.*  
22          \*\*\*       \*\*\*       \*\*\*

23  
24                   (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13)

25  
26     ~~**[Section 2. Anchorage Municipal Code Chapter 21.07, Development and**~~  
27     ~~**Design Standards, is hereby amended by adding a new Section 21.07.015,**~~  
28     ~~**Neighborhood Development Context Areas, to read as follows (the**~~  
29     ~~**remainder of the chapter is not affected and therefore not set out):**~~

30  
31     ~~**21.07           Development and Design Standards**~~  
32     ~~\*\*\*       \*\*\*       \*\*\*~~

33     ~~**21.07.015     Neighborhood Development Context Areas.**~~

34  
35     ~~**A.     Purpose. This section establishes Neighborhood Development**~~  
36     ~~**Context Areas that reflect the existing and desired**~~  
37     ~~**characteristics of Anchorage's older, urban neighborhoods as**~~  
38     ~~**identified in the Comprehensive Plan. The Neighborhood**~~  
39     ~~**Development Context Areas are distinguished from one another**~~  
40     ~~**and suburban parts of the Anchorage Bowl by their physical**~~  
41     ~~**and functional characteristics such as: street, alley, and block**~~  
42     ~~**patterns; building placement; how buildings and structures**~~  
43     ~~**relate to surrounding buildings and streets; the diversity,**~~  
44     ~~**distribution, and intensity of land uses; and availability of**~~  
45     ~~**transportation access alternatives such as pedestrian facilities**~~  
46     ~~**and public transportation. The areas mapped in this section**~~  
47     ~~**provide the basis for development regulations tailored to the**~~  
48     ~~**unique characteristics and strengths of these neighborhoods**~~  
49     ~~**and fulfills their role as recommended in the Comprehensive**~~



1                    ~~**Plan. These areas are provided relief from some generally**~~  
2                    ~~**applicable development and design standards in this chapter**~~  
3                    ~~**21.07 because of their unique characteristics.**~~

4  
5                    **B. Applicability**

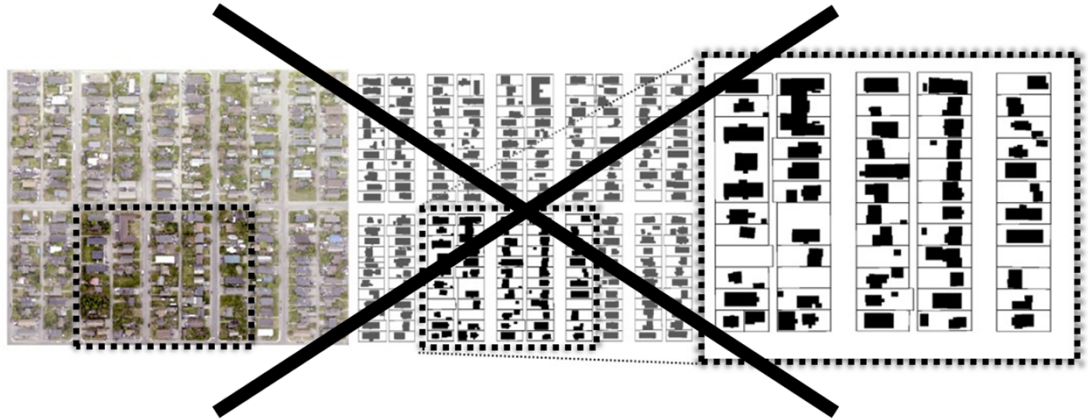
6  
7                    ~~**1. The neighborhood development context areas in this**~~  
8                    ~~**section apply as a basis for area-specific development**~~  
9                    ~~**standards in this chapter 21.07.**~~

10  
11                   ~~**2. The neighborhood development context areas in this**~~  
12                   ~~**section shall not be used as a basis to change the allowed**~~  
13                   ~~**uses or review and approval procedures of the**~~  
14                   ~~**underlying zoning district, district-specific dimensional**~~  
15                   ~~**standards, or any other standards of this title besides the**~~  
16                   ~~**development and design standards of chapter 21.07.**~~

17  
18                   ~~**3. The department shall provide and maintain maps**~~  
19                   ~~**showing the boundaries of the neighborhood**~~  
20                   ~~**development context areas at the parcel scale, in this title**~~  
21                   ~~**and in a publicly accessible location.**~~

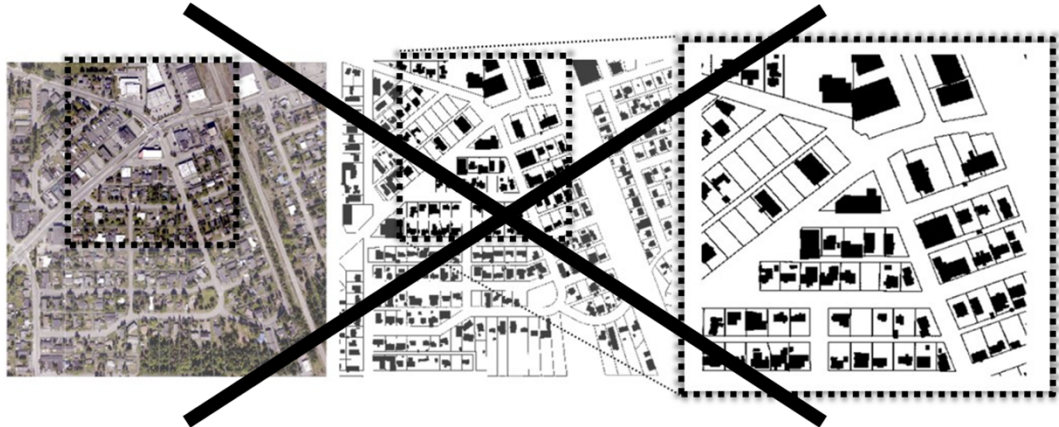
22  
23                   **C. Neighborhood Development Context Areas Established. The**  
24                   ~~**following neighborhood development context areas are**~~  
25                   ~~**established and delineated in maps 21.07-1 through 21.07-4 in**~~  
26                   ~~**subsection 21.07.015D. Each subsection below is organized to**~~  
27                   ~~**describe general character: street, block, and access patterns;**~~  
28                   ~~**building placement and location; and mix of uses. The**~~  
29                   ~~**descriptions below are intent statements providing a basis for**~~  
30                   ~~**area-specific development standards elsewhere in this chapter**~~  
31                   ~~**21.07.**~~  
32

1            1.    **Traditional Urban Neighborhood Context.**  
2            **The traditional urban neighborhood context includes**  
3            **Anchorage's original urban neighborhoods, such as**  
4            **Government Hill, South Addition, Fairview, and Mountain**  
5            **View. These neighborhoods are identified by the**  
6            **Comprehensive Plan, Land Use Plan Map as Traditional**  
7            **Neighborhood Design areas. Properties within the**  
8            **Downtown (DT) zoning districts are not included because**  
9            **they are addressed in chapter 21.11. Traditional urban**  
10           **neighborhood contexts feature a regular street grid of**  
11           **city blocks, which provide pedestrian and vehicular**  
12           **connections at frequent, regular intervals. Block and lot**  
13           **sizes are small and block shapes are consistent and often**  
14           **the same size. Sidewalks are present on most streets,**  
15           **with few exceptions such as in western Government Hill.**  
16           **There is a consistent presence of alleys. Buildings and**  
17           **residences typically have moderate front setbacks and**  
18           **orient to the street, with landscaping or pedestrian**  
19           **access comprising a majority of the frontage. Parking**  
20           **tends to be located to the side or rear of buildings. There**  
21           **is a balance of pedestrian, bicycle, public transit, and**  
22           **automobile reliance.**



**2. Edge Urban Neighborhood Context.**

**The edge urban neighborhood context includes properties generally identified in the Comprehensive Plan, Land Use Plan Map, as traditional neighborhood design areas in Anchorage's post-war era (1950s-1960s) neighborhoods, such as Spenard, North Star, Midtown, Rogers Park, Airport Heights, Russian Jack Park, and University Area. Parts of South Addition, Fairview, and other neighborhoods also match this context. Edge urban neighborhoods feature a variety of interconnected street systems with smaller lot and block sizes than later subdivisions in more suburban parts of the Anchorage Bowl. Some edge urban areas feature a regular street grid of city blocks and others have a more relaxed and irregular street grid. Some neighborhoods have alleys and sidewalks. Buildings typically have moderate to somewhat deeper front setbacks. Buildings tend to orient toward the public street, although some front setbacks are deep enough to allow for a mix of landscaping and parking. Some edge urban neighborhood context areas include transit-supportive development corridors with elevated levels of public transportation service and enhanced pedestrian facilities.**

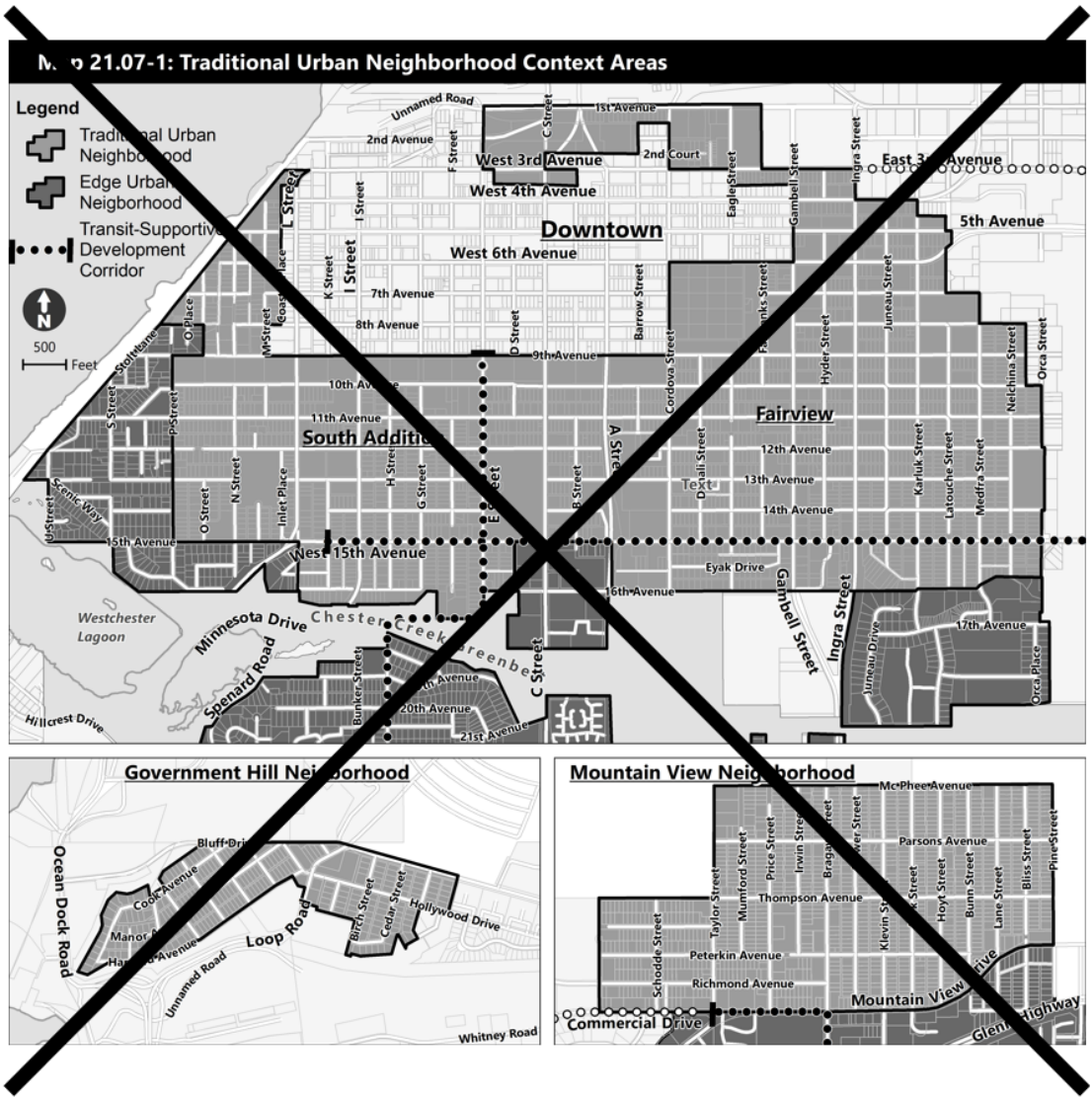


1                    **3.**     **Transit-Supportive Development Corridors.**  
2                    **Transit-supportive development corridors designated by**  
3                    **the Comprehensive Plan extend through and outward**  
4                    **from the edge urban neighborhood contexts. Transit-**  
5                    **supportive development corridors are a half-mile wide,**  
6                    **extending ¼-mile (approximately 5 to 10 minutes walking**  
7                    **distance) on each side of the public transit route. These**  
8                    **corridors feature a compact, pedestrian-friendly pattern**  
9                    **of land use development. These corridors have elevated**  
10                   **levels of municipal public transportation service with**  
11                   **frequent headways between buses and higher ridership**  
12                   **demand. These transit routes connect local and regional**  
13                   **town centers, city centers, and other service/employment**  
14                   **centers, such as the UMED District.**

15  
16                   **D.**     **Neighborhood Context Area Boundaries.**

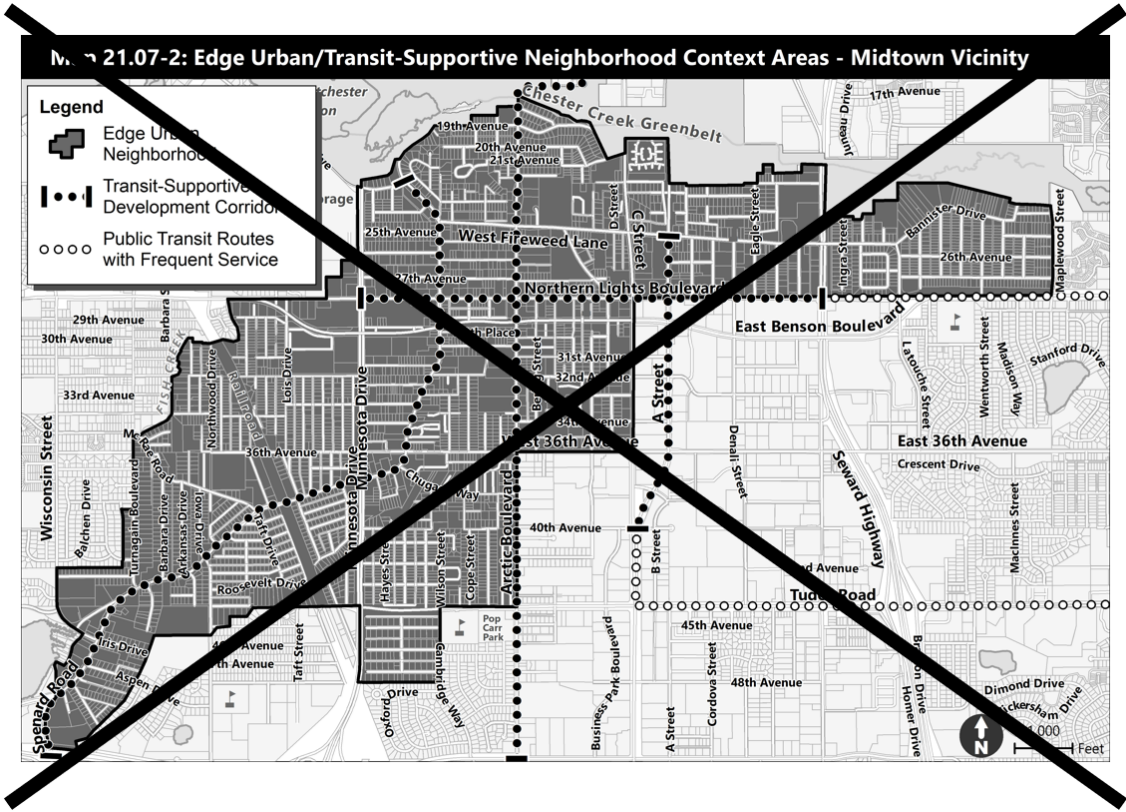
17  
18                   **1.**     **The boundaries of the neighborhood development**  
19                   **context areas are established in maps 21.07-1 through**  
20                   **21.07-4.**

21  
22                   **2.**     **The transit-supportive development corridors on maps**  
23                   **21.07-2, 21.07-3, and 21.07-4 shall include all properties**  
24                   **wholly or partially within ¼ mile of the street right-of-way**  
25                   **centerline of the linear transit corridor feature shown on**  
26                   **the map, except that: where a transit-supportive**  
27                   **development corridor overlaps with a traditional urban or**  
28                   **edge urban neighborhood context area, the traditional**  
29                   **urban or edge urban neighborhood regulations of this**  
30                   **title shall govern.]**

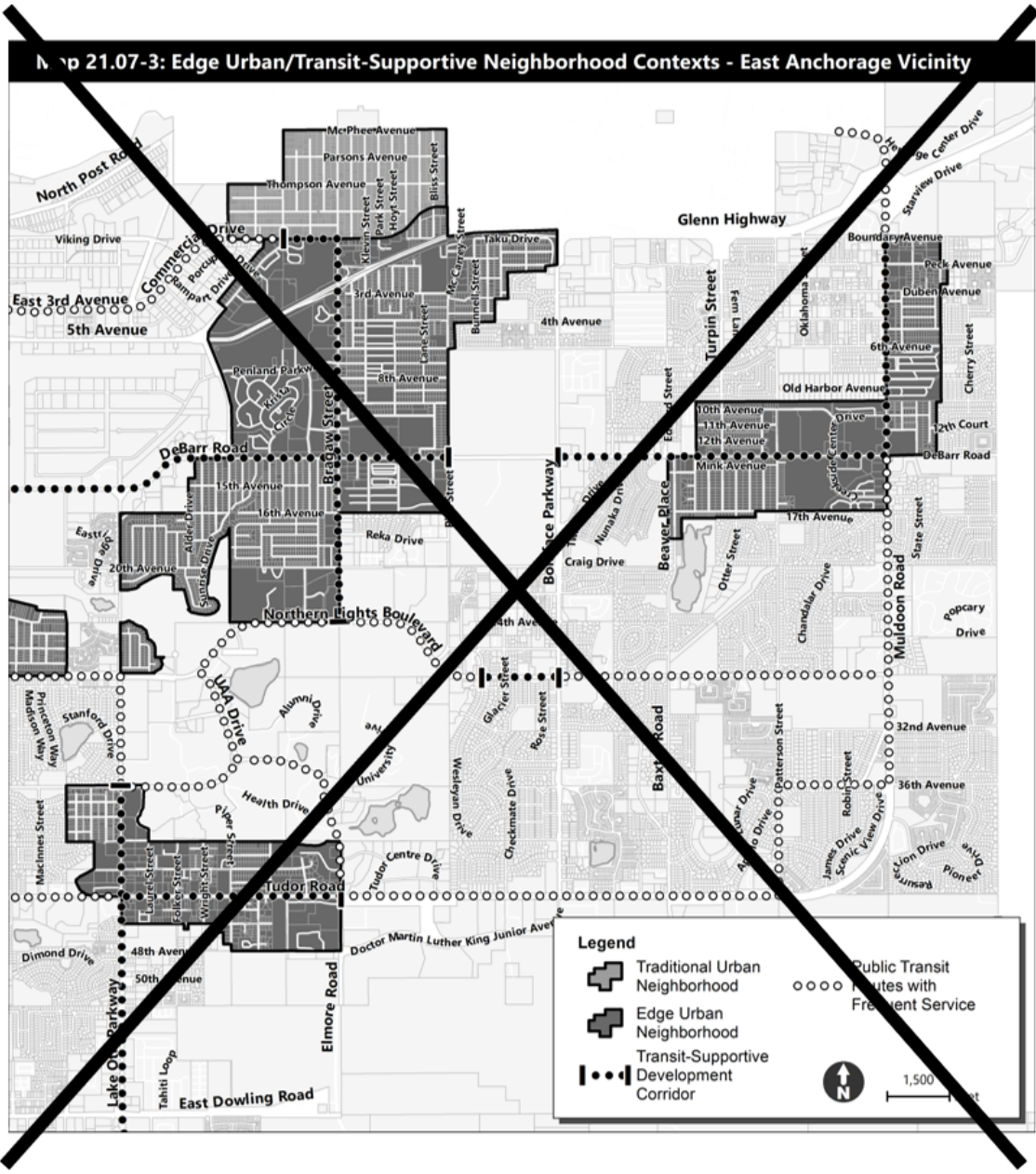


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### Map 21.07-4: Transit-Supportive Development Corridor - Lake Otis/Abbott/92nd



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1 **Section 2[3]**, Anchorage Municipal Code Chapter 21.07, Development and  
2 Design Standards, is hereby amended to read as follows (*the remainder of the*  
3 *chapter is not affected and therefore not set out*):

4  
5 **21.07 Development and Design Standards**

6 \*\*\* \*\*

7 **21.07.060 Transportation and Connectivity**

8 \*\*\* \*\*

9 (Note to Code Revisor: All amendments to subsection 21.07.060E. in AO 2022-  
10 80 are reversed by this S-Version to retain the current adopted code language of  
11 this subsection, as shown below.)

12  
13 E. *Standards for Pedestrian Facilities.*

14 \*\*\* \*\*

15 2. *Sidewalks.*

16  
17 a. All sidewalks shall be designed to comply with the  
18 standards of the Design Criteria Manual (DCM) and  
19 Municipality of Anchorage Standard Specifications  
20 (MASS) **[and shall be improved in accordance with**  
21 **subsection 21.08.050H]**.

22  
23 b. **[Street improvement projects and new streets in**  
24 **subdivisions i]**[I]In all class A zoning districts except  
25 for industrial districts[,] **[shall install]** sidewalks **shall**  
26 **be installed**[SHALL BE INSTALLED] on both sides of  
27 all streets (local, collector, arterial, public or private,  
28 including loop streets). Where indicated in the  
29 comprehensive plan, a pathway may replace a  
30 sidewalk on one side. **[Street improvement projects**  
31 **i]**[I]In industrial zoning districts, **[shall install]**[,] a  
32 sidewalk **shall be installed**[SHALL BE INSTALLED]  
33 on one side of all local streets, and on both sides of  
34 local streets if the new sidewalks would connect to  
35 existing sidewalks on both ends and the needed  
36 sidewalk length is no greater than one quarter mile.

37 \*\*\* \*\*

38 f. Development on lots along existing streets in class A  
39 zoning districts shall install sidewalks **[on all lot**  
40 **frontages abutting streets]** in the following  
41 situations:

42  
43 i. In R-4, R-4A, commercial **districts**  
44 [DISTRICTS], and DT **[zoning]** districts.

45 \*\*\* \*\*

46 4. *On-site pedestrian walkways.*

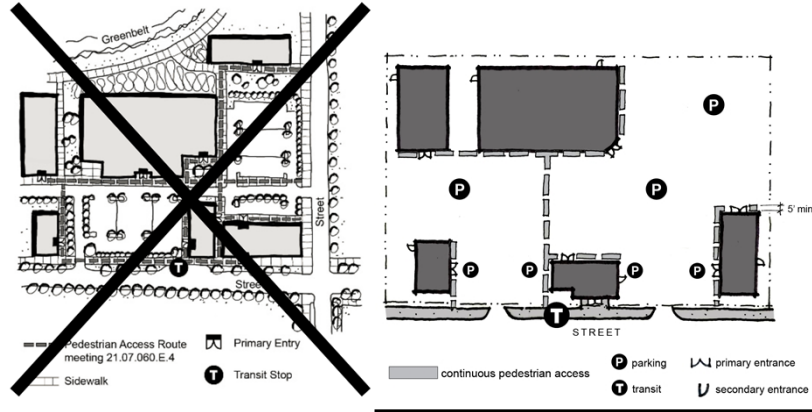
47 \*\*\* \*\*

48 b. *On-site pedestrian connections.*

49 \*\*\* \*\*

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- ii. All primary building entrances on a site shall be connected to the street by a convenient system of walkways. This includes multiple primary entrances into one building, and primary entrances into separate buildings on a site.



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(Note to code revisor: Delete new illustration from AO 2022-80 at left and retain current adopted illustration at right.)

\*\*\* \*\*

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- iv. The primary front entrance of a residential dwelling shall be connected to the street by a walkway as provided in i. through ii. above, or by the dwelling's individual driveway, or by a shared parking courtyard meeting 21.07.060G.20.**

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- c. **Walkway clear width and improvements.**

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- i. Walkway clear width.** The minimum width of a required pedestrian walkway shall be five feet of unobstructed clear width, **excluding vehicular overhang.** [EXCLUDING VEHICULAR OVERHANG,] except where otherwise stated in this title. A walkway that provides access to no more than four residential dwelling units may provide an unobstructed clear width of three feet.

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- ii. Walkways shall be improved in accordance with subsection 21.08.050H.**

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1            **F.**     **Pedestrian frontage standard.**

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3            **1.**     **Purpose. The pedestrian frontage standard promotes**  
4            **building placement and orientation toward neighborhood**  
5            **streets and sidewalks, facilitates pedestrian and bicycle**  
6            **access, supports lower parking utilization in urban**  
7            **neighborhood areas, and encourages alternative modes**  
8            **of transportation. Objectives include:**

9  
10           **a.**     **Define public streets and their associated frontage**  
11           **spaces as positive, useful features around which**  
12           **to organize and orient buildings in a manner that**  
13           **promotes pedestrian activity and connects to**  
14           **multiple modes of transportation.**

15  
16           **b.**     **Provide pedestrian access to building entries that**  
17           **is clearly defined, safe, and inviting for people of**  
18           **all abilities, and minimizes conflicts between**  
19           **pedestrians and vehicle parking.**

20  
21           **c.**     **Place active living spaces, entrances, and windows**  
22           **on street-facing building facades to improve the**  
23           **physical and visual connection from residences to**  
24           **the street.**

25  
26           **d.**     **Promote a safe, secure pedestrian and**  
27           **neighborhood environment through “eyes on the**  
28           **street” design elements, such as placement of**  
29           **windows, that foster opportunities for casual**  
30           **surveillance of the street and outwardly expressed**  
31           **proprietorship of the neighborhood.**

32  
33           **2.**     **Applicability. Subsection 3. applies to development in the**  
34           **traditional urban and edge urban neighborhood context**  
35           **areas established in 21.07.015. Subsection 4. applies to**  
36           **development in the other areas of the municipality.**  
37           **Exceptions and exemptions from this section 21.07.060F.**  
38           **include:**

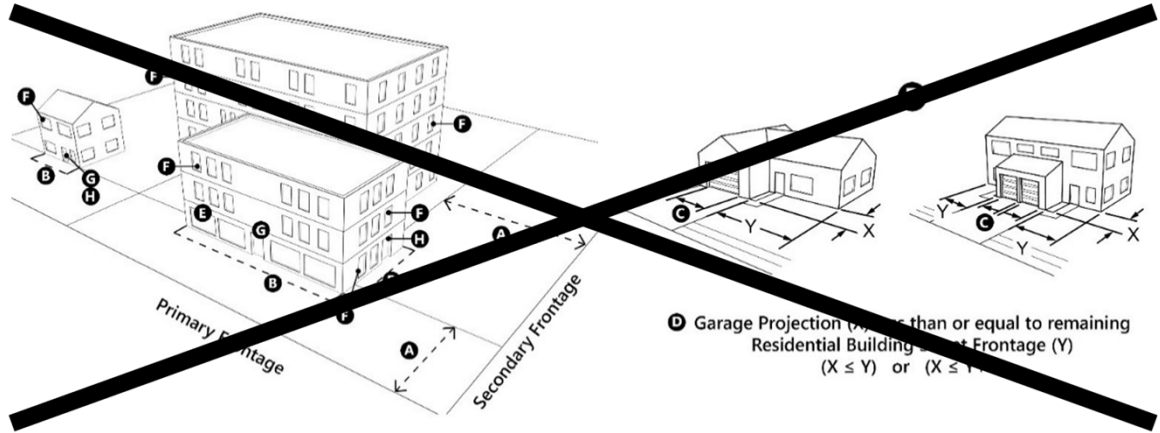
39  
40           **a.**     **Changes of use and other developments that**  
41           **comprise building modifications of less than 50**  
42           **percent of the total improvement value of the**  
43           **building(s) on the site.**

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45           **b.**     **Development in Girdwood, Downtown (DT), and**  
46           **Industrial zoning districts.**

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- ~~c.~~ **Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of 20,000 square feet or larger, or in Class B zoning districts.**
  - ~~d.~~ **Non-residential uses without habitable floor area, such as utility substations.**
  - ~~e.~~ **The director shall reduce the window area requirement by the amount the applicant demonstrates is needed to comply with a 5-star energy rating or an affordable housing (21.15.040) financing institution's energy rating requirements.**
  - ~~f.~~ **The director shall approve administrative relief provided the applicant demonstrates the adjustment is necessary to compensate for some practical difficulty of the site. The department shall keep record of the approved exception with written findings supporting the approval on file.**
  - ~~g.~~ **The Alternative Equivalent Compliance procedure set forth in subsection 21.07.010D. may be used to propose alternative means of complying with the intent of this section.]**
- ~~3.~~ **Standard for urban neighborhood contexts. The standards of table 21.07-2 apply to the primary frontage and at least one secondary frontage.]**



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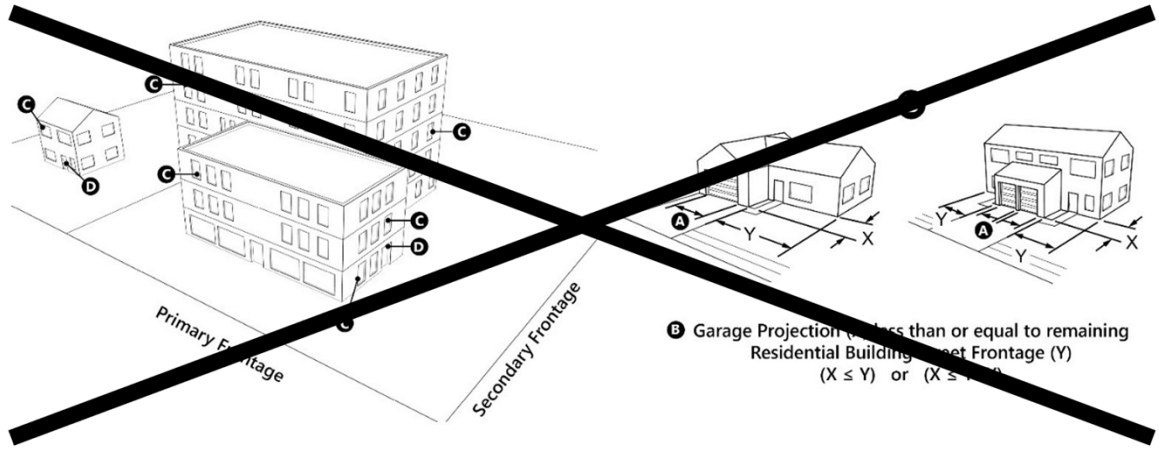
**[Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts]**

<b><u>Building and Parking Placement<sup>1</sup></u></b>	<b><u>Primary Frontage</u></b>	<b><u>Secondary Frontage</u></b>
<b><u>A. Maximum front setback (ground-floor only) in the R-4 and non-residential zoning districts<sup>1</sup></u></b>	<b><u>30 feet in Traditional Urban, and 60 feet in other areas</u></b>	<b><u>90 feet</u></b>
<b><u>B. Minimum percentage of the width of ground-floor street-facing building elevation(s) required to comply with max. setback in A<sup>1</sup></u></b>	<b><u>50%</u></b>	<b><u>50%</u></b>
<b><u>C. Residential garage entrance as a percentage of ground-floor street-facing building elevation width (maximum allowed)</u></b>	<b><u>50% in Traditional Urban, and 67% in other areas</u></b>	<b><u>67%</u></b>
<b><u>D. Maximum distance a garage may project out in front of the rest of the street-facing residential building elevation</u></b>	<b><u>No more than the width of the non-garage portion of the street-facing building elevation</u></b>	
<b><u>Building Orientation and Access</u></b>	<b><u>Primary Frontage</u></b>	<b><u>Secondary Frontage</u></b>
<b><u>E. Minimum percentage of a non-residential ground-floor, street-facing building elevation required to be windows or primary entrances<sup>2,3</sup></u></b>	<b><u>30% in Traditional Urban, and 20% in other areas</u></b>	<b><u>15%</u></b>
<b><u>F. Minimum percentage of residential and upper-floor non-residential street-facing elevation required to be windows or primary entrances<sup>3</sup></u></b>	<b><u>15% in Traditional Urban, and 10% in other areas</u></b>	<b><u>10%</u></b>
<b><u>G. At least one primary entrance located within max. setback in A</u></b>	<b><u>Required</u></b>	<b><u>Not Required</u></b>
<b><u>H. Front primary entrances for residential dwellings meet 21.07.060G.15., Covered, Visible Residential Entrance</u></b>	<b><u>Required</u></b>	
<b><u>I. Minimum number of pedestrian amenities from 21.07.060G. in addition to 21.07.060G.15.</u></b>	<b><u>2 pedestrian amenities required per multifamily, mixed-use, townhouse, or group housing development</u></b>	
<p><sup>1</sup> Generally applicable standards and exceptions for maximum setbacks are provided in 21.06.030C.5.</p> <p><sup>2</sup> Non-residential ground-floor windows used to meet these percentages shall be visual access windows (21.15.040) and have a sill height of no more than four feet above finished grade. Ground-floor wall area is defined in 21.15.040.</p> <p><sup>3</sup> Rules of measurement for calculating window area as a percentage of building wall area are provided in 21.15.020P.]</p>		

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**[4. Standard for other areas and developments in the municipality. The standards of table 21.07-3 apply to the primary frontage and at least one secondary frontage.]**



4

**[Table 21.07-3: Pedestrian Frontage Standard—Other Areas and Developments]**

<b><u>Building and Parking Placement</u></b>	<b><u>Primary Frontage</u></b>	<b><u>Secondary Frontage</u></b>
<b><u>A. Residential garage entrance as a percentage of ground-floor street-facing building elevation width (maximum allowed)</u></b>	<b><u>67%</u></b>	<b><u>75%</u></b>
<b><u>B. Maximum distance a garage may project out in front of the rest of a street-facing residential building elevation</u></b>	<b><u>No more than the width of the non-garage portion of the street-facing building elevation</u></b>	
<b><u>Building Orientation and Access</u></b>	<b><u>Primary Frontage</u></b>	<b><u>Secondary Frontage</u></b>
<b><u>C. Minimum percentage of residential street-facing building elevation wall areas required to be windows or primary entrances<sup>1</sup></u></b>	<b><u>Window(s) required, with no minimum percentage</u></b>	
<b><u>D. Front primary entrances to residential dwellings meet 21.07.060G.15., Covered, Visible Residential Entrance.</u></b>	<b><u>Required</u></b>	
<b><u>E. Minimum number of pedestrian amenities from 21.07.060G. in addition to 21.07.060G.15.</u></b>	<b><u>1 pedestrian amenity required per multifamily, mixed-use, townhouse, or group housing development</u></b>	
<b><u><sup>1</sup>Rules of measurement for calculating window area as a percentage of building wall area are provided in 21.15.020P.</u></b>		

5

1 (Note to Code Revisor: The amendments to subsection 21.07.060F., Pedestrian  
2 Amenities, in AO 2022-80 are reversed by this S-Version to retain the current  
3 adopted code language of 21.07.060F., as shown below. Exception: The S-  
4 Version moves forward with the AO 2022-80 deletion of subsection  
5 21.07.060F. 15., Bicycle Parking Facilities.)

6  
7 **F[G][F]. Pedestrian amenities [menu].**

- 8  
9 1. *Purpose.* The purpose of this section is to define and provide  
10 standards for pedestrian amenities that may be required[,] **or**  
11 [OR ]included in a menu of choices to meet a requirement, or  
12 listed as a special feature that can count toward a bonus  
13 incentive anywhere in this title. For example, another section  
14 of this title may list a pedestrian amenity as a special feature  
15 for which bonus floor area **[or a reduction in required**  
16 **parking]** may be granted. The standards contained in this  
17 section give predictability for applicants, decision-makers, and  
18 the community for the minimum acceptable standards for  
19 pedestrian amenities. It also ensures the amenities will  
20 improve and enhance the community to the benefit of all, and  
21 respond to the northern latitude climate. This title provides  
22 flexibility to encourage and allow for creativity and unique  
23 situations through the alternative equivalent compliance and  
24 minor modifications process.  
25  
26 2. *Applicability.* Pedestrian amenities shall meet the minimum  
27 standards of this section in order to be credited toward a  
28 requirement, menu choice, or **as a special feature bonus**[AS  
29 A SPECIAL FEATURE BONUS] incentive of this title, except  
30 where specifically provided otherwise in this title. The  
31 alternative equivalent compliance procedure set forth in  
32 subsection 21.07.010D. may be used to propose alternative  
33 means of complying with the standards of this **[sub]**section  
34 21.07.060**F[G][F]. [The standards of this section apply**  
35 **only to pedestrian amenities that are counted toward a**  
36 **requirement, menu choice, or incentive under this title. It**  
37 **does not apply to amenities that do not receive credit for**  
38 **requirements in title 21.]**  
39

40 (Note to Code Revisor: The S-Version's revisions to the following subsection  
41 21.07.060F. 3., Walkway, restore the current code text that the original ordinance  
42 had deleted.)  
43

- 44 3. **Walkway. A walkway is a surface that connects two**  
45 **points for pedestrian use, as defined in chapter 21.15. A**  
46 **walkway may be in a publicly dedicated pedestrian**  
47 **easement. Examples include pedestrian connections**  
48 **within one development site, mid-block, between**

1                                    **subdivisions, or leading from streets to public amenities,**  
2                                    **such as schools or parks.**

3  
4                    **a.    A walkway shall have a minimum unobstructed**  
5                    **clear width of five feet, except where otherwise**  
6                    **stated in this title. A walkway that provides access**  
7                    **to no more than four residential dwelling units may**  
8                    **have an unobstructed clear width of three feet.**

9  
10                   **b.    Walkways shall be improved in accordance with**  
11                   **subsection 21.08.050H.**

12  
13                   [3.    WALKWAY. A WALKWAY IS A SURFACE THAT  
14                          CONNECTS TWO POINTS FOR PEDESTRIAN USE, AS  
15                          DEFINED IN CHAPTER 21.15. A WALKWAY MAY BE IN A  
16                          PUBLICLY DEDICATED PEDESTRIAN EASEMENT.  
17                          EXAMPLES INCLUDE PEDESTRIAN CONNECTIONS  
18                          WITHIN ONE DEVELOPMENT SITE, MID-BLOCK,  
19                          BETWEEN SUBDIVISIONS, OR LEADING FROM STREETS  
20                          TO PUBLIC AMENITIES, SUCH AS SCHOOLS OR PARKS.]

21  
22                          [A.    A WALKWAY SHALL HAVE A MINIMUM  
23                          UNOBSTRUCTED CLEAR WIDTH OF FIVE FEET,  
24                          EXCEPT WHERE OTHERWISE STATED IN THIS  
25                          TITLE. A WALKWAY THAT PROVIDES ACCESS TO  
26                          NO MORE THAN FOUR RESIDENTIAL DWELLING  
27                          UNITS MAY HAVE AN UNOBSTRUCTED CLEAR  
28                          WIDTH OF THREE FEET.]

29  
30                          [B.    WALKWAYS SHALL BE IMPROVED IN  
31                          ACCORDANCE WITH SUBSECTION 21.08.050H.]

32  
33                   *(Note to Code Revisor: The S-Version's revisions to the following subsection*  
34                   *21.07.060F.4., Primary Pedestrian Walkway, restore all the current code text of*  
35                   *this subsection that AO 2022-80 amended.)*



1                    **4[3][4].**        **Primary Pedestrian[Enhanced on-site]** [PRIMARY  
2 REDESTRIAN] **walkway.** **A primary pedestrian[An**  
3 **enhanced on-site]**[A PRIMARY PEDESTRIAN] walkway is  
4 intended to provide an **unobstructed clear width of at least**  
5 **eight feet[option for applicants to receive credit for**  
6 **exceeding the minimum development standards for**  
7 **walkways and improving pedestrian convenience,**  
8 **comfort, and safety on the site. Enhanced on-site**  
9 **walkways provide additional width]** [UNOBSTRUCTED  
10 CLEAR WIDTH OF AT LEAST EIGHT FEET] for pedestrian  
11 movement **with additional space incorporating features**  
12 **along the walkway such as storefront sidewalk space,**  
13 **room for residential stoops or building foundation**  
14 **plantings,**[WITH ADDITIONAL SPACE INCORPORATING  
15 FEATURES ALONG THE WALKWAY SUCH AS  
16 STOREFRONT SIDEWALK SPACE, ROOM FOR  
17 RESIDENTIAL STOOPS OR BUILDING FOUNDATION  
18 PLANTINGS,] and peripheral space that accommodates  
19 landscaping, furniture, and utilities. **As established generally**  
20 **in subsection F.1 and F. 2 above, the standards of this**  
21 **subsection apply only where the specific term "primary**  
22 **pedestrian walkway" is listed as a requirement, menu**  
23 **choice, or special feature that counts toward a bonus.**  
24 **This subsection is not a generally applicable requirement**  
25 **for other large walkways.** [AS ESTABLISHED GENERALLY  
26 IN SUBSECTION F.1 AND F. 2 ABOVE, THE STANDARDS  
27 OF THIS SUBSECTION APPLY ONLY WHERE THE  
28 SPECIFIC TERM "PRIMARY PEDESTRIAN WALKWAY" IS  
29 LISTED AS A REQUIREMENT, MENU CHOICE, OR  
30 SPECIAL FEATURE THAT COUNTS TOWARD A BONUS.  
31 THIS SUBSECTION IS NOT A GENERALLY APPLICABLE  
32 REQUIREMENT FOR OTHER LARGE WALKWAYS.]

33  
34                    a.        **A primary pedestrian walkway shall be developed**  
35 **as a continuous pedestrian route extending for at**  
36 **least 50 feet.**[A PRIMARY PEDESTRIAN WALKWAY  
37 SHALL BE DEVELOPED AS A CONTINUOUS  
38 PEDESTRIAN ROUTE EXTENDING FOR AT LEAST  
39 50 FEET.]

40  
41                    b.[B.] **A primary pedestrian[An enhanced on-site]** [A  
42 PRIMARY PEDESTRIAN] walkway shall have **[a**  
43 **pedestrian movement zone with a continuous,]**[N]  
44 **an unobstructed [walkway]** clear width of at least eight  
45 feet**[, or six feet where providing access only to**  
46 **four or fewer residential dwelling units].** Where  
47 adjacent to a ground-floor building elevation it shall  
48 also have a **sidewalk storefront or**[SIDEWALK  
49 STOREFRONT OR] building interface zone a minimum

1 of two feet in width for ~~building~~ foundation  
2 landscaping or **three feet in width of**  
3 **sidewalk**[THREE FEET IN WIDTH OF SIDEWALK]  
4 space for opening doors or seating and transition  
5 pedestrian spaces. In addition ~~to the pedestrian~~  
6 ~~movement zone and any building interface zone~~,  
7 ~~the enhanced on-site walkway shall have~~ a buffer  
8 space of at least ~~four~~**two**[FOUR] feet in width ~~where~~  
9 ~~abutting motor vehicle parking lots, circulation~~  
10 ~~aisles, or driveways~~ **shall be incorporated as part**  
11 **of the walkway when abutting any street or vehicle**  
12 **area, to**[SHALL BE INCORPORATED AS PART OF  
13 THE WALKWAY WHEN ABUTTING ANY STREET OR  
14 VEHICLE AREA,]~~the buffer space shall~~[TO]  
15 accommodate **street trees**,[STREET  
16 TREES,]landscaping beds, ~~fencing or bollards,~~ light  
17 poles, utilities, benches, and other **objects to be kept**  
18 **clear of the walkway**[furnishings][OBJECTS TO BE  
19 KEPT CLEAR OF THE WALKWAY].  
20

21 **c.[b][C].** ~~A minimum of two pedestrian features as~~  
22 ~~defined by this title (21.15.040) shall be provided~~  
23 ~~along the enhanced walkway, with at least one for~~  
24 ~~every 50 feet of the walkway length] At least one~~  
25 ~~pedestrian feature as defined by this title shall be~~  
26 ~~provided for every 50 feet of length along a primary~~  
27 ~~pedestrian walkway~~[AT LEAST ONE PEDESTRIAN  
28 FEATURE AS DEFINED BY THIS TITLE SHALL BE  
29 PROVIDED FOR EVERY 50 FEET OF LENGTH  
30 ALONG A PRIMARY PEDESTRIAN WALKWAY].  
31

32 **d.** **A primary pedestrian walkway shall be illuminated**  
33 **with pedestrian scale lighting.** [D. A PRIMARY  
34 PEDESTRIAN WALKWAY SHALL BE ILLUMINATED  
35 WITH PEDESTRIAN SCALE LIGHTING.]  
36

37 **e[c][E].** **A primary pedestrian**[Enhanced on-site] [A  
38 PRIMARY PEDESTRIAN] walkway[s] shall **provide**  
39 **continuous,** directly[LY] connect[ions from building  
40 **primary entrance(s)**] to surrounding public streets  
41 and sidewalks[,], and be publicly accessible **at all**  
42 **times**[or available to all residents of the  
43 **development]** [AT ALL TIMES].  
44

45 \*\*\* \*\*

46 (Note to Code Revisor: Delete Subsection 21.07.060F.15., Bicycle Parking  
47 Facilities, as shown below. Re-number subsequent subsections of 21.07.060F.)  
48

49 [15. BICYCLE PARKING FACILITIES.

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- A. REQUIRED BICYCLE PARKING OR A SIGN LEADING THERETO SHALL BE LOCATED IN AN AREA VISIBLE FROM A PRIMARY ENTRANCE AREA AND NO FARTHER FROM A PRIMARY ENTRANCE THAN THE CLOSEST MOTOR VEHICLE PARKING SPACE, NOT INCLUDING DESIGNATED ACCESSIBLE PARKING, CARPOOL, OR VANPOOL SPACES. IT MAY ALSO BE LOCATED INSIDE THE BUILDING SERVED, IN A LOCATION THAT IS EASILY ACCESSIBLE FOR BICYCLES.
- B. A REQUIRED BICYCLE PARKING SPACE SHALL INCLUDE A SECURELY FIXED STRUCTURE THAT ALLOWS THE BICYCLE WHEEL AND FRAME TO BE LOCKED TO THE FACILITY, AND THAT SUPPORTS THE BICYCLE FRAME IN A STABLE POSITION WITHOUT DAMAGE TO THE BICYCLE, OR SHALL BE IN A BICYCLE LOCKER, LOCKABLE BICYCLE ENCLOSURE, OR LOCKABLE ROOM.
- C. A REQUIRED BICYCLE PARKING SPACE SHALL BE A MINIMUM OF SIX FEET LONG AND TWO FEET WIDE.
- D. THE SURFACING OF BICYCLE PARKING FACILITIES SHALL BE DESIGNED AND MAINTAINED TO BE CLEAR OF MUD AND SNOW.
- E. BICYCLE PARKING SHALL NOT OBSTRUCT PEDESTRIAN WALKWAYS, BUILDING ACCESS, OR USE AREAS.]

**[14. ~~Separated walkway to the street. The development shall connect the building primary entrances to the street with a clear and direct walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles. The minimum clear width portion of the walkway shall be separated from the parking facility by at least five feet.~~**

**15. ~~Covered, visible residential entrance. A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings from the street. The entrance shall meet the following standards:~~**

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- ~~a.~~ The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.
- ~~b.~~ The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle parking by vertical separation or a change in surfacing material.
- ~~c.~~ The building entrance shall also be visible (via an unobstructed line of sight) from a street or face a common private open space (21.07.030) that is visible from a street.

~~16.~~ Enhanced primary entrance. An enhanced building entry is intended to provide an option for applicants to receive credit for a prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary entrance shall incorporate at least three of the following features:

- ~~a.~~ Outdoor sheltering roof feature projecting from the building façade such as an overhang, portico, canopy, marquee with an inside dimension of at least 16 square feet;
- ~~b.~~ Recessed and/or projected entrance or other building wall modulation with projections or recesses in the building wall plane;
- ~~c.~~ Changes in the building's main roofline such as arches, peaked roof forms, or terracing parapets;
- ~~d.~~ Changes in siding material or exterior finishes, or façade detail features such as tilework that emphasize the entrance;
- ~~e.~~ Entrance plaza, patio, or similar common private open space;
- ~~f.~~ Landscaping not otherwise required by this title, such as integrated planters, landscape accent lighting, or special paving treatments; or
- ~~g.~~ One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.

1                    **17.**    **Site Entry Feature. Highlight and define a pedestrian**  
2                    **entrance to a development site using three or more of the**  
3                    **following elements:**

4  
5                    **a.**    **Landscape treatment with seasonal color and**  
6                    **trees, which clearly distinguishes and highlights**  
7                    **the site entry.**

8  
9                    **b.**    **Plaza or courtyard as described in subsection**  
10                   **21.07.060G.5.**

11  
12                   **c.**    **Identifying building primary entrance form**  
13                   **including a covered entry, when the primary**  
14                   **entrance is within 50 feet of the site entrance.**

15                   **d.**    **Special paving, pedestrian scale lighting, and/or**  
16                   **bollards.**

17  
18                   **e.**    **Ornamental gate and/or fence.]**  
19

20 *(Note to Code Revisor: The S-Version's revisions to the following subsection*  
21 *21.07.060F.16., Pedestrian Interactive Use, restore all the current code text of this*  
22 *subsection that AO 2022-80 amended, except to re-number the subsection from*  
23 *F.16. to F.15.)*

24  
25                   **15[18]**[16].    *Pedestrian-interactive* **use[building]** [USE]. A  
26                   pedestrian-interactive **use[building]** [USE] is intended to  
27                   provide **ground-floor[interior]** [GROUND-FLOOR] spaces  
28                   that **strongly**[STRONGLY] engage the sidewalk with street-  
29                   facing windows and entrances, **feature[and]**[FEATURE]  
30                   activities and services that support neighborhood residents,  
31                   and generally contribute to the **activity level and quality of**  
32                   **the]** pedestrian-**oriented**[-ORIENTED] environment **of the**  
33                   **neighborhood or district]**. The standards that follow apply  
34                   where the term "pedestrian-interactive **use[building]** [USE]"  
35                   is listed in this title as a requirement, special feature for a  
36                   bonus, or a menu choice.

37  
38                   **a.**    **A pedestrian-interactive use shall be any of the**  
39                   **following uses that are permitted in the district:**  
40                   **retail and pet services; financial institution**  
41                   **providing banking services open to the public with**  
42                   **at least one employee on site; food or beverage**  
43                   **service; personal service; cultural facility; or the**  
44                   **frontage of entryways or stairways through which**  
45                   **such uses are principally accessed;**

46  
47                   **b.**    **Retail sales uses that are permitted in the district**  
48                   **shall be considered pedestrian-interactive uses,**

1 except for the following types of retail sales uses:  
2 fueling station; building materials store.

3  
4 c. The following uses supporting residential  
5 neighborhood and housing development are also  
6 considered pedestrian-interactive uses when  
7 permitted in the district: residential dwellings with  
8 individual front entries along the street; elementary  
9 school; middle or high school; health services;  
10 child care center.

11  
12 [A. A PEDESTRIAN-INTERACTIVE USE SHALL BE ANY  
13 OF THE FOLLOWING USES THAT ARE PERMITTED  
14 IN THE DISTRICT: RETAIL AND PET SERVICES;  
15 FINANCIAL INSTITUTION PROVIDING BANKING  
16 SERVICES OPEN TO THE PUBLIC WITH AT LEAST  
17 ONE EMPLOYEE ON SITE; FOOD OR BEVERAGE  
18 SERVICE; PERSONAL SERVICE; CULTURAL  
19 FACILITY; OR THE FRONTAGE OF ENTRYWAYS  
20 OR STAIRWAYS THROUGH WHICH SUCH USES  
21 ARE PRINCIPALLY ACCESSED;

22  
23 B. RETAIL SALES USES THAT ARE PERMITTED IN  
24 THE DISTRICT SHALL BE CONSIDERED  
25 PEDESTRIAN-INTERACTIVE USES, EXCEPT FOR  
26 THE FOLLOWING TYPES OF RETAIL SALES USES:  
27 FUELING STATION; BUILDING MATERIALS STORE.

28  
29 C. THE FOLLOWING USES SUPPORTING  
30 RESIDENTIAL NEIGHBORHOOD AND HOUSING  
31 DEVELOPMENT ARE ALSO CONSIDERED  
32 PEDESTRIAN-INTERACTIVE USES WHEN  
33 PERMITTED IN THE DISTRICT: RESIDENTIAL  
34 DWELLINGS WITH INDIVIDUAL FRONT ENTRIES  
35 ALONG THE STREET; ELEMENTARY SCHOOL;  
36 MIDDLE OR HIGH SCHOOL; HEALTH SERVICES;  
37 CHILD CARE CENTER.]

38  
39 d[a][D]. A pedestrian-interactive use[building] [USE]  
40 shall provide a primary entrance facing the street.  
41 Entrances at building corners facing a street may be  
42 used to satisfy this requirement.

43  
44 e[b][E]. A pedestrian-interactive use[building] [USE]  
45 shall contain habitable floor area at least 24 feet deep  
46 extending along [a minimum of 50 percent of ]the  
47 full[FULL] length of the ground-floor, street-facing  
48 building elevation [in Urban Neighborhood  
49 Contexts], [and 30 percent elsewhere] allowing for

[ALLOWING FOR][~~]. **The habitable floor area may include**~~ pedestrian **and vehicle**[AND VEHICLE] entrances, entry lobbies or atriums, and stairwells.

**f.** **A pedestrian-interactive use shall comply with subsection 21.06.030C.5., Maximum Setbacks, but the exceptions of subsection 21.06.030C.5.d. shall not be available.**

[F. A PEDESTRIAN-INTERACTIVE USE SHALL COMPLY WITH SUBSECTION 21.06.030C.5., MAXIMUM SETBACKS, BUT THE EXCEPTIONS OF SUBSECTION 21.06.030C.5.D. SHALL NOT BE AVAILABLE.]

**g[e]**[G]. Street-facing **ground-floor**[GROUND-FLOOR] wall areas of a **pedestrian-interactive[non-residential]** [PEDESTRIAN-INTERACTIVE] use shall be **67[50]**[67] percent visual access windows, **except that such wall areas for dwellings shall be at least 20 percent visual access windows. [or primary entrances on the ground floor, and 20 percent windows above the ground floor.]** [, EXCEPT THAT SUCH]

**d.** **Street-facing** [WALL AREAS FOR] **residential uses**[DWELLINGS] **shall be at least 20 percent** [VISUAL ACCESS] **windows**[~~or primary entrances in Urban Neighborhood Contexts and 10 percent in other areas.~~]

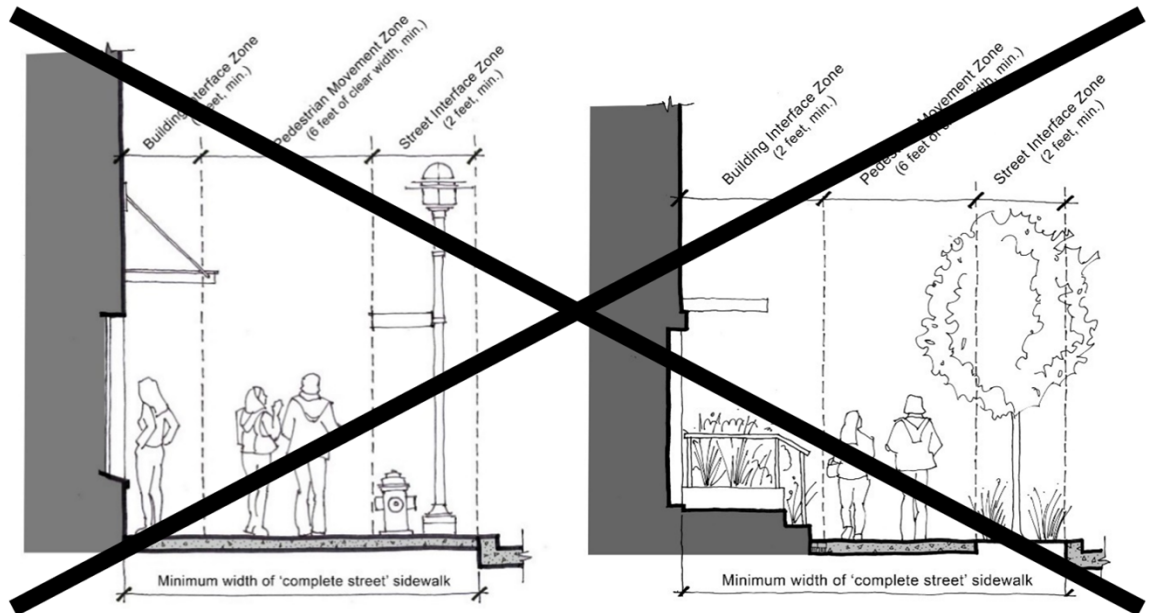
**h[e]**[H]. Where a building has three or more street frontages, these criteria apply along only two of the frontages.

*(Note to Code Revisor: The S-Version's revisions to the following subsection 21.07.060F.17., Enhanced Sidewalk Option, restore all the current code text of this subsection that AO 2022-80 amended, except to re-number the subsection from F.17. to F.16.)*

**16[19]**[17]. Enhanced **street** sidewalk **option**[OPTION]. An enhanced **street** sidewalk **is intended to provide an option for applicants to receive credit for exceeding the minimum development standards for sidewalks. An enhanced street sidewalk** promotes sidewalk widening and streetscape enhancements to support higher levels of pedestrian activity[~~, comfort, and safety in the district~~] **and access in mixed-use developments**[AND ACCESS IN MIXED-USE DEVELOPMENTS]. An enhanced **street**

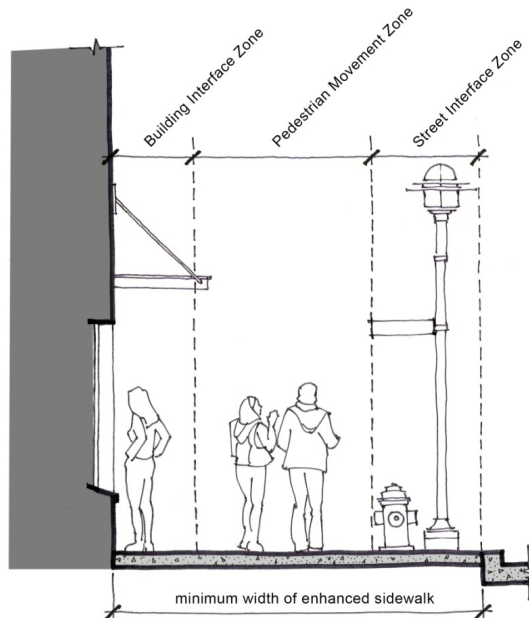
sidewalk **environment**[ENVIRONMENT] with “**main complete**”[MAIN] street” **[(21.15.040)]** style amenities may be provided in lieu of required site perimeter landscaping **where it is logical to support a pedestrian zone**[WHERE IT IS LOGICAL TO SUPPORT A PEDESTRIAN ZONE] as determined through an administrative site plan review, and subject to the following:

- a. The **enhanced street** sidewalk **width**[cross-section] [WIDTH] shall be at least 12 feet **wide**, and include a pedestrian movement zone, building interface zone, and street interface zone **[(21.15.040)]**. **The pedestrian movement zone shall have a clear width of at least six feet.** The street interface zone shall be at least **two feet wide from back of curb, and** four feet wide along major arterials. **Where a building adjoins the sidewalk, there shall be a building interface zone at least two feet wide** **At least part of the development’s frontage along the enhanced sidewalk shall feature a principal building with a 20-foot maximum setback in compliance with subsection 21.06.030C.5.** [AT LEAST PART OF THE DEVELOPMENT’S FRONTAGE ALONG THE ENHANCED SIDEWALK SHALL FEATURE A PRINCIPAL BUILDING WITH A 20-FOOT MAXIMUM SETBACK IN COMPLIANCE WITH SUBSECTION 21.06.030C.5].





1 (Note to code revisor: Delete the pair of illustrations above and restore the current  
2 adopted illustration and caption as shown below.)



Enhanced ~~Street~~ Sidewalk[s] in a[A] Commercial ~~and Residential~~ Setting[s]

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i. **A public use easement shall be recorded for any part of the designated sidewalk to be located within the subject parcel**

ii. **Physical obstructions within the sidewalk's building interface zone, such as landscaping, entry stoops, or seating, shall extend no more than two feet into the minimum required 12 foot width, so that at least ten feet remain.**

[I. A PUBLIC USE EASEMENT SHALL BE RECORDED FOR ANY PART OF THE DESIGNATED SIDEWALK TO BE LOCATED WITHIN THE SUBJECT PARCEL.

II. PHYSICAL OBSTRUCTIONS WITHIN THE SIDEWALK'S BUILDING INTERFACE ZONE, SUCH AS LANDSCAPING, ENTRY STOOPS, OR SEATING, SHALL EXTEND NO MORE THAN TWO FEET INTO THE MINIMUM REQUIRED 12 FOOT WIDTH, SO THAT AT LEAST TEN FEET REMAIN.]

b. The enhanced **street** sidewalk shall provide at least two-thirds the number of trees and shrubs **that would**

1 ~~otherwise have been~~ required for site perimeter  
2 landscaping.

3  
4 c. The enhanced **[street]** sidewalk may be placed wholly  
5 or in part within a right-of-way, subject to approval of  
6 the traffic engineer and municipal engineer.

7  
8 i. The enhanced **[street]** sidewalk shall be subject  
9 to the applicable requirements of title 24,  
10 including sections 24.30.020., *Permit to use*  
11 *Public Places*, and 24.90, *Encroachment*  
12 *Permit*.

13  
14 **ii. Improvements within the public right-of-way**  
15 **shall be consistent with the DCM and MASS.**

16 [II. IMPROVEMENTS WITHIN THE PUBLIC  
17 RIGHT-OF-WAY SHALL BE CONSISTENT  
18 WITH THE DCM AND MASS.]

19  
20 **iii[III].** Existing improvements that meet the standards  
21 of the enhanced **[street]** sidewalk may be  
22 counted towards the requirements of this  
23 section, subject to approval by the director.

24  
25 **iv[III][IV].** The owner shall maintain landscaping  
26 and amenities for the enhanced **[street]**  
27 sidewalk within the right-of-way, **and comply**  
28 **with the provisions for removal of snow and**  
29 **ice in AMC 24.80.090, 100, and 110**[, AND  
30 COMPLY WITH THE PROVISIONS FOR  
31 REMOVAL OF SNOW AND ICE IN AMC  
32 24.80.090, 100, AND 110].

33  
34 **v[IV][V].** Where the right-of-way is not adequate  
35 or cannot be configured to accommodate the  
36 enhanced **[street]** sidewalk, then the  
37 development shall be set back from the street  
38 frontage as necessary to accommodate part of  
39 the improvements within the property. **[A public**  
40 **use easement shall be recorded for any part**  
41 **of the designated sidewalk width to be**  
42 **located within the subject parcel.]**

43  
44 **17[20][18].** *Parking courtyard.*  
45 \*\*\* \*\*

46 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2015-82, 7-28-15;  
47 AO 2015-100, 10-13-15; AO 2017-55, 4-11-17; AO 2020-38, 4-28-20)

1  
2 **21.07.090 Off-Street Parking and Loading**  
3

4 A. *Purpose.* This section **[ESTABLISHES OFF-STREET PARKING**  
5 **AND LOADING REQUIREMENTS ]**~~[to accommodate anticipated~~  
6 ~~parking utilization in areas without public on-street parking~~  
7 ~~management]~~ **[AS A NECESSARY PART OF THE**  
8 **DEVELOPMENT AND USE OF LAND, TO ENSURE THE SAFE**  
9 **AND ADEQUATE FLOW OF TRAFFIC IN THE PUBLIC STREET**  
10 **SYSTEM]**, **AND TO]** ensures that parking facilities [LOTS] are  
11 designed to perform in a safe, efficient manner. It is also the intent  
12 of this section to attenuate the **[ADVERSE ]** visual, environmental,  
13 and economic impacts of parking facilities [LOTS], and **encourage**  
14 **[to balance the provision of adequate off-street parking with]**  
15 other modes of site access ~~[that promote ]~~ **[TO ACHIEVE]** **[ A**  
16 **COMPACT AND EFFICIENT LAND USE PATTERN]**, ~~affordable~~  
17 ~~housing, pedestrian-friendly environments, and other area-~~  
18 ~~wide objectives].~~ Specific objectives of this section **[PURPOSES]**  
19 include to:

20  
21 1. **[ENSURE THAT OFF-STREET PARKING, LOADING, AND**  
22 **ACCESS DEMANDS WILL BE MET WITHOUT ]**~~[impacting~~  
23 ~~adjoining and ]~~ **[ADVERSELY AFFECTING OTHER**  
24 **][NEARBY][ properties,][ LAND USES][,][ AND**  
25 **NEIGHBORHOODS;]**

26  
27 ~~[2.]~~ **Support**~~[Ensure ]~~ the safe and adequate flow of traffic,  
28 pedestrians, and service vehicles in adjoining streets;

29  
30 ~~2[3][2].~~ Provide for safe and orderly circulation and parking in  
31 parking and loading facilities, and minimize conflicts between  
32 pedestrians and vehicles;

33  
34 ~~[4][3].~~ **ENCOURAGE THE EFFICIENT USE OF LAND]**~~[ AND~~  
35 ~~][discourage] [AVOID]~~ **[ THE ENCUMBRANCE OF MORE**  
36 **SPACE THAN IS NECESSARY FOR ]**~~[anticipated~~  
37 ~~][PARKING ]~~ **[utilization];]**

38  
39 ~~[5.]~~ ~~Recognize, through parking reductions and lower~~  
40 ~~parking requirements, the parking efficiencies of mixed-~~  
41 ~~use development, mixed-income housing, pedestrian-~~  
42 ~~friendly development patterns, proximity to public~~  
43 ~~transportation, and other area-specific neighborhood~~  
44 ~~characteristics;]~~

45  
46 ~~[6.]~~ ~~Recognize, through area-specific exemptions from~~  
47 ~~parking requirements, the parking efficiencies of public~~  
48 ~~parking facilities, on-street parking management, and~~  
49 ~~alternative travel mode incentives;]~~

1  
2 **[7][4]. IMPROVE THE VISUAL APPEARANCE OF PUBLIC**  
3 **STREET CORRIDORS BY ENCOURAGING BUILDINGS**  
4 **AND OTHER ATTRACTIVE SITE FEATURES TO BECOME**  
5 **MORE PROMINENT RELATIVE TO PARKING] [facilities]**  
6 **[LOTS] [~~and driveways~~];]**  
7

8 ~~**[8. Manage the width and frequency of driveways along**~~  
9 ~~**street frontages to provide adequate space for—and**~~  
10 ~~**reduce conflicts with—snow clearing and snow storage**~~  
11 ~~**in the right-of-way, on-street parking (where appropriate),**~~  
12 ~~**and pedestrian facilities;]**~~  
13

14 **3[9][5].** Provide for better pedestrian movement and  
15 encourage alternative modes of transportation consistent with  
16 cleaner air and water, greater transportation choice, and  
17 efficient infill and redevelopment by reducing [THE] expanses  
18 of parking [THAT MUST BE TRAVERSED BETWEEN  
19 DESTINATIONS]; **and**  
20

21 [6. SUPPORT A BALANCED TRANSPORTATION SYSTEM  
22 THAT IS CONSISTENT WITH CLEANER AIR AND WATER,  
23 GREATER TRANSPORTATION CHOICES, AND  
24 EFFICIENT INFILL AND REDEVELOPMENT; AND]  
25

26 **4[10].** Facilitate bicycle use by providing safe, secure, and  
27 convenient bike parking through minimum requirements for  
28 the type of bike parking facilities and the amount of bicycle  
29 spaces.]; **and**  
30

31 ~~**[11][7] [. ALLOW FLEXIBILITY IN ADDRESSING VEHICLE**~~  
32 ~~**PARKING, LOADING, AND ACCESS, INCLUDING]**~~  
33 ~~**[PROVIDING FOR][ REDUCTIONS ][AND ALTERNATIVES]**~~  
34 ~~**[TO ][MINIMUM][ PARKING REQUIREMENTS ][and**~~  
35 ~~**mechanisms to manage parking needs while promoting**~~  
36 ~~**development, reinvestment, and other community**~~  
37 ~~**goals][.]**~~  
38

39 B. *Applicability.*

40  
41 1. *Generally.*

42  
43 a. The off-street parking and loading standards of this  
44 section 21.07.090 shall apply to all development in the  
45 municipality, including changes of use. **All areas used**  
46 **for off-street parking and loading shall meet the**  
47 **development and design standards of this section**  
48 **21.07.090.**  
49



1 APPROVAL OF THE TRAFFIC ENGINEER. PRIOR TO  
2 APPROVAL OF THE PERMIT THE TRAFFIC ENGINEER  
3 MAY REVIEW ALL METHODS OF CONTROL AND MAY  
4 DISAPPROVE OF ANY RESTRICTION SUCH AS FEES  
5 THAT ADVERSELY AFFECTS THE PURPOSE OF THIS  
6 SECTION]. The municipality may enforce any approved  
7 parking plan or restrictions through any of the code  
8 enforcement provisions set forth in chapter 21.14,  
9 Enforcement.

10  
11 **3[5].** *Parking Nonconformities.* When a site is out of compliance  
12 **with this section 21.07.090[AS TO THE NUMBER OF**  
13 **REQUIRED OR ALLOWED PARKING SPACES],** section  
14 21.13.060, *Characteristics of Use*, applies.

15  
16 C. *Computation of Parking and Loading Requirements.*  
17 \*\*\* \*\*\* \*\*\*

18 (Note to Code Revisor: Subsection C.4. below was not included in AO 2022-80  
19 as introduced by the Assembly but is included in the S-Version and deleted as  
20 shown. Subsection C.5. is re-numbered to C.4.)

21  
22 4. **[OCCUPANCY LOAD FACTORS. WHERE PARKING**  
23 **REQUIREMENTS FOR ASSEMBLY ROOMS OR OTHER**  
24 **USES ARE BASED ON MAXIMUM CAPACITY UNDER**  
25 **PROVISIONS OF AMC TITLE 23, THE OCCUPANCY LOAD**  
26 **FACTORS OF AMC TITLE 23 SHALL NOT BE ADJUSTED.]**

27  
28 **[5.]** *Additional computation standards.*

29  
30 a. *Off-Street Loading Space.* Required off-street loading  
31 space shall not be included as off-street parking  
32 spaces in computation of required or allowed number  
33 of off-street parking spaces, unless approved by the  
34 traffic engineer.

35  
36 b. **[FLEET VEHICLE PARKING. FOR THE PURPOSE**  
37 **OF CALCULATING PARKING REQUIREMENTS,**  
38 **FLEET VEHICLE PARKING SHALL NOT COUNT**  
39 **AGAINST EITHER THE MINIMUM OR MAXIMUM**  
40 **REQUIREMENTS.]**

41  
42 **[c.]** *Areas that Do Not Count Toward [MINIMUM BUT*  
43 **NOT] Maximum Parking Requirements.** For the  
44 purpose of calculating parking requirements, the  
45 following types of parking spaces shall not count  
46 against the maximum parking requirement[, **BUT**  
47 **SHALL COUNT TOWARD THE MINIMUM**  
48 **REQUIREMENT]:**

- i. Accessible parking spaces;
- ii. Parking spaces set aside for passenger loading [ZONES SPACES ]including taxicab stands and ride hailing service spaces, provided that such spaces are not required by the traffic engineer pursuant to subsection 21.07.090I.;
- iii. Vanpool and carpool parking spaces;
- iv. Stacked, tandem, and full-time attendant (valet) parking spaces in conformance with subsection 21.07.090H.9[12].;
- v. User-paid parking such as unbundled parking;
- vi. Parking spaces with electric vehicle supply equipment (EVSE) for EV charging;
- vii. **Fleet vehicle parking;**
- viii[vii][IV]. Parking spaces provided as the required parking for a use on another parcel through a municipally approved shared parking or off-site parking agreement; and
- ix[viii][V]. Parking structures, underground parking, and parking within, above, or beneath the building(s) it serves.

**[d.] Reductions to Required Parking Not Applicable to Accessible Parking Spaces or Passenger Loading. The following types of parking spaces shall not be reduced by the area-specific parking requirements or exemptions established in 21.07.090E.2. or the parking reductions and alternatives in 21.07.090F.:**

- i. **Required accessible (ADA) parking spaces pursuant to subsection 21.07.090J.**
- ii. **Required passenger loading spaces pursuant to subsection 21.07.090I.]**

D. *Parking Lot Layout and Design Plan.*

1. *Applicability.* For all commercial, commercial marijuana, mixed-use, industrial, community, multifamily, and townhouse residential developments **that provide off-street parking,** the applicant shall submit a parking facility layout, circulation,

1 and design plan for review and approval by the traffic  
2 engineer. The plan shall contain sufficient detail to enable the  
3 traffic engineer and the director to verify compliance with this  
4 section 21.07.090. Subject to approval of the traffic engineer,  
5 the parking layout and design plan may be combined with  
6 other plans required under this title, such as the landscaping  
7 plan required in 21.07.080, Landscaping, Screening, and  
8 Fences.  
9

10 *(Note to Code Revisor: Subsection D.1. above was not included in AO 2022-80*  
11 *as introduced by the Assembly but is included in the S-Version and is amended*  
12 *as shown.)*

13  
14 2. *Minimum Plan Requirements.*

- 15  
16 a. The parking facility layout, circulation, and design plan  
17 shall be prepared by a design professional and  
18 stamped by a professional registered with the Alaska  
19 State Board of Registration for Architects, Engineers,  
20 and Land Surveyors, except that parking lots with  
21 fewer than 20 parking spaces shall be exempt.  
22  
23 b. The parking facility layout, circulation, and design plan  
24 shall include:  
25  
26 i. Location of permanent or temporary snow  
27 storage areas with calculations per 21.07.040F.;  
28  
29 ii. Location of required landscaping areas, refuse  
30 screening, and fences;  
31  
32 iii. Location of required pedestrian sidewalks and  
33 walkways per 21.07.060E. Show dimensions for  
34 sidewalk widths and grades with spot  
35 elevations;  
36  
37 iv. Off-street parking and loading calculation for all  
38 uses located on the site per tables 21.07-~~5~~~~7~~[4]  
39 and 21.07-~~7~~~~11~~[6];  
40  
41 v. Parking space and loading berth locations.  
42 Include number of spaces provided, typical  
43 space dimensions, and grades per 21.07.090H.;  
44  
45 vi. Where loading facilities or on-site refuse  
46 collection are provided, show commercial truck  
47 circulation and truck turning radii when required  
48 by the traffic engineer;  
49



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vii[VI]. Show all parking facility[AREA] circulation patterns including location of curbed end islands at end of parking rows. Provide dimensions for the width of the circulation aisles, and show grades with spot elevations[ **PER 21.07.090H.**];

viii[VII]. Location of accessible parking spaces and access aisles per 21.07.090J.;

ix[VIII]. Location of passenger loading zones and spaces [IF REQUIRED ]per 21.07.090I.;

x. Emergency access, fire lanes, and refuse collection;

xi[IX]. Vehicle drive-throughs, vehicle queueing spaces and dimensions including vehicle-to-vehicle separation if required per 21.07.090 L.;

xii[X]. Number, location and dimensions of bicycle parking spaces if required per 21.07.090K.;

xiii[XI]. Driveways to streets and alleys. Provide dimensions for throat width/depth, landing grades, and driveway slope including spot elevations. Show sight distance triangles per the m[M]unicipal d[D]riveway s[S]tandards;

xiv[XII]. On site traffic control signage and locations. Provide a sign summary table that indicates Sign ID, MUTCD/ATMS sign designation, description, and the direction the sign is facing;

xv[XIII]. Required parking lot lighting locations, lighting calculations and glare statement, except that parking lots with fewer than 10 parking spaces serving three- and four-unit multifamily uses are exempt from providing a lighting engineering plan; and

xvi[XIV]. Location of significant drainage elements such as manholes, catch basin, and drainage swales.

- c. The traffic engineer shall ensure that provisions have been made for minimum interference with street traffic flow and safe interior vehicular and pedestrian circulation, transit, and parking.

E. *Off-Street Parking Requirements*

1. **Minimum Number of Spaces Required. Off-street parking spaces are not required, except:**
  - a. **Accessible (ADA) parking spaces may be required, as provided in subsection 21.07.090J.;**
  - b. **Passenger loading spaces may be required, as provided in subsection 21.07.090I.;**
  - c. **Bicycle parking spaces may be required, as provided in subsection 21.07.090K.; and**
  - d. **All areas used for off-street parking shall be constructed as parking facilities that meet the standards of section 21.07.090H., Parking and Loading Facility Design Standards.**

[UNLESS OTHERWISE EXPRESSLY STATED IN THIS TITLE, O]~~[Q]~~**FF-STREET PARKING SPACES SHALL BE PROVIDED IN ACCORDANCE WITH this subsection 21.07.090E.**[TABLE 21.07-4, OFF-STREET PARKING SPACES REQUIRED, AND SUBSECTION E.2. BELOW][. REDUCTIONS ][, EXEMPTIONS][ AND ALTERNATIVES TO THE ]~~[requirements of this subsection ]~~[REQUIRED MINIMUM NUMBER OF PARKING SPACES][ ARE PROVIDED IN SUBSECTION 21.07.090F., ]~~[Parking Reductions and Alternatives ]~~[BELOW][.]

2. ~~**Area-specific Parking Requirements. Lower parking requirements for specific areas that have less parking utilization and more alternative transportation options are set forth in table 21.07-7.**~~[MINIMUM OF THREE PARKING SPACES. WHERE A USE IS REQUIRED TO PROVIDE OFF-STREET PARKING AND THE AMOUNT SPECIFIED IN TABLE 21.07-4 WOULD RESULT IN FEWER THAN THREE SPACES BEING REQUIRED FOR THE USE, THE USE SHALL PROVIDE AT LEAST THREE PARKING SPACES INCLUDING ONE VAN-ACCESSIBLE PARKING SPACE PURSUANT TO SUBSECTION 21.07.090J. WHERE THERE ARE MULTIPLE USES LOCATED ON A SITE, THE USES MAY SHARE THE ACCESSIBLE SPACE AS LONG

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AS THE REQUIREMENTS OF SUBSECTION 21.07.090J.1. ARE MET. PARKING REDUCTIONS IN SUBSECTION 21.07.090F. SHALL ALSO COMPLY WITH THIS SUBSECTION E.2. THE MINIMUM OF THREE PARKING SPACES SHALL NOT APPLY TO USES IN THE DT DISTRICTS, RESIDENTIAL HOUSEHOLD LIVING USES, COMMUNITY GARDENS, PARKS AND OPEN SPACE, UTILITY SUBSTATIONS, OR FUELING STATIONS AND FOOD AND BEVERAGE KIOSKS THAT ARE EXCLUSIVELY FOR DRIVE-THROUGH CUSTOMERS.]

<b>TABLE 21.07-7: AREA-SPECIFIC PARKING REQUIREMENTS</b>		
<b>Areas</b>	<b>Applicability</b>	<b>Minimum Spaces Required</b>
<u>Downtown (DT) zoning districts</u>	<u>All Developments</u>	<u>No off-street parking is required, as provided in 21.11.070F.</u>
<u>Traditional Urban Neighborhood Context (Section 21.07.015D., Map 21.07-1.)</u>	<u>All Developments</u>	<u>65% of the minimum spaces required in table 21.07-8.</u>
<u>Edge Urban Neighborhood Context (Section 21.07.015D., Maps 21.07-1 thru -3.)</u>	<u>All Developments</u>	<u>75% of the minimum spaces required in table 21.07-8.</u>
<u>Transit-Supportive Development Corridors outside of Traditional Urban and Edge Urban Neighborhood Context areas (Section 21.07.015D., Maps 21.07-1 thru -4.)</u>	<u>All Developments</u>	<u>90% of the minimum spaces required in table 21.07-8.</u>
<u>Parking Benefit Districts established pursuant to Title 9, Traffic</u>	<u>All Developments</u>	<u>No off-street parking is required, subject to subsection 21.07.090E.7.</u>
<u>Girdwood</u>	<u>See section 21.09.070L. for area-specific parking requirements in Girdwood.]</u>	

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**3. Use-specific parking requirements. All development in the Municipality not addressed in the area-specific parking requirements in table 21.07-7 shall provide off-street parking spaces in accordance with table 21.07-8:]**

*(Note to Code Revisor: Delete Table 21.07-4: Off-Street Parking Spaces Required, in full, as shown below.)*

<b>TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED]</b> <b>[(“DU” = DWELLING UNIT; “SF” = SQUARE FEET; “GFA” = GROSS FLOOR AREA)]</b>			
<b>[USE CATEGORY]</b>	<b>[USE TYPE]</b>	<b>[MINIMUM SPACES REQUIRED]</b>	<b>[SEE LOADING SUBSECTION 21.07.090G.]</b>
<b>[RESIDENTIAL USES]</b>			
<b>[HOUSEHOLD LIVING]</b>	<b>[DWELLING, MIXED-USE, MULTIFAMILY,</b>	<b>[1 PER STUDIO OR EFFICIENCY OR ONE BEDROOM DU</b>	<b>[X]</b>

[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [("DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA)]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
	SINGLE-FAMILY ATTACHED, TWO-FAMILY, AND TOWNHOUSE]	ADD 0.5 SPACES FOR EACH ADDITIONAL BEDROOM ADD 0.25 GUEST PARKING SPACES FOR EACH MULTIFAMILY DU WITH SINGLE-FAMILY OR TWO-FAMILY STYLE CONSTRUCTION ADD 0.15 GUEST PARKING SPACES FOR EACH MULTIFAMILY DU WITH TOWNHOUSE STYLE CONSTRUCTION ADD 0.10 GUEST PARKING SPACES FOR EACH MULTIFAMILY DU, WITH A MINIMUM OF 1 GUEST SPACE ADD 0.10 GUEST PARKING SPACES FOR EACH MIXED-USE DU, WITH A MINIMUM OF 1 GUEST SPACE]	
	[DWELLING, SINGLE-FAMILY DETACHED ]	[2 PER DU UP TO 2,400 SQUARE FEET; 3 PER DU OVER 2,400 SQUARE FEET, INCLUDING ANY UNFINISHED AREA WHICH MAY BE CONVERTED TO LIVING AREA]	
	[ACCESSORY DWELLING UNIT (ADU)]	[SEE SUBSECTION 21.05.070D.]	
	[ALL OTHER HOUSEHOLD LIVING USES]	[2 PER DU]	
[GROUP LIVING]	[ASSISTED LIVING FACILITY (9+ CLIENT CAPACITY)]	[1 PER 4 BEDS PLUS 1 PER 350 SF OF OFFICE AREA PLUS REQUIREMENT FOR DWELLING, IF LOCATED IN A DWELLING]	[X]
	[CORRECTIONAL COMMUNITY RESIDENTIAL CENTER]	[1 PER 2,000 SF GFA]	[X]
	[HABILITATIVE CARE FACILITY]	[1 PER 400 SF GFA, AND 1 PASSENGER LOADING SPACE, RESERVED FOR PICKUP AND DELIVERY OF ADULTS, PER 800 SF GFA]	[X]
	[ROOMINGHOUSE]	[0.6 PER GUESTROOM]	[X]
	[TRANSITIONAL LIVING FACILITY]	[1 PER 2 BEDS PLUS 1 PER 4 PERSONS IN PRINCIPAL ASSEMBLY AREA BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	[X]
[COMMUNITY USES]			
[ADULT CARE]	[ADULT CARE FACILITY, 3-8 PERSONS]	[1 PER 400 SF GFA, AND 1 PASSENGER LOADING SPACE, RESERVED FOR PICKUP AND DELIVERY OF ADULTS, PER 2,000 SF GFA (PLUS REQUIREMENT FOR PRINCIPAL USE, IF APPROVED AS ACCESSORY USE)]	
	[ADULT CARE FACILITY, 9+ PERSONS]	[1 PER 400 SF GFA, AND 1 PASSENGER LOADING SPACE, RESERVED FOR PICKUP AND DELIVERY OF ADULTS, PER 2,000 SF GFA]	[X]
[CHILD CARE]	[CHILD CARE HOME]	[NO ADDITIONAL REQUIREMENTS BEYOND THOSE REQUIRED FOR THE DWELLING UNIT IF THE ESTABLISHMENT IS FOR FEWER THAN 9 CHILDREN AND IS NOT LOCATED IN A DWELLING, THEN THE REQUIREMENT IS AS PROVIDED IN SUBSECTION 21.07.090E.2.]	
	[CHILD CARE CENTER, 9-15 CHILDREN]	[1 SPACE IN ADDITION TO WHAT IS REQUIRED FOR THE DWELLING]	

[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [["DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA]]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
	[CHILD CARE CENTER, MORE THAN 15 CHILDREN]	[1 PER 400 SF GFA, AND 1 PASSENGER LOADING SPACE, RESERVED FOR PICKUP AND DELIVERY OF CHILDREN, PER 800 SF GFA]	
[COMMUNITY SERVICE]	[CEMETERY OR MAUSOLEUM]	[SEE SUBSECTION 21.07.090E.3.]	
	[COMMUNITY CENTER OR RELIGIOUS ASSEMBLY]	[1 PER 5 PERSONS IN PRINCIPAL ASSEMBLY AREA BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	[X]
	[CREMATORIUM]	[1 PER 4 PERSONS IN THE MAIN CHAPEL BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	
	[FAMILY SELF-SUFFICIENCY SERVICE]	[1 PER 300 SF GFA]	
	[GOVERNMENT ADMINISTRATION AND CIVIC BUILDINGS]	[1 PER 300 SF GFA]	[X]
	[HOMELESS AND TRANSIENT SHELTER]	[1 PER 300 SF ADMINISTRATIVE AREA, AND 1 PER 20 PILLOWS]	
	[NEIGHBORHOOD RECREATION CENTER]	[SEE SUBSECTION 21.07.090E.3.]	
	[SOCIAL SERVICE FACILITY]	[1 PER 300 SF GFA]	
[CULTURAL FACILITY]	[AQUARIUM]	[1 PER 500 SF GFA]	[X]
	[BOTANICAL GARDENS]	[SEE SUBSECTION 21.07.090E.3.]	[X]
	[LIBRARY]	[1 PER 400 SF GFA]	[X]
	[MUSEUM OR CULTURAL CENTER]	[1 PER 400 SF GFA]	[X]
	[ZOO]	[1 PER 5,000 SF OF SITE AREA]	[X]
	[ALL OTHER USES]	[1 PER 400 SF GFA OR 1 PER 10,000 SF OF SITE AREA FOR OUTDOOR USES]	[X]
[EDUCATIONAL FACILITY]	[BOARDING SCHOOL]	[SEE SUBSECTION 21.07.090E.3.]	[X]
	[COLLEGE AND UNIVERSITY]	[SEE SUBSECTION 21.07.090E.3.]	[X]
	[COMPUTER-AIDED LEARNING CENTER]	[1 PER 300 SF OF ENCLOSED FLOOR SPACE]	[X]
	[ELEMENTARY SCHOOL AND MIDDLE SCHOOL]	[1 PER 6 STUDENTS, BASED ON STATE OF ALASKA EED CAPACITY PROVISIONS]	[X]
	[HIGH SCHOOL]	[6 PER CLASSROOM WHERE THE TRAFFIC ENGINEER HAS REASON TO BELIEVE THAT, BASED ON SIMILAR OR COMPARABLE SCHOOLS, PARKING STUDY DATA, OR OTHER INFORMATION, THAT PARKING DEMAND FOR THE PROPOSED HIGH SCHOOL DEVELOPMENT IS LIKELY TO EXCEED THE REQUIREMENT, THE TRAFFIC ENGINEER MAY REQUIRE UP TO 1 PARKING SPACE PER 3 STUDENTS, BASED ON STATE OF ALASKA EED CAPACITY PROVISIONS.]	[X]

[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [["DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA]]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
	[INSTRUCTIONAL SERVICES]	[6 PER CLASSROOM, PLUS 1 PER 300 SQUARE FEET OF DANCE OR OTHER TRAINING AREA]	[X]
	[VOCATIONAL OR TRADE SCHOOL]	[1 PER 2 STUDENTS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	[X]
[HEALTH CARE FACILITY]	[HEALTH SERVICES, INCLUDING OUTPATIENT MEDICAL AND DENTAL OFFICES, CO-LOCATED WITH A HOSPITAL/ HOSPITAL CAMPUS OTHER HEALTH SERVICES, INCLUDING OUTPATIENT MEDICAL AND DENTAL OFFICES]	[1 PER 250 SF GFA]  [1 PER 300 SF GFA]	[X]
	[HOSPITAL/ HEALTH CARE FACILITY]	[1 PER 2 BEDS, BASED ON MAXIMUM CAPACITY, PLUS 1 PER 350 SF OF OFFICE AND ADMINISTRATIVE AREA]	[X]
	[NURSING FACILITY]	[1 PER 4 BEDS, BASED UPON MAXIMUM CAPACITY.]	[X]
[PARK AND OPEN AREA]	[COMMUNITY GARDEN]	[1 PER 5,000 SF OF LOT AREA]	
	[PARK, PUBLIC OR PRIVATE]	[SEE SUBSECTION 21.07.090E.3. PLAYFIELDS (SOCCER, BASEBALL, ETC.) SHALL HAVE MINIMUM OF 25 SPACES PER FIELD, UNLESS OTHERWISE APPROVED BY THE TRAFFIC ENGINEER, FOR UP TO FOUR FIELDS. FACILITIES WITH MORE THAN FOUR FIELDS SHALL BE SUBJECT TO THE DETERMINATION OF THE TRAFFIC ENGINEER.]	
[PUBLIC SAFETY FACILITY]	[ALL USES]	[SEE SUBSECTION 21.07.090E.3.]	
[TRANSPORTATION FACILITY]	[ALL USES]	[SEE SUBSECTION 21.07.090E.3.]	[X]
[UTILITY FACILITY]	[UTILITY FACILITY]	[1 PER 1,000 SF GFA]	[X]
	[UTILITY SUBSTATION, WIND ENERGY CONVERSION SYSTEM]	[SEE SUBSECTION 21.07.090E.3.]	
[COMMUNICATION STRUCTURES]	[ALL USES]	[NONE]	
[COMMERCIAL USES]			
[AGRICULTURAL USES]	[COMMERCIAL HORTICULTURE]	[SEE SUBSECTION 21.07.090E.3.]	[X]

[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [["DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA]]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
[ANIMAL SALES, SERVICE & CARE]	[ANIMAL BOARDING]	[1 PER 800 SF GFA]	
	[ANIMAL SHELTER]	[1 PER 400 SF GFA]	
	[LARGE DOMESTIC ANIMAL FACILITY, PRINCIPAL USE]	[1 PER 4 SEATS OR 1 PER STALL, WHICHEVER IS GREATER]	
	[RETAIL AND PET SERVICES]	[1 PER 350 SF GFA]	[X]
	[VETERINARY CLINIC]	[1 PER 600 SF GFA]	[X]
[ASSEMBLY]	[CIVIC/CONVENTION CENTER]	[1 PER 4 PERSONS IN ASSEMBLY AREAS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	[X]
	[CLUB/LODGE/MEETING HALL]	[1 PER 4 PERSONS IN ASSEMBLY AREAS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23.]	[X]
[ENTERTAINMENT AND RECREATION]	[AMUSEMENT ESTABLISHMENT BOWLING ALLEY BINGO PARLOR  INDOOR SHOOTING RANGE]	[INDOOR ENTERTAINMENT FACILITY: 1 PER 300 SF GFA 4 PER BOWLING LANE 1 PER 4 PERSONS IN ASSEMBLY AREAS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23.  1 PER TARGET AREA, OR 1 PER 5 SEATS, WHICHEVER IS GREATER]	
	[ENTERTAINMENT FACILITY, MAJOR]	[SEE SUBSECTION 21.07.090E.3.]	[X]
	[FITNESS AND RECREATIONAL SPORTS CENTER]	[1 PER 300 SF GFA]	[X]
	[GENERAL OUTDOOR RECREATION, COMMERCIAL]	[SEE SUBSECTION 21.07.090E.3.]	
	[GOLF COURSE]	[4 PER GREEN]	
	[GOLF DRIVING RANGE]	[0.5 PER TEE]	
	[MOTORIZED SPORTS FACILITY]	[1 PER 2 SPECTATOR SEATS IN A STRUCTURE SUCH AS A GRANDSTAND, STADIUM; OR 1 PER 2,000 SF OF SITE AREA; WHICHEVER IS GREATER]	[X]
	[MOVIE THEATER]	[1 PER 4 PERSONS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	[X]
	[NIGHTCLUB]	[1 PER 3 PERSONS BASED ON MAXIMUM CAPACITY UNDER PROVISIONS OF AMC TITLE 23]	[X]
	[SHOOTING RANGE, OUTDOOR]	[1 PER TARGET AREA, OR 1 PER 5 SEATS, WHICHEVER IS GREATER]	
[SKIING FACILITY, ALPINE]	[SEE SUBSECTION 21.07.090E.3.]		
[THEATER COMPANY OR DINNER THEATER]	[1 PER 4 PERSONS BASED ON MAXIMUM CAPACITY UNDER PROVISIONS OF AMC TITLE 23]	[X]	
[FOOD AND BEVERAGE SERVICE]	[BAR]	[1 PER 100 SF GFA]	[X]
	[FOOD AND BEVERAGE KIOSK]	[0 PER ESTABLISHMENT, PLUS VEHICLE QUEUING SPACES]	

[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [["DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA]]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
	[RESTAURANT]	[1 PER 100 SF GFA FOR SEATING AREA 1 PER 125 SF GFA FOR DRIVE-THROUGH RESTAURANTS SEATING AREA (PLUS VEHICLE QUEUING SPACES) 1 PER 400 SF GFA FOR KITCHEN AREA 1 PER 1000 SF GFA FOR STORAGE AREA]	[X]
[OFFICE]	[BROADCASTING FACILITY]	[1 PER 350 SF GFA]	
	[FINANCIAL INSTITUTION]	[1 PER 350 SF GFA (PLUS VEHICLE QUEUING SPACES IF DRIVE-THROUGH IS PROVIDED)]	[X]
	[OFFICE, BUSINESS OR PROFESSIONAL]	[1 PER 350 SF GFA]	[X]
[PERSONAL SERVICE, REPAIR, AND RENTAL]	[BUSINESS SERVICE ESTABLISHMENT]	[1 PER 500 SF GFA]	[X]
	[FUNERAL SERVICES]	[1 PER 4 PERSONS IN MAIN ASSEMBLY AREAS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23]	[X]
	[GENERAL PERSONAL SERVICES]	[1 PER 400 SF GFA]	[X]
	[SMALL EQUIPMENT RENTAL]	[1 PER 400 SF GFA]	[X]
[RETAIL SALES]	[AUCTION HOUSE]	[1 PER 300 SF GFA]	[X]
	[BUILDING MATERIALS STORE]	[1 PER 600 SF GFA AND OUTDOOR DISPLAY AREA]	[X]
	[CONVENIENCE STORE]	[1 PER 350 SF GFA]	[X]
	[FARMERS MARKET]	[SEE SUBSECTION 21.07.090E.3.]	
	[FUELING STATION]	[1 PER ATTENDANT FOR STAND-ALONE FUELING STATIONS; ALSO REFER TO SUBSECTION 21.07.090L.2. FOR QUEUING REQUIREMENT]	
	[FURNITURE AND HOME APPLIANCE STORE]	[1 PER 800 SF GFA]	[X]
	[GENERAL RETAIL]	[1 PER 350 SF GFA]	[X]
	[GROCERY OR FOOD STORE]	[1 PER 250 SF GFA]	[X]
	[LIQUOR STORE]	[1 PER 400 SF GFA]	[X]
[PAWNSHOP]	[1 PER 350 SF GFA]	[X]	
[VEHICLES AND EQUIPMENT]	[AIRCRAFT AND MARINE VESSEL SALES]	[1 PER 7,000 SF OUTDOOR DISPLAY/SALES AREA; 1 PER 400 SF INDOOR FLOOR AREA]	[X]
	[VEHICLE PARTS AND SUPPLIES]	[1 PER 400 SF GFA; 1 PER 7,000 SF OUTDOOR DISPLAY/SALES AREA]	[X]
	[VEHICLE – LARGE AND SMALL, SALES VEHICLE – LARGE AND SMALL, RENTAL]	[1 PER 7,000 SF OUTDOOR DISPLAY/SALES AREA; 1 PER 400 SF INDOOR FLOOR AREA 1 PER 400 SF OF INDOOR FLOOR AREA]	[X]



[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [["DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA]]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
	[VEHICLE SERVICE AND REPAIR, MAJOR AND MINOR]	[0.5 PER CAR WASH BAY; 4 PER OTHER SERVICE BAY (PROVIDED THAT ALL VEHICLES IN CUSTODY OF OPERATOR OF BUSINESS FOR PURPOSE OF SERVICE, REPAIR OR STORAGE SHALL BE STORED ON PREMISES OR ON A SEPARATE OFF-STREET PARKING LOT OR BUILDING)]	
[VISITOR ACCOMMODATIONS]	[CAMPER PARK]	[1 SPACE PER 10 RECREATIONAL VEHICLE OR TENT CAMPING SPACES]	
	[EXTENDED-STAY LODGINGS]	[1 PER GUESTROOM OR ONE BEDROOM UNIT; 1.25 PER TWO BEDROOM UNIT; 1.5 PER THREE BEDROOM OR MORE UNIT; PLUS 1 PER 4 PERSONS IN MEETING ROOMS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23.]	[X]
	[HOSTEL]	[1 PER 600 SF GFA]	
	[HOTEL, MOTEL AND INN]	[0.9 PER GUESTROOM, PLUS 1 PER 4 PERSONS IN MEETING ROOMS BASED ON MAXIMUM OCCUPANCY PROVISIONS OF AMC TITLE 23.]	[X]
	[RECREATIONAL AND VACATION CAMP]	[SEE SUBSECTION 21.07.090E.3.]	
[MARIJUANA USES]			
[MARIJUANA USES]	[MARIJUANA CULTIVATION FACILITY]	[1 PER 1,000 SF GFA]	[X]
	[MARIJUANA MANUFACTURING FACILITY]	[1 PER 400 SF GFA]	[X]
	[MARIJUANA TESTING FACILITY]	[1 PER 350 SF GFA]	
	[MARIJUANA RETAIL SALES ESTABLISHMENT]	[1 PER 350 SF GFA]	[X]
[INDUSTRIAL USES <sup>11</sup> ]			
[INDUSTRIAL SERVICE <sup>11</sup> ]	[DATA PROCESSING FACILITY]	[1 PER 1,000 SF GFA]	[X]
	[DRY CLEANING ESTABLISHMENT]	[1 PER 750 SF DRY CLEANING PLANT AREA PLUS 1 PER 600 SF OF CUSTOMER SERVICE AREA]	[X]
	[GENERAL INDUSTRIAL SERVICE, CONTRACTOR AND SPECIAL TRADES, LIGHT]	[1,000-3,000 SF GFA: 1 PER 750 SF GFA; ADD 1 SPACE PER EACH 1,000 SF GFA ABOVE 3,000 SF GFA, UP TO 5,000 SF GFA; ADD 1 SPACE PER EACH 1,500 SF GFA ABOVE 5,000 SF GFA, UP TO 50,000 SF GFA; ADD 1 SPACE PER EACH 2,000 SF GFA ABOVE 50,000 SF GFA]	[X]
	[GOVERNMENTAL SERVICE]	[1 PER 600 SF GFA]	[X]
	[HEAVY EQUIPMENT, SALES AND RENTAL]	[1 PER 400 SF INDOOR FLOOR AREA]	[X]
	[RESEARCH LABORATORY]	[1 PER 350 SF GFA]	[X]

[TABLE 21.07-][8][4]: OFF-STREET PARKING ][REQUIREMENTS BY LAND USE ][SPACES REQUIRED] [["DU" = DWELLING UNIT; "SF" = SQUARE FEET; "GFA" = GROSS FLOOR AREA]]			
[USE CATEGORY]	[USE TYPE]	[MINIMUM SPACES REQUIRED]	[SEE LOADING SUBSECTION 21.07.090G.]
[MANUFACTURING AND PRODUCTION <sup>11</sup> ]	[COMMERCIAL FOOD PRODUCTION]	[1 PER 400 SF GFA FOR CATERING; 1 PER 800 SF GFA FOR FOOD PROCESSING]	[X]
	[COTTAGE CRAFTS]	[1 PER 600 SF GFA]	
	[MANUFACTURING (GENERAL, HEAVY, AND LIGHT)]	[1,000-3,000 SF GFA: 1 PER 750 SF GFA; ADD 1 SPACE PER EACH 1,000 SF GFA ABOVE 3,000 SF GFA, UP TO 5,000 SF GFA; ADD 1 SPACE PER EACH 1,500 SF GFA ABOVE 5,000 SF GFA,]	[X]
	[NATURAL RESOURCE EXTRACTION]	[SEE SUBSECTION 21.07.090E.3.]	
[MARINE FACILITY <sup>11</sup> ]	[AQUACULTURE]	[SEE SUBSECTION 21.07.090E.3.]	
	[FACILITY FOR COMBINED MARINE AND GENERAL CONSTRUCTION]	[SEE SUBSECTION 21.07.090E.3.]	
	[MARINE OPERATIONS]	[SEE SUBSECTION 21.07.090E.3.]	
	[MARINE WHOLESALING]	[1 PER 800 SF GFA]	[X]
[WAREHOUSE AND FREIGHT MOVEMENT <sup>11</sup> ]	[BULK STORAGE OF HAZARDOUS MATERIALS]	[SEE SUBSECTION 21.07.090E.3.]	
	[IMPOUND YARD]	[1 PER 500 SF GFA, PLUS 1 PER 5,000 SF OF OUTDOOR STORAGE AREA]	
	[MOTOR FREIGHT TERMINAL]	[SEE WAREHOUSE OR WHOLESALE ESTABLISHMENT]	[X]
	[SELF-STORAGE FACILITY]	[1 PER 75 SELF-STORAGE UNITS, PLUS VEHICLE QUEUING SPACES FOR SECURITY GATE. AISLES SUITABLE FOR TEMPORARY LOADING AND UNLOADING MAY BE COUNTED AS REQUIRED PARKING SPACES IN ACCORDANCE WITH TABLE 21.07-4 AS DETERMINED BY THE TRAFFIC ENGINEER.  1 PER 75 VEHICLE/BOAT STORAGE SPACES]	[X]
	[STORAGE YARD]	[1 PER 2,000 SF OF OUTDOOR STORAGE AREA]	
	[WAREHOUSE OR WHOLESALE ESTABLISHMENT, GENERAL OR LIGHT]	[1,000-10,000 SF GFA: 1 PER 1,000 SF GFA; ADD 1 SPACE PER EACH 1,250 SF GFA ABOVE 10,000 SF GFA, UP TO 50,000 SF GFA; ADD 1 SPACE PER EACH 1,500 SF GFA ABOVE 50,000 SF GFA,]	[X]
[WASTE AND SALVAGE]	[ALL USES]	[SEE SUBSECTION 21.07.090E.3.]	
[NOTES: <sup>11</sup> THE OFF-STREET PARKING REQUIREMENTS FOR INDUSTRIAL USES IN THIS SCHEDULE A SHALL NOT INCLUDE SPACE DEVOTED TO OFFICE OR OTHER NON-INDUSTRIAL RELATED USE. WHERE A WAREHOUSING OR INDUSTRIAL FACILITY CONTAINS OFFICE OR OTHER NON-INDUSTRIAL RELATED USE, OFF-STREET PARKING FOR SUCH SPACES SHALL BE COMPUTED USING THE REQUIREMENTS SET FORTH IN THIS TABLE.]			

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[4][3]. [USES NOT LISTED OR THAT HAVE NO SPECIFIC REQUIREMENT. IN THE CASE OF A USE OR CATEGORY OF USES NOT LISTED IN TABLE 21.07-4, OR THAT IS

1 LISTED WITHOUT A SPECIFIC PARKING REQUIREMENT,  
2 THE REQUIREMENTS FOR OFF-STREET PARKING  
3 FACILITIES SHALL BE DETERMINED BY THE DIRECTOR  
4 AND THE TRAFFIC ENGINEER. SUCH DETERMINATION  
5 SHALL BE BASED UPON THE REQUIREMENTS FOR THE  
6 USE SPECIFIED IN TABLE 21.07-4 THAT IS MOST  
7 NEARLY COMPARABLE TO THE UNSPECIFIED USE,  
8 TRAFFIC ENGINEERING PRINCIPLES, AND/OR PARKING  
9 STUDIES. ANY PARKING STUDY PREPARED BY THE  
10 APPLICANT SHALL INCLUDE ESTIMATES OF PARKING  
11 DEMAND BASED ON RECOMMENDATIONS OF THE  
12 INSTITUTE OF TRANSPORTATION ENGINEERS (ITE), OR  
13 OTHER ACCEPTABLE ESTIMATES AS APPROVED BY  
14 THE TRAFFIC ENGINEER, AND SHALL INCLUDE OTHER  
15 RELIABLE DATA COLLECTED FROM USES OR  
16 COMBINATIONS OF USES THAT ARE THE SAME AS OR  
17 COMPARABLE WITH THE PROPOSED USE.  
18 COMPARABILITY SHALL BE DETERMINED BY DENSITY,  
19 SCALE, BULK, AREA, TYPE OF ACTIVITY, AND  
20 LOCATION. THE STUDY SHALL DOCUMENT THE  
21 SOURCE OF DATA USED TO DEVELOP THE  
22 RECOMMENDATIONS.]  
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24 *(Note to Code Revisor: The content of the following subsection 21.07.090E.4. was*  
25 *not included in AO 2022-80. It is included in the S-Version and amended as shown*  
26 *below. It is re-numbered to 21.07.090E.2.)*

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28 **[5]**[4]. *Maximum number of spaces permitted.*

- 29  
30 a. *Purpose.* The purpose of this subsection is to establish  
31 an upper limit on the number of parking spaces allowed  
32 **[IN ORDER ]**to promote efficient use of land, enhanced  
33 urban design, a safe and walkable pedestrian  
34 environment, alternative modes of transportation, and  
35 to protect air and water quality. Exceptions and  
36 flexibility procedures are provided where the required  
37 limit on the number of parking spaces is problematic for  
38 a certain use.  
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40 b. *Applicability.* For any use categorized as a community  
41 or commercial use in table 21.05-1, Table of Allowed  
42 Uses, the maximum number of off-street vehicle  
43 parking spaces shall be as provided below. Temporary  
44 parking, the uses “parking lot, principal use” and  
45 “parking structure, principal use”, and uses in the  
46 Educational Facility, Parks and Open Areas,  
47 Transportation Facility, and Utility Facility use  
48 categories are exempt.  
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- c. *Maximum Number of Spaces.* Developments **shall[MAY]** provide a maximum of one parking space per 250 square feet of gross floor area, **except as provided in e[, OR 125 PERCENT OF THE MINIMUM NUMBER OF PARKING SPACES REQUIRED IN TABLE 21.07-4, WHICHEVER IS GREATER].**
  
- d. *Increased Landscaping in Large Parking Lots.* Development sites with more than 200 parking spaces **[REQUIRED IN TABLE 21.07-4 AND THAT ARE PROPOSED BY THE APPLICANT TO HAVE AT LEAST 25 PERCENT MORE THAN THE MINIMUM NUMBER OF PARKING SPACES REQUIRED IN TABLE 21.07-4]** shall increase the overall amount of area devoted to parking lot interior landscaping area to an area equal to at least 12 percent of the parking lot, including parking, internal circulation, and appurtenant driveways. This shall apply to uses which utilize the exceptions offered in subsection **2[4].e.** below.
  
- e. *Exceptions.*
  - i. Restaurants **[WITHOUT A DRIVE-THROUGH], dinner theaters, and bars may provide up to one parking space per 100 square feet of gross floor area [200 PERCENT OF THE MINIMUM NUMBER OF PARKING SPACES REQUIRED IN TABLE 21.07-4]. Health services uses may provide up to one parking space per 200 square feet of gross floor area. Community centers, religious assemblies, crematoriums, assembly uses, and entertainment and recreation uses may provide a maximum of 1 parking space per 3 seats or persons in the principal assembly area based on the maximum occupancy provisions of AMC Title 23.**
  
  - ii. If application of the maximum parking standard would result in fewer than six parking spaces, the development shall be allowed six parking spaces.
  
  - iii. Exceptions to the maximum parking requirement may be allowed by the traffic engineer and the director in situations that meet all of the following criteria:

1 (A) The applicant provides a parking demand  
2 study of similar sites in the municipality  
3 that demonstrates that parking demand  
4 cannot be accommodated within the  
5 maximum number of parking spaces  
6 allowed or through **[ANY OF THE**  
7 **AVAILABLE]** parking **congestion**  
8 reduction **strategies** **[AND**  
9 **ALTERNATIVES]** such as on-street  
10 parking, shared parking with nearby  
11 uses, or incentives for alternatives to  
12 single-occupancy vehicle use; and

13  
14 (B) The request is the minimum necessary  
15 variation from the standards.

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17 **[6][5]. [PARKING LOCATION. EXCEPT AS PROVIDED IN**  
18 **SUBSECTION 21.07.090F., ALL REQUIRED PARKING**  
19 **SHALL BE ON THE SAME LOT AS THE USE SERVED.]**  
20 **[HOWEVER, REQUIRED PARKING MAY BE ON AN**  
21 **ABUTTING OR ADJACENT LOT PROVIDED THE ZONING**  
22 **DISTRICT IN WHICH THE LOT IS LOCATED ALLOWS FOR**  
23 **OFF-STREET PARKING AS A PERMITTED PRINCIPAL**  
24 **USE, SITE PLAN REVIEW USE, OR CONDITIONAL USE; IN**  
25 **WHICH CASE THERE SHALL BE A PARKING AGREEMENT**  
26 **WHICH MEETS THE REQUIREMENTS OF SUBSECTION**  
27 **F.1. BELOW.]**

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29 **[7. REQUIREMENTS FOR DEVELOPMENTS IN PARKING**  
30 **BENEFIT DISTRICTS**

31 **THE FOLLOWING AREA-SPECIFIC PARKING**  
32 **REGULATIONS APPLY TO ALL DEVELOPMENTS**  
33 **LOCATED IN PARKING BENEFIT DISTRICTS**  
34 **ESTABLISHED PURSUANT TO TITLE 9:**

35  
36 **A. NO OFF-STREET PARKING IS REQUIRED, EXCEPT**  
37 **THAT ACCESSIBLE (ADA) PARKING SPACES**  
38 **REQUIRED BY 21.07.090J. SHALL BE PROVIDED.**

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40 **B. DEVELOPMENT PROJECTS THAT RESULT IN 10**  
41 **OR MORE DWELLING UNITS, NEW**  
42 **CONSTRUCTION OF 10,000 OR MORE SQUARE**  
43 **FEET OF GROSS FLOOR AREA, OR BUILDING**  
44 **RENOVATIONS INVOLVING A CHANGE OF USE OF**  
45 **50,000 OR MORE SQUARE FEET OF NON-**  
46 **RESIDENTIAL GROSS FLOOR AREA, SHALL**  
47 **PROVIDE ONE OR MORE PARKING REDUCTION**  
48 **STRATEGIES FROM SUBSECTION 21.07.090F.**  
49 **STRUCTURED PARKING, INDUSTRIAL USES, AND**

**AFFORDABLE HOUSING UNITS THAT MEET  
21.07.110F., ARE EXEMPT FROM THIS  
REQUIREMENT.**

**I. THE APPLICANT SHALL SELECT PARKING  
REDUCTION STRATEGIES FROM  
SUBSECTION 21.07.090F.1., TABLE 21.07-9  
THAT AMOUNT TO A TOTAL REDUCTION  
OF AT LEAST 10 PERCENT USING THE  
"REDUCTION AMOUNT" COLUMN IN TABLE  
21.07-9. THE TOTAL REDUCTION  
REQUIRED SHALL INCREASE AN  
ADDITIONAL 1 PERCENT FOR EACH 10  
OFF-STREET PARKING SPACES NOT USED  
FOR A PARKING REDUCTION STRATEGY IN  
THE DEVELOPMENT. IN NO CASE SHALL  
THE TOTAL REDUCTION BE REQUIRED TO  
EXCEED 25 PERCENT.**

**II. THE REQUIREMENTS OF 21.07.090F. FOR  
RECEIVING PARKING REDUCTIONS APPLY.**

**III. THE APPLICANT MAY PROPOSE  
STRATEGIES NOT INCLUDED IN TABLE  
21.07-9, SUBJECT TO 21.07.090F.8,  
DISCRETIONARY PARKING REDUCTIONS.]**

F. *Parking usage reductions; [AND] alternative transportation  
incentives.*

**1. Purpose. Incentivize and improve access to walking,  
bicycling, ride-sharing, and public transit for residents,  
employees, and visitors of developments, in order to:**

**a. Save development costs through less parking;**

**b. Support efficient use of land and development;**

**c. Relieve traffic and parking congestion; and**

**d. Reduce parking conflicts between neighbors.**

**2. Applicability. This subsection is effective beginning  
January 1, 2024, and applies to multifamily developments  
with 20 or more dwelling units, and commercial and  
community service developments of 20,000 or more  
square feet of gross floor area. Affordable housing units  
and senior housing units are exempt.**

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- 3. Standard. Applicants shall select one choice from the alternative transportation incentives in Table 21.07-4, or propose an alternative under subsection 4.**

<b>Table 21.07-4: Alternative Transportation Amenities and Incentives</b>	
<b>Parking Management</b>	
<b><u>Unbundled Parking</u></b>	<b><u>Off-street parking for the use consists only of unbundled parking as defined in 21.15.040.</u></b>
<b><u>Parking Cash-out</u></b>	<b><u>The non-residential use implements a parking cash-out program as defined in 21.15.040 and informs all employees of the program.</u></b>
<b>Pedestrian Amenities</b>	
<b><u>Additional Bicycle Parking</u></b>	<b><u>The development provides twice the number of bicycle parking spaces required by subsection 21.07.090K.</u></b>
<b><u>Pedestrian Amenities</u></b>	<b><u>The development provides two of the following pedestrian amenities from subsection 21.07.060F. that are not otherwise required for the development: primary pedestrian walkway, transit stop or shelter, pedestrian-interactive use, enhanced sidewalk.</u></b>
<b>Transportation Programs</b>	
<b><u>Rideshare Program</u></b>	<b><u>The non-residential use participates in a rideshare program that is available to all employees at no cost and provides designated and signed rideshare parking spaces that meet the passenger loading space standards of 21.07.090I.2.</u></b>
<b><u>Car Share Program</u></b>	<b><u>The residential use sponsors memberships to a car share program as defined in 21.15.040 that is available to all households and provides designated and signed car share spaces.</u></b>
<b><u>Free Transit Pass</u></b>	<b><u>The employer or property owner sponsors public transit passes cost-free to all employees or residents.</u></b>

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- 4. Other alternatives. Applicants may propose a different amenity or incentive to reduce parking utilization or modify any of the strategies shown or in table 21.07-4, subject to approval by the traffic engineer and planning director.**

- 5. Recorded agreement. The property owner shall enter into a recorded agreement with the municipality that guarantees the maintenance and continuation of the alternative transportation amenity or incentive. The Municipality shall provide the format (template) for the agreement and record the agreement at the district recorder's office as a covenant that runs with the land.**

*(Note to Code Revisor: Delete the remainder of section 21.07.090F. in its entirety, as follows:)*

**[Parking reductions allowed. This subsection 21.07.090F. allows administrative reductions to ]**  
[THE TRAFFIC ENGINEER AND DIRECTOR MAY APPROVE

REDUCTIONS AND ALTERNATIVES TO PROVIDING] [ THE  
NUMBER OF OFF-STREET PARKING SPACES  
REQUIRED BY ] [subsection 21.07.090E. ] [TABLE 21.07-4,  
AND/OR TO THE CIRCULATION AND DIMENSIONAL  
STANDARDS OF SUBSECTIONS H.8. AND H.9., IN  
ACCORDANCE WITH THE FOLLOWING STANDARDS.]  
[Developments are allowed to apply the reductions  
shown in Table 21.07-9, provided the following:

- a. The development meets the additional requirements set forth in Table 21.07-9;
- b. The property owner enters into a parking agreement with the Municipality of Anchorage as provided in 21.07.090F.2.;
- c. Multiple parking reductions are calculated as provided in F.3.;
- d. Pedestrian access improvements are provided as set forth in 21.07.090F.4.; and
- e. Proposals for larger percentage reductions than shown, that modify any of the provisions for the reductions shown, or that propose other types of parking reductions from those in in table 21.07-9, may be approved subject to a discretionary review and approval by the traffic engineer and director as provided in subsection 21.07.090F.8., Discretionary Parking Reductions.]

**[Table 21.07-9: Parking Reductions and Alternatives]**

<b><u>[Type of Reduction]</u></b>	<b><u>[Applicability]</u></b>	<b><u>[Additional Requirements]</u></b>	<b><u>[Reduction Amount]</u></b>
<b><u>[A. Shared Vehicle Programs: Participation in one or more of the shared vehicle programs below may substitute for required parking spaces, provided that information about the programs is available in a location visible to all residents and employees.]</u></b>			
<b><u>[Carpool Program]</u></b>	<b><u>Non-residential uses</u></b>	<b><u>The employer or property owner sponsors a carpool program that is available to all employees and provides designated carpool parking spaces signed for exclusive use by the carpool program.</u></b>	<b><u>Each carpool space may count as two spaces toward meeting the minimum number of required parking spaces, up to a 2% reduction in the number of required parking spaces.</u></b>
<b><u>[Rideshare Program]</u></b>	<b><u>Non-residential uses</u></b>	<b><u>The employer or property owner participates in a rideshare program that is available to all employees at no cost and provides designated rideshare parking spaces that meet the passenger loading space dimensional standards of 21.07.090I.2. and are signed for</u></b>	<b><u>Each rideshare space may count as six spaces toward meeting the minimum number of required parking spaces, up to a 5% reduction in the number of required parking spaces.</u></b>



		<u>exclusive use by the rideshare program.</u>	
<u>Car-Share Program</u>	<u>Residential development sites located in the Neighborhood Development Contexts<sup>1</sup> or within ¼-mile of the centerline of a right-of-way of a public transit route with frequent service mapped in 21.07.015D.</u>	<u>The property owner sponsors memberships to an active car-share program for all households or group living residents on the site and provides designated car-share spaces that are signed for exclusive use.</u>	<u>Each carshare space may count as five spaces toward meeting the minimum number of required parking spaces, up to a 10% reduction in the number of required parking spaces.</u>
<u>Transit Pass Benefits</u>	<u>Any development site in the Neighborhood Development Contexts<sup>1</sup> or within ¼-mile of the centerline of a right-of-way of a public transit route with frequent service mapped in 21.07.015D.</u>	<u>The property owner sponsors public transit passes cost-free to all employees or residents.</u>	<u>10% reduction in the number of required parking spaces.</u>
<b><u>B. Pedestrian and Transit Amenities: Developments that provide improved conditions for walking and bicycling are eligible for reductions in the minimum number of required parking spaces, as provided below.</u></b>			
<u>Additional Bicycle Parking</u>	<u>Non-residential use, group living use, and any household living use with four or more dwelling units</u>	<u>The development provides more than the minimum number of required bicycle parking spaces. Each bicycle space meets the standards of 21.07.090K.4.</u>	<u>Each four bicycle parking spaces count as one automobile parking space, with a minimum allowed reduction of one space and a maximum of a 10% reduction in the number of required automobile parking spaces.</u>
<u>Enhanced On-Site Walkway</u>	<u>Any development site in the Neighborhood Development Contexts<sup>1</sup> or within ¼-mile of the centerline of a right-of-way of a public transit route with frequent service mapped in 21.07.015D.</u>	<u>The development provides an enhanced on-site walkway per 21.07.060G.3.</u>	<u>2% reduction in the number of required parking spaces</u>
<u>Enhanced Street Sidewalk</u>		<u>The development provides an enhanced street sidewalk per 21.07.060G.19.</u>	<u>2% reduction in the number of required parking spaces</u>
<u>Transit Stop or Shelter</u>	<u>Any use in the Neighborhood Development Contexts<sup>1</sup> or on a public transit route with frequent service mapped in 21.07.015D.</u>	<u>Based on a determination of need by the public transportation department, the development provides a public use easement or transit stop improvements per 21.07.060G.7.</u>	<u>2% reduction in the number of required parking spaces</u>
<u>Nearby Public Transit Route with frequent Service to a Transit-Supportive Development Corridor</u>	<u>Development sites located within one-half mile from the centerline of a right-of-way of a transit-supportive development corridor or a public transit route with frequent service shown on maps 21.07-1 to 21.07-4 in section 21.07.015D. This distance shall be measured on publicly accessible streets or trails.</u>	<u>The development site is located outside of the ¼-mile radius of the Transit-Supportive Development Corridor and is not within any other Neighborhood Development Context<sup>1</sup>. The development meets the area-specific bicycle parking requirements in subsection 21.07.090K., Table 21.07-14.</u>	<u>10% reduction in the number of required parking spaces for sites located within 1,320 feet (1/4 mile) of the right-of-way centerline. 8% reduction for sites within 1,650 feet. 6% reduction for sites within 1,980 feet. 4% reduction for sites within 2,310 feet. 2% reduction for sites within 2,640 feet (half-mile).</u>

<b><u>Other Pedestrian Amenities</u></b>	<b><u>Any development site in the Neighborhood Development Contexts<sup>4</sup> or within ¼-mile of the centerline of a right-of-way of a public transit route with frequent service mapped in 21.07.016D.</u></b>	<b><u>The development provides one or more additional pedestrian amenities from section 21.07.060G, not otherwise required by this title.</u></b>	<b><u>1% reduction in the number of required parking spaces for each pedestrian amenity.]</u></b>
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<b>[Table 21.07-9: Parking Reductions and Alternatives]</b>			
<b>[Type of Reduction]</b>	<b>[Applicability]</b>	<b>[Additional Requirements]</b>	<b>[Reduction Amount]</b>
***      ***      *** (table continued from previous page)			
<b>[C. Parking Pricing: Developments that offer the parking pricing strategies below are eligible for reductions in the minimum number of required parking spaces.</b>			
<b>Parking Cash-out</b>	<b>Non-residential uses</b>	The use implements a parking cash-out program as defined in 21.15.040 and informs all employees of the program. The cash-out value of the parking space is allowed to be up to one-year in duration.	10% reduction in the number of required parking spaces.
<b>Unbundled Parking</b>	<b>Household living uses and non-residential uses</b>	All parking accessory to the use is unbundled parking as defined in 21.15.040, by which the parking spaces are leased or sold separately from the rental or purchase fees for the dwelling units or building space.	10% reduction in the number of required parking spaces.
<b>D. Housing: Housing units with characteristics that reduce parking utilization are eligible for reductions in the minimum number of required parking spaces, as provided below.</b>			
<b>Affordable Rental Housing</b>	<b>Household living uses</b>	Rental housing units that meet the standards of 21.07.110G., Standards for Affordable Housing, are rented at rates affordable to lower income households.	Each affordable dwelling unit is eligible for a 25% reduction in the number of required parking spaces.
<b>Senior Housing</b>	<b>Residential uses</b>	Housing that meets the definition of senior housing (21.15.040) and is solely occupied by persons 62 years or older.	Each senior housing unit is eligible for a 25% reduction in the number of required parking spaces.
<b>E. Shared and Off-site Parking Facilities: Properties that utilize off-site parking facilities or shared parking between uses are eligible for reductions to the minimum number of required parking spaces, as provided below.</b>			
<b>Shared Parking</b>			See section 21.07.090F.5.
<b>Off-site Parking</b>			See section 21.07.090F.6.
<b>Nearby Public Parking</b>			See section 21.07.090F.7.
<b>F. Prioritization of Other Goals: Developments with the following public benefit features are eligible for a reduction in the minimum number of parking spaces, as follows:</b>			
<b>Land Banking</b>	<b>Any development that sets aside an area to provide for the future construction of deferred parking spaces.</b>	The area set aside is landscaped with site enhancement landscaping or pedestrian amenities. The applicant provides an alternate site plan for approval that accommodates the deferred parking, landscaping, pedestrian facilities, and other site elements that would be required by this title without the land banking.	The development may set aside the land area that would otherwise be needed in order to provide up to 25% of the number of required parking spaces.
<b>Adaptive Reuse of Older Buildings</b>	<b>Any use located in the Neighborhood Development Contexts<sup>1</sup>, except not drive-through service or vehicle-related uses.</b>	The development is a building expansion, alteration, or change of use, in an existing building that was originally permitted prior to June 13, 1978. The development does not convert housing units to non-residential uses.	Exemption from the first 10 percent increase in the total number of spaces required on the development site, for a maximum allowed exemption of five parking spaces. A parking reduction for adaptive reuse shall be used only once per individual building.
<b>Historic and Cultural Landmark Preservation</b>	<b>Any use that involves preservation of a landmark listed in the Anchorage local landmarks register.</b>	The development does not decrease the number of parking spaces that existed on the site as of <i>effective date of this ordinance</i> to	25% reduction in the number of required parking spaces.

	<u>less than the number otherwise required by this title.</u>	
<u>*Neighborhood Development Contexts include the Traditional Urban Neighborhood, Edge Urban Neighborhood, and Transit-Supportive Development Context areas delineated in section 21.15.015.</u>		

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~~[2][1][.]~~ ~~[Parking agreements.]~~ [A][H][ PARKING REDUCTION][s] [OR ALTERNATIVE SHALL] [REQUIRE A WRITTEN PARKING AGREEMENT BETWEEN THE PROPERTY OWNER(S) AND THE MUNICIPALITY][, subject to the following standards:] [EXCEPT WHERE EXPRESSLY STATED OTHERWISE.]

[A. RECORDATION. THE MUNICIPALITY SHALL RECORD THE PARKING AGREEMENT AT THE DISTRICT RECORDER'S OFFICE AS A COVENANT THAT RUNS WITH THE LAND AND IS BINDING ON THE OWNER AND ALL SUCCESSORS AND ASSIGNS FOR AS LONG AS THE REQUIRED NUMBER OF OFF-STREET PARKING SPACES IS NOT PROVIDED AS A RESULT OF THE PARKING REDUCTION ] [OR ALTERNATIVE. ALL PARTIES INVOLVED IN THE PARKING REDUCTION OR ALTERNATIVE SHALL PARTICIPATE IN THE PARKING AGREEMENT][. RECORDATION OF THE AGREEMENT SHALL TAKE PLACE BEFORE ISSUANCE OF AN ENTITLEMENT][ ~~that is contingent upon~~][REQUIRING][ A PARKING REDUCTION][ OR ALTERNATIVE].

[B. CONTENT. THE FORM][~~at (template)~~] [AND CONTENT][ OF THE PARKING AGREEMENT SHALL BE] [~~provided by the municipality, as~~] [APPROVED BY THE] [~~traffic engineer and~~] [DIRECTOR. ] [~~The parking agreement~~][IT] [SHALL GUARANTEE INSTALLATION AND MAINTENANCE OF ANY REQUIRED IMPROVEMENTS BY THE] [~~property~~] [OWNER, AND][OR] [THE OWNER'S CONTINUED PARTICIPATION IN ANY PARKING MANAGEMENT STRATEGY REQUIRED FOR A PARKING REDUCTION. ] [~~The parking agreement shall be accompanied by a site plan showing the parking and facilities required for the parking reduction.~~]

[~~c.~~ Additional Content for Agreements Subject to Discretionary Review. Discretionary parking reductions subject to 21.07.090F.8. shall be preceded by a formal letter requesting the reduction for concurrence by the traffic engineer and director. The letter shall include justification

1 ~~for the parking reduction, including any parking~~  
2 ~~demand study if prescribed by the traffic engineer.]~~  
3 ~~[THE PARKING AGREEMENT SHALL]~~ ~~include a~~  
4 ~~contingency plan, and shall guarantee~~ [ASSURE]  
5 [FUTURE IMPLEMENTATION OF] ~~the~~ [A]  
6 [CONTINGENCY PLAN BY THE] ~~property~~  
7 [OWNER IF SO ORDERED BY THE TRAFFIC  
8 ENGINEER. THE CONTINGENCY PLAN MAY  
9 INCLUDE STRATEGIES SUCH AS] [;]

10  
11 ~~[i. A deferred parking site plan]~~ [INSTALLATION  
12 OF PARKING,] [;]

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14 ~~[ii. P]~~ [PAYMENT TO THE MUNICIPALITY FOR  
15 THE FULL COST OF PROVIDING THE  
16 REQUIRED PARKING] [;].]

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18 ~~[iii. T]~~ [TRANSPORTATION DEMAND  
19 MANAGEMENT PROGRAMS] [;].] [OR]

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21 ~~[iv. O]~~ [OTHER PARKING MANAGEMENT  
22 STRATEGIES IDENTIFIED IN THE PARKING  
23 REDUCTIONS OR ALTERNATIVES] [OF THIS  
24 SECTION] [;].]

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26 ~~[d][C].~~ TERMINATION. ~~][The municipality may~~  
27 ~~terminate the parking agreement if it is~~  
28 ~~demonstrated that the parking reduction does not~~  
29 ~~support the parking utilization for the site, or if the~~  
30 ~~development is in violation of the parking~~  
31 ~~agreement. The traffic engineer or planning~~  
32 ~~director may require a contingency plan per AMC~~  
33 ~~21.07.090F.1.c. and a new parking agreement, or~~  
34 ~~the site may be required to provide all parking per~~  
35 ~~AMC 21.07.090E.]~~ [IF FOR ANY REASON THE  
36 PARKING AGREEMENT TERMINATES, OWNERS  
37 AND ALL SUCCESSORS AND ASSIGNS WHO ARE  
38 PARTIES TO THE PARKING AGREEMENT SHALL  
39 COMPLY WITH ALL PROVISIONS OF THIS TITLE  
40 GOVERNING THE REQUIRED NUMBER OF OFF-  
41 STREET PARKING SPACES.]

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43 ~~[3][2].~~ CALCULATION OF PARKING REDUCTIONS.]

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45 [A.] ~~[Calculation of ]~~ [MULTIPLE REDUCTIONS. A  
46 DEVELOPMENT MAY BE ELIGIBLE FOR ] ~~[more~~  
47 ~~than one parking reduction listed in this section~~  
48 ~~21.07.090F.]~~ [MULTIPLE REDUCTIONS FROM THE  
49 REQUIRED NUMBER OF PARKING SPACES.] [ THE

1 TOTAL IMPACT OF ~~][multiple]~~][PARKING  
2 REDUCTIONS ~~][on a development's parking~~  
3 ~~requirement ]~~][SHALL BE] ~~the sum of the individual~~  
4 ~~reductions]~~][CALCULATED AS BEING  
5 MULTIPLICATIVE AND NOT ADDITIVE WHERE A  
6 DEVELOPMENT IS ELIGIBLE FOR MORE THAN  
7 ONE. FOR EXAMPLE, IF ONE REDUCTION IS 20  
8 PERCENT, AND A SECOND REDUCTION IS AN  
9 ADDITIONAL 15 PERCENT, THEIR COMBINED  
10 REDUCTION SHALL BE CALCULATED AS 80  
11 PERCENT TIMES 85 PERCENT EQUALS 68  
12 PERCENT, FOR A 32 PERCENT AGE POINT TOTAL  
13 REDUCTION, RATHER THAN ADDING 20 PERCENT  
14 PLUS 15 PERCENT EQUALS 35 PERCENT. THIS IS  
15 BECAUSE THE 15 PERCENT REDUCTION APPLIES  
16 TO A BASE THAT IS ALREADY REDUCED 20  
17 PERCENT][.]

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19 **[b.]** ~~*Maximum Nondiscretionary Reduction. A request*~~  
20 ~~*for a greater than 25 percent reduction in the*~~  
21 ~~*required amount of parking from one or more*~~  
22 ~~*reductions listed in table 21.07-9 shall be subject*~~  
23 ~~*to discretionary review by the traffic engineer and*~~  
24 ~~*director, except that the following reductions listed*~~  
25 ~~*in table 21.07-9 are not subject to this percentage*~~  
26 ~~*limitation:*~~

27  
28 ~~i. Housing (Table 21.07-9).~~

29  
30 ~~ii. Land Banking (Table 21.07-9).~~

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32 ~~iii. Shared parking (21.07.090F.5.).~~

33  
34 ~~iv. Off-site parking (21.07.090F.6.).~~

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36 ~~**[c][B][.]**~~ ~~][Rounding of Fractional]~~][MINIMUM]  
37 ~~][REDUCTION][s]~~ [CREDIT OF ONE SPACE][.]  
38 ~~][Rounding of fractional numbers shall occur only~~  
39 ~~after subtracting the parking reduction(s) from the~~  
40 ~~minimum number of required parking spaces, as~~  
41 ~~provided in 21.07.090C.1]~~][IF THE TOTAL  
42 APPROVED REDUCTION FROM THE REQUIRED  
43 NUMBER OF PARKING SPACES FOR A  
44 DEVELOPMENT IS CALCULATED TO BE A  
45 REDUCTION OF LESS THAN ONE PARKING  
46 SPACE, IT SHALL BE CREDITED AS A REDUCTION  
47 OF ONE PARKING SPACE][.]

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2 **[4][3][.]** **[Pedestrian Access Improvements Required]**  
3 **[QUALIFYING SITE DEVELOPMENT] [.] [Developments**  
4 **shall improve pedestrian access as provided below, in**  
5 **order to be eligible for parking reductions.] [USES SHALL**  
6 **PROVIDE THE FOLLOWING ENHANCEMENTS TO BE**  
7 **ELIGIBLE FOR ANY REDUCTIONS IN THE NUMBER OF**  
8 **REQUIRED PARKING SPACES, EXCEPT WHERE STATED**  
9 **OTHERWISE. THE QUALIFYING SITE CRITERIA SHALL**  
10 **NOT BE REQUIRED FOR THE FOLLOWING PARKING**  
11 **REDUCTIONS AND ALTERNATIVES IN THIS**  
12 **SUBSECTION 21.07.090F: DOWNTOWN ANCHORAGE**  
13 **PARKING EXEMPTION, LAND BANKING, STACKED AND**  
14 **TANDEM PARKING, OR SMALLER PARKING SPACES**  
15 **FOR LOW-TURNOVER USES.] [INDUSTRIAL USES,**  
16 **PUBLIC SAFETY FACILITIES, TRANSPORTATION**  
17 **FACILITIES, AND UTILITY FACILITIES ARE EXEMPT**  
18 **FROM] [this subsection] [THE QUALIFYING SITE**  
19 **DEVELOPMENT CRITERIA][.]**  
20

21 **[A.]** **[Administrative Relief and Adjustment.**  
22 **The traffic engineer and director may approve**  
23 **administrative relief or adjustments to the**  
24 **standards of this subsection F.4. for changes of**  
25 **use or modifications to existing buildings and**  
26 **sites, as part of the review and approval of a**  
27 **parking reduction, provided the applicant**  
28 **demonstrates the adjustment is necessary to**  
29 **compensate for some practical difficulty of the site,**  
30 **or some unusual aspect of the site not shared by**  
31 **landowners in general. The justification for the**  
32 **administrative adjustment shall be recorded as an**  
33 **appendix to the parking reduction agreement in**  
34 **21.07.090F.2.] [STREET ORIENTED BUILDING.**  
35 **FOR BUILDINGS CONSTRUCTED AFTER JANUARY**  
36 **1, 2014, PRIMARY ENTRANCES AND/OR**  
37 **WINDOWS PROVIDING VISUAL ACCESS SHALL**  
38 **COMPRISE AT LEAST 15 PERCENT OF THE AREA**  
39 **OF ANY STREET FACING BUILDING ELEVATION.**  
40 **FOR NONRESIDENTIAL USES, WINDOWS**  
41 **PROVIDING VISUAL ACCESS AND/OR PRIMARY**  
42 **ENTRANCES SHALL COMPRISE AT LEAST 50**  
43 **PERCENT OF THE LENGTH AND 25 PERCENT OF**  
44 **THE GROUND-FLOOR WALL AREA OF ANY**  
45 **STREET FACING BUILDING ELEVATION.]**  
46

47 **[B.]** **WALKWAY] [and Sidewalk Access] [TO THE**  
48 **STREET][.]**

1 ~~**[Developments shall comply with subsections**~~  
2 ~~**21.07.060E.2., Sidewalks if applicable, and**~~  
3 ~~**21.07.060E.4., On-Site Pedestrian Walkways.]**~~ [A  
4 WALKWAY MEETING THE REQUIREMENTS OF  
5 SECTION 21.07.060 SHALL CONNECT AT LEAST  
6 ONE PRIMARY ENTRANCE TO A STREET. THE  
7 DIRECTOR AND THE TRAFFIC ENGINEER MAY  
8 WAIVE THIS REQUIREMENT IN SITUATIONS WITH  
9 EXISTING STRUCTURES WHERE IT IS  
10 DEMONSTRATED THAT THE ADDITION OF A  
11 WALKWAY WILL CAUSE A REDUCTION IN  
12 PARKING AND/OR LANDSCAPING BELOW  
13 REQUIRED LEVELS, OR WHERE THE WORK  
14 REQUIRED TO ADD A WALKWAY IS OUT OF  
15 PROPORTION WITH THE WORK BEING DONE TO  
16 EFFECT A CHANGE OF USE.]  
17

18 [C.] ~~**[Bicycle Parking ]**~~[PARKING FACILITY LOCATION].  
19 ~~**[Developments shall comply with subsection**~~  
20 ~~**21.07.090K., Bicycle Parking Spaces.]**~~[FOR  
21 BUILDINGS CONSTRUCTED AFTER JANUARY 1,  
22 2014, PARKING FACILITIES INCLUDING  
23 DRIVEWAYS SHALL COMPRISE NO MORE THAN  
24 50 PERCENT OF THE AREA BETWEEN THE  
25 STREET PROPERTY LINE AND THE STREET  
26 FACING BUILDING ELEVATION, AND GARAGE  
27 DOORS SHALL COMPRISE NO MORE THAN 50  
28 PERCENT OF THE LENGTH OF THE STREET  
29 FACING BUILDING ELEVATION. THESE  
30 REQUIREMENTS APPLY TO NO MORE THAN TWO  
31 STREET FRONTAGES.]  
32

33 [D. PRIVATE OPEN SPACE. FOR RESIDENTIAL  
34 DEVELOPMENTS THAT ARE REQUIRED TO  
35 PROVIDE PRIVATE OPEN SPACE, AN ADDITIONAL  
36 40 SQUARE FEET OF PRIVATE OPEN SPACE THAT  
37 MEETS THE REQUIREMENTS OF SECTION  
38 21.07.030 SHALL BE PROVIDED FOR EACH  
39 REDUCTION OF ONE PARKING SPACE IN  
40 DEVELOPMENTS BUILT AFTER JANUARY 1, 2014.  
41 THIS SHALL BE COMMON PRIVATE OPEN SPACE  
42 IN THE CASE OF MULTIFAMILY AND MIXED-USE  
43 DWELLINGS.  
44

45 E. CROSS-ACCESS TO ADJACENT PROPERTIES.  
46 THE DIRECTOR AND THE TRAFFIC ENGINEER  
47 MAY DETERMINE THERE IS POTENTIAL FOR  
48 DRIVEWAY OR WALKWAY CROSS-ACCESS TO  
49 ABUTTING PROPERTIES AND MAY REQUIRE A



1 CROSS-ACCESS FACILITY AND/OR EASEMENT  
2 WITHIN THE SUBJECT PROPERTY TO THE SITE  
3 BOUNDARY. VEHICULAR CROSS-ACCESS MAY  
4 ONLY BE REQUIRED IN COMMERCIAL DISTRICTS.]  
5

6 [4. *DOWNTOWN.* USES LOCATED IN DT DISTRICTS ARE  
7 EXEMPT FROM PROVIDING OFF-STREET PARKING  
8 SPACES. HOWEVER, IF PARKING IS PROVIDED, ALL  
9 OTHER STANDARDS OF THIS SECTION SHALL APPLY IN  
10 THE DT DISTRICTS, EXCEPT WHERE SPECIFICALLY  
11 STATED OTHERWISE. PARKING AGREEMENTS AND  
12 QUALIFYING SITE CRITERIA SHALL NOT BE REQUIRED  
13 FOR THIS EXEMPTION.  
14

15 5. *RESIDENCES IN WALKING DISTANCE TO DOWNTOWN.*  
16 RESIDENTIAL HOUSEHOLD USES LOCATED NORTH OF  
17 15TH AVENUE, WEST OF ORCA STREET, EAST OF L  
18 STREET, AND SOUTH OF SHIP CREEK ARE ELIGIBLE  
19 FOR A REDUCTION OF UP TO 25 PERCENT OF THE  
20 MINIMUM NUMBER OF REQUIRED PARKING SPACES.  
21

22 6. *DISTRICTS THAT PROMOTE A MIX OF USES.*  
23

24 A. USES LOCATED IN THE R-3A AND R-4A DISTRICTS  
25 ARE ELIGIBLE FOR A REDUCTION OF UP TO 10  
26 PERCENT OF THE MINIMUM NUMBER OF  
27 REQUIRED PARKING SPACES.  
28

29 B. USES LOCATED IN THE B-1A DISTRICT ARE  
30 ELIGIBLE FOR A REDUCTION OF UP TO 10  
31 PERCENT OF THE MINIMUM NUMBER OF  
32 REQUIRED PARKING SPACES, IF THE B-1A  
33 DISTRICT ABUTS RESIDENTIAL DISTRICTS ON  
34 THE MAJORITY OF ITS PERIMETER, AND HAS A  
35 CONTIGUOUS AREA OF NO MORE THAN ONE  
36 ACRE, EXCLUDING RIGHTS-OF-WAY. IN  
37 ADDITION, CERTAIN DEVELOPMENTS IN THE B-1A  
38 DISTRICT ARE ELIGIBLE FOR A SEPARATE  
39 PARKING REDUCTION AS SPECIFIED IN THE  
40 MIXED-USE DEVELOPMENT OR OVERLAY  
41 DISTRICT STANDARDS OF CHAPTER 21.04. SUCH  
42 REDUCTIONS SHALL BE REVIEWED AND  
43 ADMINISTERED UNDER THIS SECTION  
44 21.07.090F.  
45

46 C. CERTAIN DEVELOPMENTS IN THE B-1B AND B-3  
47 DISTRICTS ARE ELIGIBLE FOR A REDUCTION THE  
48 MINIMUM NUMBER OF REQUIRED PARKING  
49 SPACES, AS SPECIFIED IN THE MIXED-USE

1 DEVELOPMENT STANDARDS OR OVERLAY  
2 DISTRICT STANDARDS OF CHAPTER 21.04. SUCH  
3 REDUCTIONS SHALL BE REVIEWED AND  
4 ADMINISTERED UNDER THIS SECTION  
5 21.07.090F.]  
6

7 [7. *RESIDENCES IN CENTER CITY NEIGHBORHOODS.*

8  
9 A. RESIDENTIAL HOUSEHOLD USES LOCATED IN  
10 CENTER CITY NEIGHBORHOODS ARE ELIGIBLE  
11 FOR A REDUCTION OF UP TO 10 PERCENT OF  
12 THE MINIMUM NUMBER OF REQUIRED PARKING  
13 SPACES.  
14

15 B. FOR THE PURPOSES OF THIS PROVISION, THE  
16 CENTER CITY AREA IS BOUNDED TO THE NORTH  
17 BY JOINT BASE ELMENDORF-RICHARDSON, TO  
18 THE SOUTH BY TUDOR ROAD, TO THE EAST BY  
19 INGRA STREET AND THE SEWARD HIGHWAY,  
20 AND TO THE WEST BY MINNESOTA DRIVE. ANY  
21 PART OF FAIRVIEW, SOUTH ADDITION,  
22 GOVERNMENT HILL, OR MOUNTAIN VIEW  
23 COMMUNITY COUNCIL IS ALSO IN THE ELIGIBLE  
24 AREA.  
25

26 C. THIS REDUCTION RECOGNIZES PROXIMITY TO  
27 EMPLOYMENT CENTERS, CHARACTERISTICS  
28 SUCH AS TRADITIONAL STREET GRIDS AND  
29 DEVELOPMENT PATTERNS, HOUSEHOLD  
30 CHARACTERISTICS, EMPHASIS ON WALKABLE  
31 NORTHERN CITY ENVIRONMENTS, AND LOWER  
32 PARKING DEMAND IN THESE AREAS.  
33

34 8. *USES ADJACENT TO TRANSIT SERVICE.* A USE IS  
35 ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT  
36 OF THE MINIMUM NUMBER OF REQUIRED PARKING  
37 SPACES IF IT IS LOCATED WITHIN 800 FEET OF THE  
38 STREET RIGHT-OF-WAY CENTERLINE OF ANY  
39 MUNICIPAL PUBLIC TRANSIT ROUTE, SUBJECT TO  
40 APPROVAL BY THE TRAFFIC ENGINEER AND THE  
41 DIRECTOR. THE PUBLIC TRANSPORTATION  
42 DEPARTMENT MAY REQUIRE A PUBLIC USE EASEMENT  
43 OR TRANSIT STOP AND/OR TRANSIT SHELTER  
44 IMPROVEMENTS IF THE SUBJECT PROPERTY ABUTS  
45 AN EXISTING OR PLANNED TRANSIT STOP. IF THE  
46 PUBLIC TRANSPORTATION DEPARTMENT REQUIRES  
47 SUCH AN EASEMENT OR IMPROVEMENTS, THEN THE  
48 USE IS ELIGIBLE FOR AN ADDITIONAL REDUCTION OF

1 TWO PERCENT OR ONE MORE PARKING SPACE,  
2 WHICHEVER IS GREATER.

3  
4 9. *RIDESHARE PROGRAMS.* A NONRESIDENTIAL USE IS  
5 ELIGIBLE TO SUBSTITUTE PARTICIPATION IN  
6 MUNICIPAL CARPOOL OR VANPOOL RIDESHARE  
7 PROGRAM UP TO A MAXIMUM OF FIVE PERCENT OF  
8 THE MINIMUM NUMBER OF REQUIRED PARKING  
9 SPACES. [THE LAND AREA THAT WOULD OTHERWISE  
10 BE NEEDED IN ORDER TO PROVIDE THE REQUIRED  
11 NUMBER OF PARKING SPACES SHALL BE SET ASIDE ON  
12 THE SITE TO PROVIDE FOR THE FUTURE  
13 CONSTRUCTION OF A PARKING LOT IN CONFORMANCE  
14 WITH SUBSECTION 21.07.090F.12., LAND BANKING.

15  
16 A. *CARPOOL.* EVERY DESIGNATED CARPOOL  
17 SPACE MAY COUNT AS 1.8 SPACES TOWARD  
18 MEETING THE MINIMUM NUMBER OF REQUIRED  
19 PARKING SPACES. THE CARPOOL SPACES SHALL  
20 BE THOSE CLOSEST TO THE PRIMARY  
21 ENTRANCE OR ELEVATOR, BUT NOT CLOSER  
22 THAN ACCESSIBLE SPACES OR THOSE SIGNED  
23 FOR EXCLUSIVE CUSTOMER/VISITOR USE.  
24 SIGNS SHALL BE POSTED INDICATING THESE  
25 SPACES ARE RESERVED FOR CARPOOL USE.  
26 THE TRAFFIC ENGINEER SHALL CONSULT WITH  
27 THE PUBLIC TRANSPORTATION DEPARTMENT IN  
28 PROVIDING CARPOOL SPACES AND THE  
29 LOCATION OF CARPOOL PARKING.

30  
31 B. *VANPOOL.* FOR EVERY VANPOOL PURCHASED  
32 OR LEASED BY THE APPLICANT FOR EMPLOYEE  
33 USE OPERATED THROUGH THE MUNICIPAL  
34 RIDESHARE PROGRAM, THE NUMBER OF  
35 REQUIRED PARKING SPACES SHALL BE  
36 REDUCED BY UP TO SIX SPACES. THE TRAFFIC  
37 ENGINEER MAY REQUIRE A SAFE AND  
38 CONVENIENT DESIGNATED VANPOOL  
39 PASSENGER LOADING ZONE.

40  
41 10. *TRANSIT PASS BENEFITS.* A USE IN WHICH THE OWNER  
42 OR EMPLOYER OFFERS TRANSIT PASSES COST-FREE  
43 TO ALL EMPLOYEES OR RESIDENTS IS ELIGIBLE FOR A  
44 PARKING REDUCTION OF UP TO 5 PERCENT OF THE  
45 MINIMUM NUMBER OF REQUIRED PARKING SPACES.  
46 THE USE SHALL BE LOCATED WITHIN 800 FEET OF THE  
47 STREET RIGHT-OF-WAY CENTERLINE OF ANY  
48 MUNICIPAL TRANSIT ROUTE. THE PUBLIC  
49 TRANSPORTATION DEPARTMENT MAY REQUIRE A

1 PUBLIC USE EASEMENT OR TRANSIT STOP AND/OR  
2 TRANSIT SHELTER IMPROVEMENTS IF THE SUBJECT  
3 PROPERTY ABUTS AN EXISTING OR PLANNED TRANSIT  
4 STOP. IF THE PUBLIC TRANSPORTATION DEPARTMENT  
5 REQUIRES SUCH AN EASEMENT OR IMPROVEMENTS,  
6 THEN THE USE IS ELIGIBLE FOR AN ADDITIONAL  
7 REDUCTION OF TWO PERCENT OR ONE MORE  
8 PARKING SPACE, WHICHEVER IS GREATER.  
9

10 11. *PARKING CASH-OUTS*. A USE IS ELIGIBLE FOR A  
11 REDUCTION OF UP TO 10 PERCENT OF THE MINIMUM  
12 NUMBER OF REQUIRED PARKING SPACES IF IT  
13 IMPLEMENTS A PARKING CASH-OUT PROGRAM BY  
14 WHICH COMMUTERS ARE PROVIDED THE OPTION TO  
15 CHOOSE BETWEEN FREE PARKING AND ITS  
16 EQUIVALENT CASH VALUE FOR USING AN  
17 ALTERNATIVE MODE OF TRAVEL.  
18

19 12. *LAND BANKING*. SUBJECT TO APPROVAL BY THE  
20 TRAFFIC ENGINEER AND THE DIRECTOR, THE LAND  
21 AREA THAT WOULD OTHERWISE BE NEEDED IN ORDER  
22 TO PROVIDE UP TO 25 PERCENT OF THE MINIMUM  
23 NUMBER OF REQUIRED PARKING SPACES MAY BE SET  
24 ASIDE ON THE SITE TO PROVIDE FOR THE FUTURE  
25 CONSTRUCTION OF A PARKING FACILITY. THE  
26 APPLICANT SHALL SUBMIT A PARKING DEMAND STUDY  
27 PREPARED IN A FORM AND MANNER PRESCRIBED BY  
28 THE TRAFFIC ENGINEER THAT INDICATES THE  
29 REDUCED PARKING LOT WILL ACCOMMODATE  
30 EXPECTED PARKING NEEDS, AND AN ALTERNATE SITE  
31 PLAN TO BE APPROVED BY THE TRAFFIC ENGINEER  
32 THAT ACCOMMODATES THE PARKING THAT WOULD BE  
33 REQUIRED WITHOUT THE LAND BANKED PARKING  
34 REDUCTION. THE AREA SET ASIDE SHALL BE  
35 LANDSCAPED WITH SITE ENHANCEMENT  
36 LANDSCAPING AND/OR PEDESTRIAN AMENITIES  
37 APPROVED BY THE DIRECTOR. THE PARKING  
38 AGREEMENT SHALL GUARANTEE THAT, IF THE  
39 DIRECTOR AND THE TRAFFIC ENGINEER DETERMINE  
40 AT SOME POINT IN THE FUTURE THAT ADDITIONAL  
41 PARKING SPACES ARE NEEDED, THE OWNER SHALL  
42 CONSTRUCT PARKING ON THE LAND BANKED AREA IN  
43 CONFORMANCE WITH THE ALTERNATE SITE PLAN.  
44

45 13. *AFFORDABLE HOUSING*. AFFORDABLE HOUSING UNITS  
46 THAT ARE DEED-RESTRICTED FOR HOUSEHOLDS  
47 HAVING AN INCOME AT THE TIME OF INITIAL  
48 OCCUPANCY OF 30 PERCENT OR LESS OF MEDIAN  
49 FAMILY INCOME ARE ELIGIBLE FOR A REDUCTION OF

1 UP TO 30 PERCENT OF THE MINIMUM NUMBER OF  
2 REQUIRED PARKING SPACES. AFFORDABLE HOUSING  
3 UNITS FOR LOW INCOME HOUSEHOLDS HAVING AN  
4 INCOME AT THE TIME OF INITIAL OCCUPANCY OF 60  
5 PERCENT OR LESS OF MEDIAN FAMILY INCOME ARE  
6 ELIGIBLE FOR A REDUCTION OF UP TO 15 OF THE  
7 MINIMUM NUMBER OF REQUIRED PARKING SPACES.  
8 THE AFFORDABLE HOUSING UNITS SHALL BE  
9 CONSISTENT WITH THE STANDARDS OF SUBSECTION  
10 21.07.110H., STANDARDS FOR AFFORDABLE HOUSING.

11  
12 14. *SENIOR HOUSING.* DWELLING UNITS THAT MEET THE  
13 DEFINITION OF SENIOR HOUSING ARE ELIGIBLE FOR A  
14 REDUCTION OF UP TO 15 PERCENT OF THE MINIMUM  
15 NUMBER OF REQUIRED PARKING SPACES. DWELLING  
16 UNITS THAT MEET THE DEFINITION OF SENIOR  
17 HOUSING THAT IS INTENDED FOR, AND SOLELY  
18 OCCUPIED BY, PERSONS 62 YEARS OF AGE OR OLDER  
19 ARE ELIGIBLE FOR A REDUCTION OF UP TO 25  
20 PERCENT OF THE MINIMUM NUMBER OF REQUIRED  
21 PARKING SPACES.

22  
23 15. *HOUSING DENSITY.* RESIDENTIAL HOUSEHOLD USES  
24 ARE ELIGIBLE FOR A REDUCTION OF ONE PERCENT OF  
25 THE MINIMUM NUMBER OF REQUIRED PARKING  
26 SPACES FOR EVERY FOUR DWELLINGS PER ACRE  
27 ABOVE A NET DENSITY OF 40 DWELLINGS PER ACRE  
28 ON THE SITE, UP TO A MAXIMUM REDUCTION OF 20  
29 PERCENT OF THE MINIMUM NUMBER OF REQUIRED  
30 PARKING SPACES.]

31  
32 ~~[5][16][.] **[Parking Reduction for ]**][**SHARED PARKING.**]  
33 **[Where two or more land uses that have different peak**  
34 **parking utilization time periods share the same parking**  
35 **facility, the total off-street parking required for those uses**  
36 **may be reduced, as provided below.**][SHARED USE OF  
37 REQUIRED PARKING SPACES MAY OCCUR WHERE TWO  
38 OR MORE USES ON THE SAME OR SEPARATE SITES  
39 ARE ABLE TO SHARE THE SAME PARKING SPACES  
40 BECAUSE THEIR PEAK PARKING DEMANDS OCCUR AT  
41 DIFFERENT TIMES THE TRAFFIC ENGINEER AND  
42 DIRECTOR MAY APPROVE SHARED PARKING  
43 FACILITIES FOR USES WITH DIFFERENT PEAK  
44 BUSINESS PERIODS IF THE SHARED PARKING  
45 COMPLIES WITH ALL OF THE FOLLOWING STANDARDS:]~~

46  
47 **[A. SHARED PARKING ]****[Standard**  
48 **Calculation]****[STUDY][.] ~~[Where up to three separate~~**  
49 **land uses listed in Table 21.07-10, Shared Parking**

~~**Credit, share a parking facility, the total off-street parking required for those uses is eligible to be reduced by the percentage factors shown in subsection b., Table 21.07-10, subject to the standards in subsections d. through j. Where four or more land uses listed in Table 21.07-10 share a parking facility, the shared parking reduction is subject to discretionary review and approval by the Traffic Engineer and Director as provided in 21.07.090F.8.]**~~ [THE APPLICANT SHALL SUBMIT A SHARED PARKING STUDY TO THE DIRECTOR THAT DEMONSTRATES THE FEASIBILITY OF SHARED PARKING. THE STUDY SHALL BE PROVIDED IN A FORM ESTABLISHED BY THE TRAFFIC ENGINEER AND SHALL BE MADE AVAILABLE TO THE PUBLIC. THE STUDY SHALL DEMONSTRATE THAT ANY PARKING REDUCTION REQUESTED WILL NOT RESULT IN THE SPILLOVER OF PARKING ONTO OTHER PROPERTIES OR PUBLIC STREETS, BY, AT A MINIMUM, ADDRESSING THE FOLLOWING: THE SIZE AND TYPE OF THE PROPOSED DEVELOPMENT AND THE COMPOSITION OF USES, LOCATION OF REQUIRED PARKING, THE COMPOSITION OF TENANTS, THE ANTICIPATED RATE OF PARKING TURNOVER, AND THE ANTICIPATED PEAK PARKING AND TRAFFIC LOADS FOR ALL USES THAT WILL BE SHARING OFF-STREET PARKING SPACES.]

[B. *CALCULATION OF PARKING SPACES REQUIRED.* THE SHARED PARKING STUDY SHALL ONE OF THE FOLLOWING PROCEDURES:

- I. THE METHOD UNDER SUBSECTION 16.C.;
- II. THE MOST CURRENT PUBLISHED PROCEDURES OF THE URBAN LAND INSTITUTE OR THE INSTITUTE OF TRANSPORTATION ENGINEERS; OR
- III. OTHER PROCEDURES BASED ON INDUSTRY DATA OR OTHER SUFFICIENT EVIDENCE AND ANALYSIS OF PEAK PARKING DEMAND, AS SPECIFICALLY APPROVED BY THE TRAFFIC ENGINEER.]

[b][C][.] ~~**[Computation of Shared Parking Standard]**~~  
[ALTERNATIVE] ~~**[CALCULATION METHOD.]**~~ ~~**The**~~

following steps shall be used to calculate a shared parking reduction. The Planning Department shall maintain a publicly available worksheet form online that applicants may access and use to run calculations. Calculation steps:

i. Determine the minimum amount of parking required for each use, as set forth in subsection 21.07.090E., Off-street Parking Requirements, using the calculation rules established in subsection 21.07.090C., Computation of Parking and Loading Requirements.

ii. For each use, select the appropriate matching land use category in table 21.07-10, Shared Parking Credits.

iii. Multiply the minimum amount of required parking for each use, as set forth in subsection 21.07.090E., by the appropriate percentage shown in table 21.07-10, for each of the eight time periods in the table, to estimate the typical parking demand generated by that use during each of the eight time periods.

iv. Add the resulting products from the uses for each of the eight columns (time periods). Include each time period shown in the table, including during hours when the proposed business will be closed.

v. The highest sum among the eight columns becomes the shared parking requirement. This represents the time period that is expected to generate the highest total parking demand.]

[FOR EACH USE SHARING THE PARKING FACILITY, CALCULATE THE NUMBER OF OFF-STREET PARKING SPACES REQUIRED FOR THAT USE IN TABLE 21.07-4. MULTIPLY THAT NUMBER ACROSS THE ROW FOR ITS LAND USE IN TABLE 21.07-5, SHARED PARKING CREDIT REQUIRED FOR THAT USE DURING THE EIGHT TIME PERIODS. FOR EACH TIME PERIOD, ADD THE RESULTING PRODUCTS FOR EACH OF THE USES SHARING THE PARKING. THE

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COLUMN TOTAL THAT GENERATES THE HIGHEST NUMBER OF PARKING SPACES THEN BECOMES THE SHARED PARKING REQUIREMENT. THIS REPRESENTS THE TIME PERIOD WITH THE HIGHEST TOTAL PARKING DEMAND.]

[TABLE 21.07-][10][5]: SHARED PARKING CREDIT								
[LAND USES] [4] [12]	[WEEKDAY TIME PERIODS]				[WEEKEND TIME PERIODS]			
	[7 am to 6 pm]	[6 PM TO 1 AM]	[1 AM TO 3 AM]	[3 AM TO 7 AM]	[7 AM TO 6 PM]	[6 PM TO 1 AM]	[1 AM TO 3 AM]	[3 AM TO 7 AM]
[RESIDENTIAL]	[65%]	[100%]	[100%]	[100%]	[75%]	[90%]	[10%]	[100%]
[RELIGIOUS ASSEMBLY]	[25%]	[50%]	[0%]	[0%]	[100%]	[50%]	[0%]	[0%]
[Childcare; K-12 school]	[100%]	[20%]	[0%]	[0%]	[20%]	[10%]	[0%]	[0%]
[HEALTH SERVICES]	[100%]	[30%]	[5%]	[5%]	[100%]	[0%]	[0%]	[0%]
[ASSEMBLY]	[100%]	[50%]	[5%]	[5%]	[100%]	[50%]	[5%]	[5%]
[FITNESS CENTER]	[90%]	[100%]	[25][60][%]	[60%]	[100%]	[100%]	[25][80][%]	[60][80][%]
[MOVIE THEATER]	[60%]	[100%]	[0%]	[0%]	[80%]	[100%]	[0%]	[0%]
[BAR OR NIGHTCLUB]	[40%]	[100%]	[90%]	[0%]	[50%]	[100%]	[90%]	[0%]
[RESTAURANT]	[80%]	[100%]	[50%]	[50%]	[85%]	[100%]	[25%]	[25%]
[RESTAURANT – DRIVE-THROUGH]	[100%]	[90%]	[15%]	[15%]	[100%]	[80%]	[15%]	[15%]
[OFFICE OR FINANCIAL][1] or Government administr.]	[100%]	[10%]	[0%]	[5%]	[15%]	[0%]	[0%]	[0%]
[RETAIL SALES ][/][1] Personal services]	[100%]	[80%]	[0%]	[0%]	[100%]	[60%]	[0%]	[0%]
[VISITOR ACCOMMODATIONS] [(guest rooms)]	[75%]	[100%]	[100%]	[100%]	[75%]	[100%]	[100%]	[100%]
[Industrial service, manufacturing, or warehouse/storage]	[100%]	[10%]	[0%]	[5%]	[15%]	[0%]	[0%]	[0%]

[NOTES:] [4][12] [IF ONE OR MORE OF THE LAND USES PROPOSED TO MAKE USE OF SHARED PARKING FACILITIES DO NOT CONFORM TO THE LAND USE CLASSIFICATIONS IN THIS TABLE, AS DETERMINED BY THE DIRECTOR, THEN THE APPLICANT SHALL SUBMIT SUFFICIENT DATA TO INDICATE THE PERIODS OF PEAK PARKING DEMAND FOR THE USES. BASED ON THIS INFORMATION, THE TRAFFIC ENGINEER SHALL DETERMINE THE APPROPRIATE SHARED PARKING REQUIREMENT.]

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**c. Alternative Shared Parking Calculation. The applicant may request a greater reduction in the total number of spaces required for two or more land uses where Table 21.07-10 does not adequately account for circumstances or mix of use types specific to the development, subject to review and approval by the traffic engineer and director as provided in 21.07.090F.8. The applicant shall submit a shared parking study following 21.07.090F.8.c. and:**



- i. ~~The most current published procedures of the Urban Land Institute or the Institute of Transportation Engineers; or~~
- ii. ~~Other procedures based on industry data or other sufficient evidence and analysis of peak parking demand, as specifically approved by the traffic engineer.]~~

[D. ***DISTANCE TO PARKING SPACES.*** ~~[Shared parking spaces may be located off-site, subject to the standards in this subsection 21.07.090F.5.]~~ **[SHARED PARKING SPACES FOR RESIDENTIAL UNITS SHALL BE LOCATED WITHIN 500 FEET OF THE DWELLING UNIT ENTRANCE THEY SERVE. SHARED SPACES FOR OTHER USES SHALL BE WITHIN 800 FEET OF A PRIMARY ENTRANCE OF THE USES SERVED.]** ~~[Distance shall be measured along the pedestrian connection required in subsection 21.07.090F.5.e.]~~ **[THE TRAFFIC ENGINEER AND THE DIRECTOR MAY APPROVE A PORTION OF SHARED PARKING SPACES AT A GREATER DISTANCE BASED ON FACTORS SUCH AS THE PEDESTRIAN ENVIRONMENT, AVAILABILITY OF ATTENDANT PARKING, WEATHER PROTECTION, AND THE TYPE OF USE SERVED.]**

[E. ***PEDESTRIAN CONNECTION. CLEAR AND SAFE PEDESTRIAN WALKWAYS***] ~~[conforming to the standards of 21.07.060E]~~. **SHALL CONNECT THE SHARED PARKING FACILITY AND THE PRIMARY ENTRANCES OF THE USES IT SERVES. THE TRAFFIC ENGINEER MAY REQUIRE]** ~~[sidewalk or]~~ **[PEDESTRIAN STREET CROSSING IMPROVEMENTS.]**

[F. ***SEPARATION BY STREETS.*** **SEPARATION OF A USE AND ITS SHARED PARKING FACILITY BY A LOCAL STREET IS ALLOWED]**, ~~subject to discretionary review and approval by the traffic engineer in 21.07.090F.8]~~. **SHARED PARKING SPACES SHALL NOT BE SEPARATED FROM THE SERVED USE BY A COLLECTOR OR GREATER CLASSIFICATION STREET, UNLESS APPROVED BY THE TRAFFIC ENGINEER WITH CONSIDERATION OF THE EASE AND SAFETY OF PEDESTRIAN ACCESS, OR AS] [SPECIFICALLY ]**

1 [ALLOWED BY A COMPREHENSIVE PLAN  
2 ELEMENT SPECIFIC TO AN AREA OR DISTRICT.]  
3

4 [G. *RESIDENTIAL NEIGHBORHOODS.* A  
5 NONRESIDENTIAL USE SHALL NOT PARTICIPATE  
6 IN A SHARED PARKING FACILITY] [THAT IS]  
7 [LOCATED IN A RESIDENTIAL DISTRICT, IF THE  
8 USE] [ITSELF] [IS NOT PERMITTED IN THE  
9 RESIDENTIAL DISTRICT.]  
10

11 [H. *INSTRUCTIONAL SIGNS.* THE SHARED PARKING  
12 FACILITY SHALL PROVIDE INSTRUCTIONAL  
13 SIGNS ON THE PREMISES INDICATING THE  
14 AVAILABILITY OF THE FACILITY FOR PATRONS  
15 OF THE USES IT SERVES.]  
16

17 [I. *SHARED PARKING PLAN.* A SHARED PARKING  
18 PLAN SHALL BE SUBMITTED FOR REVIEW AND  
19 APPROVAL [BY THE TRAFFIC ENGINEER AND  
20 THE DIRECTOR]. THE SHARED PARKING PLAN  
21 MAY BE COMBINED WITH OTHER PARKING  
22 PLANS REQUIRED BY THIS TITLE.]  
23

24 [J. *CHANGES IN USE OR SHARED PARKING*  
25 *FACILITY.* ANY SUBSEQUENT CHANGE TO THE  
26 SHARED PARKING FACILITY OR] [~~any~~][IN] [USE  
27 TYPE] [~~in the building(s)~~] [SHALL REQUIRE A  
28 REVIEW] [BY THE DEPARTMENT AND THE  
29 TRAFFIC ENGINEER] [FOR COMPLIANCE WITH  
30 THIS SECTION, INCLUDING PROOF THAT  
31 SUFFICIENT PARKING WILL BE AVAILABLE. ANY  
32 CHANGE SHALL BE APPROVED] [~~and if necessary~~  
33 ~~a modification to the existing shared parking~~  
34 ~~agreement shall be made~~] [PRIOR TO] [~~the change~~]  
35 [BEING IMPLEMENTED.]  
36

37 [K. *EXPIRATION.* NOTWITHSTANDING F.1.A. ABOVE,  
38 A SHARED PARKING AGREEMENT MAY BE  
39 RECORDED FOR A TIME CERTAIN PERIOD, NOT  
40 TO BE LESS THAN TEN YEARS. AT THE END OF  
41 THE LIFE OF THE AGREEMENT, PROPERTY  
42 OWNERS WHO ARE PARTIES TO THE  
43 AGREEMENT SHALL COMPLY WITH ALL  
44 PROVISIONS OF THIS CODE GOVERNING THE  
45 REQUIRED NUMBER OF OFF-STREET PARKING  
46 SPACES.]

1  
2 **[6][17][.]** ~~**[Parking reductions for] [OFF-SITE PARKING][A**~~  
3 ~~**use's required parking may be located on a lot different**~~  
4 ~~**from the lot containing the use, as provided below.]**~~ [THE  
5 TRAFFIC ENGINEER AND THE DIRECTOR MAY  
6 APPROVE THE LOCATION OF REQUIRED PARKING  
7 SPACES ON A SEPARATE LOT FROM THE PRINCIPAL  
8 USE IF THE OFF-SITE PARKING COMPLIES WITH ALL OF  
9 THE FOLLOWING STANDARDS:]

10  
11 **[A.]** ~~**[Off-site Parking – Nondiscretionary Approval]**~~  
12 ~~**[ACCESSIBLE PARKING SPACES]. [A principal use**~~  
13 ~~**is eligible for a non-discretionary parking**~~  
14 ~~**reduction for off-site parking, provided the off-site**~~  
15 ~~**required parking for the use is off-street, located**~~  
16 ~~**on a lot not separated by a street from the principal**~~  
17 ~~**use and in conformance with the standards below.]**~~  
18 [REQUIRED ACCESSIBLE PARKING SPACES  
19 SHALL NOT BE LOCATED OFF-SITE.]

20  
21 **[B.]** ~~**[Distance to Off-site Spaces][LOCATION][.] [Off-**~~  
22 ~~**site parking spaces for residential units shall be**~~  
23 ~~**located within 500 feet of the dwelling unit entrance**~~  
24 ~~**they serve. Off-site spaces for other uses shall be**~~  
25 ~~**within 800 feet of a primary entrance of the uses**~~  
26 ~~**served. Distance shall be measured along the**~~  
27 ~~**pedestrian connection in c ]**~~ [THE MAXIMUM  
28 DISTANCE BETWEEN OFF-SITE PARKING SPACES  
29 AND THE USE(S) SERVED SHALL BE THE SAME AS  
30 PROVIDED IN SUBSECTION 21.07.090F.16.D. FOR  
31 SHARING PARKING SPACES (MEASURED ALONG  
32 THE SHORTEST LEGAL PEDESTRIAN ROUTE).  
33 SEPARATION OF A USE AND ITS OFF-SITE  
34 PARKING SPACES BY A STREET SHALL BE  
35 SUBJECT TO SUBSECTION 21.07.090F.16.F].

36  
37 **[C.]** ~~**PEDESTRIAN CONNECTION. CLEAR AND SAFE**~~  
38 ~~**PEDESTRIAN WALKWAYS]**~~ ~~**[conforming to the**~~  
39 ~~**standards of 21.07.060E.]**~~ [SHALL CONNECT THE  
40 OFF-SITE PARKING FACILITY AND THE PRIMARY  
41 ENTRANCE(S) OF THE USES SERVED. THE  
42 TRAFFIC ENGINEER MAY REQUIRE SIDEWALK  
43 OR PEDESTRIAN CROSSING IMPROVEMENTS TO  
44 ENHANCE PEDESTRIAN SAFETY OR MOBILITY  
45 TO AND FROM THE OFF-SITE PARKING.]

46  
47 **[d.]** ~~**Separation by Streets. Separation of a use and its**~~  
48 ~~**off-site parking spaces by a local street is allowed,**~~  
49 ~~**subject to discretionary review and approval by the**~~

~~traffic engineer in 21.07.090F.8. Off-site parking spaces shall not be separated from the served use by a collector or greater classification street, unless approved by the traffic engineer with consideration of the ease and safety of pedestrian access, or as specifically allowed by an area-specific element of the comprehensive plan.]~~

[e][D]. **INSTRUCTIONAL SIGNS.** INSTRUCTIONAL SIGNS SHALL BE POSTED ON THE PRINCIPAL SITE PROVIDING NOTICE OF THE AVAILABILITY AND LOCATION OF ADDITIONAL PARKING. THE OFF-SITE PARKING FACILITY SHALL PROVIDE INSTRUCTIONAL SIGNS INDICATING THE AVAILABILITY OF THE FACILITY FOR PATRONS OF THE USES IT SERVES.]

[f][E]. **RESIDENTIAL NEIGHBORHOODS.** A NONRESIDENTIAL USE SHALL NOT PARTICIPATE IN AN OFF-SITE PARKING FACILITY [THAT IS ]LOCATED IN A RESIDENTIAL DISTRICT, IF THE USE [ITSELF ]IS NOT PERMITTED IN THE RESIDENTIAL DISTRICT.]

~~g. **Required Off-site Spaces to be Counted for One Use Only.** The off-site vehicle parking spaces shall not be required parking spaces for any other use, except as provided in 21.07.090F.5. (Shared Parking) or F.7. (District Parking).]~~

[7][18][.] ~~[Parking Reduction for Nearby Public] [DISTRICT][PARKING. THE TRAFFIC ENGINEER MAY REDUCE THE MINIMUM NUMBER OF REQUIRED OFF-STREET PARKING SPACES FOR USES] [near a public parking facility where the parking spaces are managed and maintained for shared public use][WITHIN THE BOUNDARIES OF A MUNICIPALLY RECOGNIZED PUBLIC PARKING DISTRICT THAT PROVIDES OFF-SITE PARKING FACILITIES TO SERVE AN AREA][. TO DETERMINE ELIGIBILITY FOR THIS REDUCTION OR THE SIZE OF THE REDUCTION TO BE ALLOWED, THE TRAFFIC ENGINEER SHALL CONSIDER] [the relative distance to the use from the district parking facility and the factors listed in 21.07.090F.8.b.] [FACTORS SUCH AS:~~

- A. PEAK HOURS OF USE AND TURNOVER RATE;
- B. THE ABILITY OF THE USE TO MEET THE PARKING REQUIREMENT THROUGH OTHER MEANS;

- C. THE AVAILABILITY OF SPACES IN THE NEARBY DISTRICT PARKING FACILITY;
- D. THE RELATIVE DISTANCE TO THE USE FROM THE DISTRICT PARKING FACILITY; AND
- E. MEASURES PROVIDED BY THE APPLICANT TO ENSURE EMPLOYEE AND PATRON USE OF THE DISTRICT PARKING FACILITY, AND EASE AND SAFETY OF PEDESTRIAN ACCESS.]

**[8. Discretionary Parking Reductions. Subject to discretionary review and approval by the traffic engineer and director, applicants may request greater percentage reductions than shown in Table 21.07-9 and subsections F.5. through F.7., propose other types of parking reduction strategies besides those listed, or request departures from the specific standards for the parking reductions in F.5. through F.7. The applicant shall demonstrate the proposed reduction is appropriate based on the expected parking utilization rate of the development and the factors below.**

**a. Examples of Discretionary Parking Reductions. Examples of other parking reduction strategies besides those listed that may be proposed include other shared vehicle programs (e.g., bike-share), other parking pricing strategies, other uses that have low parking utilization, or other transportation demand management (TDM) programs.**

**b. Discretionary Approval Criteria. To determine eligibility for a discretionary reduction or the size of the reduction to be allowed, the traffic engineer and director shall consider factors such as:**

**i. The characteristics of the proposed use, travel behavior and anticipated peak parking utilization by users, peak hours of use, and parking turnover rate;**

**ii. The ability of the use to meet the parking requirement through other means, such as existing or potential shared parking agreements or other parking strategies;**



1 REQUIRED NUMBER OF OFF-STREET PARKING  
2 SPACES, INCLUDING GUEST PARKING SPACES. IN  
3 ADDITION, AS DETERMINED BY THE TRAFFIC  
4 ENGINEER, A PORTION OF THE REMAINING ON-STREET  
5 PARKING SPACES LOCATED WITHIN THE MAXIMUM  
6 DISTANCE PROVIDED IN SUBSECTION 21.07.090F.16.D.  
7 FOR SHARED PARKING SPACES MAY BE COUNTED  
8 TOWARD THE MINIMUM REQUIRED OFF-STREET  
9 PARKING SPACES, IN AN AMOUNT CONSISTENT WITH A  
10 FAIR APPORTIONMENT OF ON-STREET PARKING  
11 SPACES AMONG THE PROPERTIES ON THE STREET.  
12 UPON APPROVAL, EACH ON-STREET SPACE MAY BE  
13 SUBSTITUTED FOR ONE REQUIRED OFF-STREET  
14 SPACE. THE PROVISIONS APPLY ONLY TO STREET  
15 FRONTAGES WHERE ON-STREET PARKING IS  
16 ALLOWED. DETERMINATION OF THE LOCATION AND  
17 DIMENSIONS OF ON-STREET PARKING SPACES TO BE  
18 COUNTED TOWARD THE PARKING REQUIREMENT  
19 SHALL BE THE AUTHORITY OF THE TRAFFIC ENGINEER  
20 BASED ON A REVIEW OF THE SITUATION. THE STREET  
21 CURB NEXT TO ON-STREET PARKING SPACES SHALL  
22 BE A VERTICAL CURB (NOT A ROLLED CURB), AND A  
23 SIDEWALK SHALL EXTEND THE FULL LENGTH OF THE  
24 SUBJECT PROPERTY.

25  
26 20. *STACKED AND TANDEM PARKING.*

27  
28 A. *NONRESIDENTIAL USES.* STACKED AND TANDEM  
29 PARKING SPACES FOR NONRESIDENTIAL USES  
30 ARE ALLOWED TO COUNT TOWARD THE  
31 MINIMUM NUMBER OF REQUIRED SPACES IF THE  
32 OWNER ENSURES THROUGH THE PARKING  
33 AGREEMENT THAT ATTENDANT PARKING IS  
34 PROVIDED FOR SUCH SPACES. AN ACCESSIBLE  
35 PASSENGER LOADING ZONE SHALL BE  
36 PROVIDED WITH ATTENDANT PARKING  
37 SERVICES AT OR NEAR A PRIMARY ENTRANCE.  
38 AVAILABILITY OF THIS SERVICE SHALL BE  
39 CONSPICUOUSLY POSTED INSIDE AND OUTSIDE  
40 THE PRIMARY ENTRANCE. THE TRAFFIC  
41 ENGINEER MAY WAIVE THE PARKING  
42 ATTENDANT REQUIREMENT FOR AUTOMATED  
43 PARKING STRUCTURES.

44  
45 B. *RESIDENTIAL USES.* TWO REQUIRED PARKING  
46 SPACES FOR ANY RESIDENTIAL DWELLING MAY  
47 BE ARRANGED IN TANDEM OR STACKED ONE  
48 ABOVE THE OTHER USING A CAR STACKER, SO  
49 LONG AS PARKING REQUIRED FOR THE

1 DWELLING UNIT IS ARRANGED INDEPENDENTLY  
2 FROM PARKING SERVING ANY OTHER DWELLING  
3 UNIT, WITH UNOBSTRUCTED VEHICLE ACCESS  
4 FOR AT LEAST ONE OF THE SPACES REQUIRED  
5 FOR EACH DWELLING UNIT, AND THE OWNER  
6 ASSIGNS THE TWO SPACES TOWARD THE SAME  
7 DWELLING AND ENFORCES THEIR ASSIGNED  
8 USE.  
9

10 21. *SMALLER PARKING SPACES FOR PARKING*  
11 *STRUCTURES AND LOW-TURNOVER USES.* IF  
12 APPROVED BY THE TRAFFIC ENGINEER, UP TO 20  
13 PERCENT OF THE TOTAL NUMBER OF REQUIRED  
14 PARKING SPACES LOCATED IN A PARKING STRUCTURE  
15 AND/OR DESIGNATED FOR EMPLOYEE OR RESIDENT  
16 PARKING ONLY MAY BE EIGHT FEET SIX INCHES WIDE,  
17 SUBJECT TO THE REQUIREMENTS OF TABLE 21.07-7,  
18 PARKING SPACE AND AISLE DIMENSIONS. SUCH  
19 SPACES SHALL BE SIGNED FOR EMPLOYEE OR  
20 RESIDENT PARKING ONLY.  
21

22 22. *BICYCLE PARKING.* A USE IS ELIGIBLE TO  
23 PERMANENTLY OR SEASONALLY SUBSTITUTE BICYCLE  
24 PARKING SPACES REQUIRED AUTOMOBILE PARKING  
25 SPACES. EACH AUTOMOBILE PARKING SPACE SHALL  
26 BE REPLACED BY A MINIMUM OF SIX BICYCLE PARKING  
27 SPACES NOT REQUIRED BY THIS TITLE. BICYCLE  
28 PARKING SPACES SHALL COMPLY WITH THE  
29 STANDARDS OF SUBSECTION 21.07.060F.15. AND BE  
30 SEPARATED FROM MOTOR VEHICLE AREAS BY  
31 BOLLARDS OR OTHER PHYSICAL BUFFER APPROVED  
32 BY THE TRAFFIC ENGINEER.  
33

34 23. *OTHER ELIGIBLE REDUCTIONS OR ALTERNATIVES.* THE  
35 TRAFFIC ENGINEER AND THE DIRECTOR MAY  
36 APPROVE ANY PARKING REDUCTION OR OTHER  
37 ALTERNATIVE IN ADDITION TO THE CHOICES ABOVE,  
38 OR THAT INCREASES THE PERCENTAGE REDUCTION IN  
39 ANY OF THE CHOICES ABOVE, IF THE APPLICANT  
40 DEMONSTRATES TO THE SATISFACTION OF THE  
41 TRAFFIC ENGINEER AND THE DIRECTOR THAT THE  
42 PROPOSED PARKING MANAGEMENT STRATEGY WILL  
43 PROTECT SURROUNDING NEIGHBORHOODS, AND  
44 MAINTAIN TRAFFIC CIRCULATION PATTERNS AT LEAST  
45 THE SAME EXTENT AS WOULD STRICT COMPLIANCE  
46 WITH OTHERWISE APPLICABLE OFF-STREET PARKING  
47 STANDARDS. ADDITIONAL PARKING MANAGEMENT  
48 STRATEGIES MAY INCLUDE, FOR EXAMPLE,  
49 TRANSPORTATION DEMAND PROGRAMS, CAR



1 SHARING, UNBUNDLED PARKING, OR A COMBINATION  
2 OF STRATEGIES. THE APPLICANT SHALL PROVIDE A  
3 PARKING DEMAND STUDY PREPARED IN A FORM AND  
4 MANNER PRESCRIBED BY THE TRAFFIC ENGINEER  
5 THAT DEMONSTRATES A REDUCTION IS APPROPRIATE  
6 BASED ON THE EXPECTED PARKING NEEDS OF THE  
7 DEVELOPMENT, AVAILABILITY OF TRANSIT, AND  
8 SIMILAR FACTORS. IT SHALL BE DETERMINED THAT:

9  
10 A. THE USE WILL BE ADEQUATELY SERVED BY THE  
11 PROPOSED PARKING DUE TO PROJECT  
12 LOCATION, TRANSPORTATION  
13 CHARACTERISTICS OF THE PERSONS RESIDING,  
14 WORKING, OR VISITING THE SITE, OR BECAUSE  
15 THE APPLICANT HAS UNDERTAKEN A PROGRAM  
16 OR STRATEGY THAT WILL REDUCE PARKING  
17 DEMAND AT THE SITE; AND

18  
19 B. PARKING DEMAND GENERATED BY THE  
20 PROJECT WILL NOT EXCEED THE CAPACITY OF  
21 OR HAVE A DETRIMENTAL IMPACT ON THE  
22 SUPPLY OF ON-STREET PARKING IN THE  
23 SURROUNDING AREA.]

24 \*\*\* \*\*

25 (Note to code revisor: Re-number existing table 21.07-6 in subsection  
26 21.07.090G. to become table 21.07-5.)  
27  
28

H. *Parking and loading facility design standards.*  
\*\*\*           \*\*\*           \*\*\*

(Note to Code Revisor: The S-Version's revisions to the following subsection 21.07.090H.8., Vehicular Access and Circulation, restore the current adopted code text of subsection 21.07.090H.8.)

8. *Vehicular Access and Circulation* [~~General~~]. Parking lots and structures ~~[access, layout, and dimensions]~~ shall be designed for a safe and orderly flow of traffic throughout the site, as provided in ~~the~~[THE] subsections ~~[8. through 11.]~~ that follow.

a. **Key Elements.** **The parking facility layout, circulation, and design plan shall address the following elements as they relate to parking lots, including but not limited to: fire lanes, emergency access, drive-throughs, queuing spaces, passenger loading zones, pedestrian circulation, and loading berths.** [KEY ELEMENTS. THE PARKING FACILITY LAYOUT, CIRCULATION, AND DESIGN PLAN SHALL ADDRESS THE FOLLOWING ELEMENTS AS THEY RELATE TO PARKING LOTS, INCLUDING BUT NOT LIMITED TO: FIRE LANES, EMERGENCY ACCESS, DRIVE-THROUGHS, QUEUING SPACES, PASSENGER LOADING ZONES, PEDESTRIAN CIRCULATION, AND LOADING BERTHS.]

b.[B.] *Circulation Patterns.* Internal circulation patterns and the location and traffic direction of all circulation aisles, driveways, and queuing lanes shall be designed and maintained in accordance with the municipal driveway standards currently established by the traffic engineer, and with accepted principles of traffic engineering and safety, per the traffic engineer's review based on the current manuals of the Institute of Transportation Engineers and the Urban Land Institute, and the *Manual of Uniform Traffic Control Devices* or the successor documents. Circulation patterns within parking facilities shall be well defined with pavement marking and signage, curbs, landscaping, landscaped islands, and/or other similar features. **In order to define circulation and provide better sight distance, curbed end islands shall be required at the end of each row of parking spaces. Where loading facilities or on-site refuse collection are provided, commercial truck circulation shall be considered, and truck turning radii shall be shown on the parking facility layout, circulation, and**

**design plan when required by the traffic engineer.**

[IN ORDER TO DEFINE CIRCULATION AND PROVIDE BETTER SIGHT DISTANCE, CURBED END ISLANDS SHALL BE REQUIRED AT THE END OF EACH ROW OF PARKING SPACES. WHERE LOADING FACILITIES OR ON-SITE REFUSE COLLECTION ARE PROVIDED, COMMERCIAL TRUCK CIRCULATION SHALL BE CONSIDERED, AND TRUCK TURNING RADII SHALL BE SHOWN ON THE PARKING FACILITY LAYOUT, CIRCULATION, AND DESIGN PLAN WHEN REQUIRED BY THE TRAFFIC ENGINEER.]

**[b. Administrative Adjustments and Exceptions. The traffic engineer and director may approve modifications from the standards of this section, provided documentation prepared by a licensed professional in the state of Alaska registered with the Alaska State Board of Registration for Architects, Engineers and Land Surveyors demonstrates the parking facility or driveway meets the intent of this section and the municipal driveway standards and that the change is appropriate. Approval shall be contingent on factors such as street classification, street typology, urban context, traffic volume and speed, curb return radii, street travel lane offset from face of curb, pedestrian and bicycle facilities, snow storage, driveway configuration and length, site and project characteristics, number of vehicles expected to use the driveway, and comprehensive plan policies.]**

**c. Parking Spaces Along Major Site Entrance Drives. The provision, location, design, and dimensions of parking spaces on a major access driveway that serves as an entry or exit for a large establishment with multiple lots, tracts, or businesses, shall conform to municipal standards for on-street parking and be subject to review and approval by the traffic engineer. [C. PARKING SPACES ALONG MAJOR SITE ENTRANCE DRIVES. THE PROVISION, LOCATION, DESIGN, AND DIMENSIONS OF PARKING SPACES ON A MAJOR ACCESS DRIVEWAY THAT SERVES AS AN ENTRY OR EXIT FOR A LARGE ESTABLISHMENT WITH MULTIPLE LOTS, TRACTS, OR BUSINESSES, SHALL CONFORM TO MUNICIPAL STANDARDS FOR ON-STREET PARKING AND BE SUBJECT TO**

REVIEW AND APPROVAL BY THE TRAFFIC  
ENGINEER.]

**[9. Vehicular Access and Parking Location**

**a. *Street Access Location.* The number and spacing of driveways, including minimum distance from street intersections, shall be as provided in the *Municipal Driveways Standards* and this section. Access to streets owned by the state of Alaska requires compliance with state driveway standards, department of transportation and public facilities approval and driveway permit.**

**b. *Alley Access Requirement (in Urban Neighborhood Context Areas).* Where a residential use is served by an alley in a Traditional Urban or Edge Urban Neighborhood Context (21.07.015), direct vehicle access to the street shall be limited to the following:**

**i. *Residential driveway access not exceeding one driveway no wider than 12 feet at any point, except that lot frontages 100 feet or wider on a primary street frontage may have one driveway per 50 feet of lot frontage on that street.***

**ii. *One additional driveway no wider than 12 feet providing access to the secondary street frontage on corner lots for multifamily, townhouse, or two-family developments.***

**iii. *Additional driveway access to the primary street frontage for multifamily or townhouse developments may be allowed if the alley is unimproved (i.e., vegetated, no build accessway), unmaintained, or inaccessible, and the traffic engineer determines that improvement and vehicle access is not feasible, or that use of the alley for access would create a substantial traffic impact or safety hazard. The additional driveway access shall be the minimum the traffic engineer determines is necessary to provide access for the development. The Planning Department shall keep record of the approved additional driveway access with***

written findings supporting the approval on  
file and available for public inspection.

c. *Cross-Access and Shared Access with Adjacent Sites.* Parking facilities serving a site, whether located on that same lot or on an adjacent lot, may be connected by means of a common access driveway within or between the interior of such lots. Where a property receives its access to the street through a shared driveway with another lot, a shared access easement shall be provided on the plat, or a shared access agreement running with the land shall be recorded by the municipality, as approved and executed by the director, guaranteeing the continued availability of the shared access between the properties.

d. *Paired Residential Driveways Allowed.* Driveways for two single-family, two-family, or townhouse units may be attached side-by-side across a property line, provided the maximum combined width of the paved area is 24 feet in the Urban Neighborhood Development Context Areas (21.07.015), and 32 feet otherwise (16 feet maximum on each lot), without a landscaped break.

e. *Access and Parking Location in Urban Neighborhood Context Areas.* In the Traditional and Edge Urban Neighborhood Contexts established in 21.07.015, the maximum percentage of the front setback area on a property that may be paved for vehicle access and parking shall be as shown in Table 21.07-12, provided that:

i. *Secondary frontages on lots with more than one street frontage are exempt;*

ii. *Shared parking courtyards meeting 21.07.060G.20. are exempt;*

iii. *Minimum driveway widths per subsection 21.07.090H.11.d. are allowed; and*

iv. *Administrative adjustments may be approved as provided in 21.07.090H.8.b.]*

Table 21.07-12: Percentage of Front Setback Area in Urban Neighborhood Context Areas  
that May be Paved for Vehicle Access and Parking

<u>Uses</u>	<u>Traditional Urban Neighborhood Context</u>	<u>Edge Urban Neighborhood Context</u>	
		<u>with Alley Access (per 21.07.090H.9.b)</u>	<u>without Alley Access</u>
<u>Single-family, Two-family and Mobile-Home Uses</u>	<u>50% max.</u>	<u>50% max.</u>	<u>75% max.</u>
<u>Other Uses</u>	<u>50% max.</u>	<u>50% max.</u>	<u>65% max.]</u>

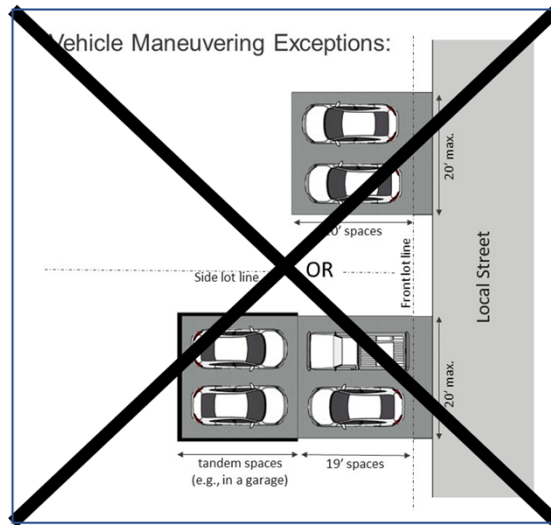
**[10. Access to Parking Spaces.**

- a. Access to Parking Spaces. Each parking space shall open to a parking aisle or driveway of such width and design as provided in subsection 21.07.090H.12. to provide safe and efficient means of vehicular access with no more than a standard two-movement entrance or exit from the parking space and without having to move another vehicle. Stacked and tandem parking spaces are allowed exceptions as provided in 21.07.090H.12.**
- b. Backing Distances. Adequate ingress and egress to each parking space shall be provided without backing more than 25 feet.**
- c. Dead-End Parking Aisles. Any parking bay that does not provide two means of vehicle egress shall provide, at the closed end, an extension of the parking aisle at least nine feet in depth, designated and signed as a “No Parking” turn-around area. Dead-end parking bays longer than 100 feet shall be subject to discretionary review and approval by the traffic engineer.**
- d. Vehicle Maneuvering. Off-street parking facilities shall be designed so that all vehicle maneuvers occur on the development site and not in the public right-of-way, and vehicles enter and exit the right-of-way in a forward motion, except the following are exempt:**
  - i. Parking for single-family, two-family, and mobile home dwellings on individual lots, accessing a local street.**
  - ii. For other developments, parking areas comprising only one or two parking spaces whose only access is to a local street, provided that the paved vehicle area(s)**

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occupy no more than 20 feet of the lot frontage in the front setback. The number of spaces may be increased to four spaces if arranged in tandem for residential dwellings as provided in 21.07.090K.12.k. Additional spaces for multifamily developments with up to four dwelling units may be approved by the traffic engineer in appropriate circumstances such as lots on dead-end streets, cul-de-sacs, or other local streets with low traffic volumes.



(Note to Code Revisor: The graphic above is deleted.)

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iii. The usable portion of an alley may be credited as circulation and/or parking aisle space for parking areas with up to 4 spaces, provided there is at least 24 feet of maneuvering area between the end of each parking space and the opposite side of the improved alley. Additional parking spaces may be designed so that vehicles back out into an alley, subject to approval by the traffic engineer.]

**[11. Driveway Design and Dimensions.]**

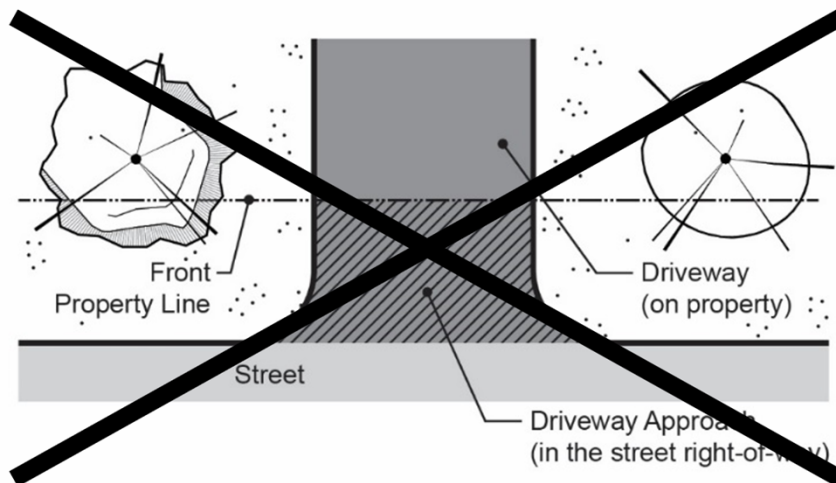
(Note to Code Revisor: The S-Version's revisions to the following subsection 21.07.090H.8.d, Parking Lot Entries/Driveways, restore the current adopted code text of this subsection.)

d[a][D]. Parking Lot .Entries/Driveway[Approache]s. Entries and driveway[approache]s providing access to parking lots[from the street edge to the front property line ][TO PARKING LOTS] shall conform to

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the municipal driveway standards **currently established by the traffic engineer**[CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER] **[and this subsection 21.07.090H.11]**. Access to streets owned by the state of Alaska requires compliance with state driveway standards, **department of transportation and public facilities approval, and a current valid state of Alaska driveway permit. Ingress and egress to parking facilities shall be designed to maintain adequate sight distance and safety and as prescribed in the municipal driveway standards.**[as provided in 21.07.090H.8]. [DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES APPROVAL, AND A CURRENT VALID STATE OF ALASKA DRIVEWAY PERMIT. INGRESS AND EGRESS TO PARKING FACILITIES SHALL BE DESIGNED TO MAINTAIN ADEQUATE SIGHT DISTANCE AND SAFETY AND AS PRESCRIBED IN THE MUNICIPAL DRIVEWAY STANDARDS.]

- i. **Residential Uses. Residential driveway entrances shall comply with subsection 21.07.110F.3., Driveway Width.**
- ii. **Nonresidential Uses. The total width of driveway entrances to a nonresidential lot from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the frontage if the platting authority or traffic engineer finds that conditions warrant it, unless the applicant provides for snow storage in a manner approved by the decision-making body.**





1 (Note to Code Revisor: The graphic above is deleted.)  
2  
3

4 **[b.] Curb Openings and Public Walkway Crossings.**  
5

6 **i. Curb cut and curb returns at driveway**  
7 **openings to the street shall be provided as**  
8 **prescribed in the municipal driveway**  
9 **standards.**

10 **ii. Public walkways shall be maintained or**  
11 **restored to the maximum running slope and**  
12 **cross-slope prescribed by M.A.S.S. and**  
13 **A.D.A., except that in the traditional urban**  
14 **neighborhood contexts (21.07.015), public**  
15 **walkways on local streets shall be restored**  
16 **to as close to a level running grade as**  
17 **practical.]**  
18

19  
20 **[c.] Driveway Approach (in ROW) as a Percentage of**  
21 **Lot Frontage Width.]**

22 **[i. RESIDENTIAL USES. RESIDENTIAL**  
23 **DRIVEWAY ENTRANCES SHALL COMPLY**  
24 **WITH SUBSECTION 21.07.110F.3.,**  
25 **DRIVEWAY WIDTH.**  
26

27 **ii. NONRESIDENTIAL USES.] [THE TOTAL**  
28 **WIDTH OF THE DRIVEWAY] [approach]**  
29 **[ENTRANCES TO A NONRESIDENTIAL LOT]**  
30 **[FROM A STREET SHALL NOT EXCEED 40**  
31 **PERCENT OF THE FRONTAGE OF THE LOT,**  
32 **OR 33 PERCENT OF THE FRONTAGE IF THE**  
33 **PLATTING AUTHORITY OR TRAFFIC**  
34 **ENGINEER FINDS THAT CONDITIONS**  
35 **WARRANT IT][, provided the following:] [,**  
36 **UNLESS THE APPLICANT PROVIDES FOR**  
37 **SNOW STORAGE IN A MANNER APPROVED**  
38 **BY THE DECISION-MAKING BODY.]**  
39

40 **[i. The driveway approach is always allowed to**  
41 **have the minimum driveway width provided**  
42 **by subsection d. The traffic engineer may**  
43 **approve more than the minimum driveway**  
44 **width, provided the traffic engineer**  
45 **determines that snow storage, traffic flow**  
46 **and safety, and the neighborhood context**  
47 **are addressed.**  
48



1  
2 **g.** **Circulation Definition.** **Curbed end islands shall be**  
3 **provided at the end of each row of parking spaces**  
4 **to define circulation and provide sight distance at**  
5 **internal intersections of parking aisles, driveways,**  
6 **and/or on-site roadways. Shared parking**  
7 **courtyards meeting 21.07.060G.20. may depart**  
8 **from this requirement.]**  
9

10 (Note to Code Revisor: The S-Version's revisions to the following subsection  
11 21.07.090H.8.e., Parking and Maneuvering restore the current code text of this  
12 subsection that the ordinance had deleted.)  
13

14 **e.** **Parking and Maneuvering.** **All circulation aisles,**  
15 **driveways, and vehicle maneuvering areas**  
16 **required by this section shall be located entirely**  
17 **off-street and on the property unless specifically**  
18 **provided otherwise by this section.**  
19

20 **i.** **Access to Parking Spaces.** **To ensure safe**  
21 **and efficient vehicular access to parking**  
22 **spaces, each off-street parking space shall**  
23 **open directly on a parking aisle or driveway**  
24 **of such width and design as provided in**  
25 **table 21.07-7 and the illustrations that follow**  
26 **the table. Adequate ingress and egress to**  
27 **each parking space shall be provided**  
28 **without backing more than 25 feet.**  
29

30 **ii.** **Maneuvering Area.** **Off-street parking**  
31 **facilities shall be designed with sufficient**  
32 **maneuvering room so that all maneuvers**  
33 **associated with the parking shall occur in**  
34 **the off-street parking facility, and that all**  
35 **vehicles enter the abutting street in a**  
36 **forward motion.**  
37

38 **iii.** **Some Dwellings Exempted.** **Single-family,**  
39 **two-family, townhouse, and mobile home**  
40 **dwellings on individual lots shall be**  
41 **exempted from this subsection. Multifamily**  
42 **dwellings with up to four units shall be**  
43 **exempted from this subsection in**  
44 **appropriate circumstances if approved by**  
45 **the traffic engineer. Appropriate**  
46 **circumstances may include lots with alley**  
47 **access, lots located on low-volume streets,**  
48 **and lots located on dead-end streets or cul-**  
49 **de-sacs.**



LOTS LOCATED ON DEAD-END STREETS  
OR CUL-DE-SACS.

(Note to Code Revisor: The S-Version's revisions to the following subsections 21.07.090H.8.f., Dead-End Parking Aisles, 21.07.090H.8.g., Alleys, and 21.07.090H.8.h., Cross Access and Joint Access with Adjacent Sites, restore the current adopted code text of these three subsections that the ordinance had deleted.)

**f. Dead-End Parking Aisles. Dead-end parking aisles may be allowed only with the approval of the traffic engineer.**

**g. Alleys. Subject to safety approval by the traffic engineer, the usable portion of an alley may be credited as circulation and/or parking aisle space.**

**h. Cross Access and Joint Access with Adjacent Sites. The plan shall show existing parking and circulation patterns on adjacent properties and potential connections. Required parking lots serving a site, whether located on that same lot or on an adjacent lot, may be connected by means of a common access driveway within or between the interior of such lots. Applicants are encouraged to provide shared vehicle and pedestrian access to adjacent properties for convenience, safety, and efficient circulation. An access easement shall be provided on the plat, or a shared access agreement running with the land shall be recorded by the municipality, as approved and executed by the director, guaranteeing the continued availability of the shared access between the properties.**

F. DEAD-END PARKING AISLES. DEAD-END PARKING AISLES MAY BE ALLOWED ONLY WITH THE APPROVAL OF THE TRAFFIC ENGINEER.

G. ALLEYS. SUBJECT TO SAFETY APPROVAL BY THE TRAFFIC ENGINEER, THE USABLE PORTION OF AN ALLEY MAY BE CREDITED AS CIRCULATION AND/OR PARKING AISLE SPACE.

H. CROSS ACCESS AND JOINT ACCESS WITH ADJACENT SITES. THE PLAN SHALL SHOW EXISTING PARKING AND CIRCULATION PATTERNS ON ADJACENT PROPERTIES AND POTENTIAL CONNECTIONS. REQUIRED PARKING LOTS SERVING A SITE, WHETHER LOCATED ON

1 THAT SAME LOT OR ON AN ADJACENT LOT, MAY  
2 BE CONNECTED BY MEANS OF A COMMON  
3 ACCESS DRIVEWAY WITHIN OR BETWEEN THE  
4 INTERIOR OF SUCH LOTS. APPLICANTS ARE  
5 ENCOURAGED TO PROVIDE SHARED VEHICLE  
6 AND PEDESTRIAN ACCESS TO ADJACENT  
7 PROPERTIES FOR CONVENIENCE, SAFETY, AND  
8 EFFICIENT CIRCULATION. AN ACCESS  
9 EASEMENT SHALL BE PROVIDED ON THE PLAT,  
10 OR A SHARED ACCESS AGREEMENT RUNNING  
11 WITH THE LAND SHALL BE RECORDED BY THE  
12 MUNICIPALITY, AS APPROVED AND EXECUTED  
13 BY THE DIRECTOR, GUARANTEEING THE  
14 CONTINUED AVAILABILITY OF THE SHARED  
15 ACCESS BETWEEN THE PROPERTIES.]

16  
17 **9[12]**[9]. *Dimensions of Parking Spaces and Aisles.* The parking  
18 space and aisle dimensional standards of this subsection  
19 21.07.090H.9[12]. shall apply to all parking except as stated  
20 otherwise in subsections 21.07.090I., passenger loading  
21 zones and 21.07.090J., accessible parking spaces. Parking  
22 dimensions and layout shall be as provided in subsection a.,  
23 including Table 21.07-6[13] and the figures that follow the  
24 table, with the additions and exceptions provided in  
25 subsections b. through m.]THE MINIMUM DIMENSIONS  
26 FOR PARKING SPACES AND PARKING AISLES SHALL BE  
27 AS PROVIDED IN TABLE 21.07-7, AND CALCULATED AS  
28 DEPICTED IN THE FIGURES THAT FOLLOW THE TABLE.  
29 THE MINIMUM PARKING SPACE. WIDTH SHALL BE 9'0  
30 EXCEPT AS PROVIDED ELSEWHERE IN THIS SECTION.  
31 THE PARKING CONFIGURATION STATED IN THE  
32 FOLLOWING TABLES AND FIGURES SHALL APPLY TO  
33 ALL OFF-STREET PARKING, EXCEPT AS STATED  
34 ELSEWHERE IN THIS SECTION.]

35  
36 a. *Minimum Dimensions for Required Parking.* Parking  
37 shall meet or exceed the STANDARD parking space  
38 and aisle dimensions set forth in table 21.07-6[13].  
39 except as follows:

40  
41 i. Parking facilities in the Downtown (DT) zoning  
42 districts, and in the **Traditional Neighborhood**  
43 **Design areas designated on Map 2-1:**  
44 **Anchorage 2040 Land Use Plan Map in the**  
45 **Anchorage 2040 Land Use Plan, [Traditional**  
46 **Urban Neighborhood Contexts delineated in**  
47 **21.07.015, —]**may meet the SMALL parking  
48 space and aisle dimensions in table 21.07-  
49 6[13]. Up to 30 percent of parking in the

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Downtown (DT) zoning districts may meet the COMPACT parking dimensions, if signed for compact vehicles only.

ii. In all other areas of the municipality, up to 50 percent of employee-only parking, structured parking facilities, or parking for uses with a low parking turnover rate may meet the SMALL parking space and aisle dimensions in table 21.07-6[13]. Uses with a low parking turnover rate are multifamily and mixed-use household living uses, office uses, and educational facility uses other than instructional services. [Required] P[p]arking shared between low-turnover and non-low-turnover uses shall meet STANDARD dimensions.

iii. Any use that involves the preservation of a landmark listed in the Anchorage local landmarks register may also meet the SMALL parking space and aisle dimensions in Table 21.07-6[13].

TABLE 21.07-6[13][7] PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Type	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
90	STANDARD	9' 0"	9' 0"	20' 0" <sup>1</sup>	23' 0"	24'	64' 0"	0' 0"	2' 0"
	SMALL	8' 6"	8' 6"	18' 0"	23' 6"	24'	60' 0"	0' 0"	
	COMPACT	8' 0"	8' 0"	16' 0"	23' 6"	24'	56' 0"	0' 0"	
75	STANDARD	9' 0"	9' 4"	21' 8" <sup>1</sup>	19' 0"	24'	67' 4"	1' 2"	1' 11"
	SMALL	8' 6"	8' 10"	19' 7"	19' 6"	24'	63' 2"	1' 11"	
	COMPACT	8' 0"	8' 3"	19' 5"	19' 6"	24'	62' 11"	0' 6"	
60	STANDARD	9' 0"	10' 5"	21' 10" <sup>1</sup>	18' 0"	24'	67' 8"	2' 3"	1' 8"
	SMALL	8' 6"	9' 10"	19' 10"	18' 6"	24'	63' 8"	2' 2"	
	COMPACT	8' 0"	9' 3"	17' 10"	18' 6"	24'	59' 9"	1' 0"	
45	STANDARD	9' 0"	12' 9"	20' 6" <sup>1</sup>	12' 0"	24'	65' 0"	3' 2"	1' 5"
	SMALL	8' 6"	12' 0"	18' 9"	12' 6"	24'	61' 6"	3' 0"	
	COMPACT	8' 0"	11' 4"	16' 12"	12' 6"	24'	57' 11"	1' 5"	
0 (parallel)	STANDARD	9' 0"	23' 0"	9' 0"	12' 0"	24'	42' 0"	0' 0"	0' 0"
	SMALL	8' 6"	23' 0"	8' 6"	12' 6"	24'	41' 0"	0' 0"	
	COMPACT	8' 0"	23' 0"	8' 0"	12' 6"	24'	40' 0"	0' 0"	

TABLE 21.07-6[13][7] PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Type	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
		[9' 6"]	[23' 0"]	[9' 6"]	[12' 0"]	[24']	[43' 0"]	[0' 0"]	
		[10' 0"]	[23' 0"]	[10' 0"]	[12' 0"]	[24']	[44' 0"]	[0' 0"]	
[45]		[8' 6"]	[12' 0"]	[18' 9"]	[12' 6"]	[24']	[61' 6"]	[3' 0"]	[1' 5"]
		[9' 0"]	[12' 9"]	[20' 6"]	[12' 0"]	[24']	[65' 0"]	[3' 2"]	
		[9' 6"]	[13' 5"]	[20' 10"]	[12' 0"]	[24']	[65' 9"]	[3' 4"]	
		[10' 0"]	[14' 2"]	[21' 3"]	[12' 0"]	[24']	[66' 5"]	[3' 6"]	
[60]		[8' 6"]	[9' 10"]	[19' 10"]	[18' 6"]	[24']	[63' 8"]	[2' 2"]	[1' 8"]
		[9' 0"]	[10' 5"]	[21' 10"]	[18' 0"]	[24']	[67' 8"]	[2' 3"]	
		[9' 6"]	[10' 12"]	[22' 1"]	[18' 0"]	[24']	[68' 2"]	[2' 5"]	
		[10' 0"]	[11' 7"]	[22' 4"]	[18' 0"]	[24']	[68' 8"]	[2' 6"]	
[75]		[8' 6"]	[8' 10"]	[19' 7"]	[19' 6"]	[24']	[63' 2"]	[1' 1"]	[1' 11"]
		[9' 0"]	[9' 4"]	[21' 8"]	[19' 0"]	[24']	[67' 4"]	[1' 2"]	
		[9' 6"]	[9' 10"]	[21' 9"]	[18' 6"]	[24']	[67' 7"]	[1' 3"]	
		[10' 0"]	[10' 4"]	[21' 11"]	[18' 0"]	[24']	[67' 10"]	[1' 5"]	
[90]		[8' 6"]	[8' 6"]	[18' 0"]	[23' 6"]	[24']	[60' 0"]	[0' 0"]	[2' 0"]
		[9' 0"]	[9' 0"]	[20' 0"]	[23' 0"]	[24']	[64' 0"]	[0' 0"]	
		[9' 6"]	[9' 6"]	[20' 0"]	[22' 0"]	[24']	[64' 0"]	[0' 0"]	
		[10' 0"]	[10' 0"]	[20' 0"]	[22' 0"]	[24']	[64' 0"]	[0' 0"]	

<sup>1</sup> Developments in the **Traditional Neighborhood Design areas designated on Map 2-1: Anchorage 2040 Land Use Plan Map in the Anchorage 2040 Land Use Plan [Traditional Urban and Edge Urban Neighborhood Development Context Areas]** may use the following alternative STANDARD parking space depth (Vehicle Projection): 19' 0" for 90-degree parking angle; 20' 8" for 75-degree parking angle; 20' 11" for 60-degree parking angle, and 19' 6" for 45-degree parking angle. Parking bay width may decrease as a result.

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(Note to code revisor: Re-number existing subsections a., b., c., d., and e. to b., c., d., e., and f.)

g.[F.] *Structural Columns.* A structural column may encroach into the width of a parking space by up to one foot, provided the remaining parking space width is no less than eight feet. Such[IF THE] column shall be[IS] located within four feet of either end of the parking space depth. Such column shall not be located within one foot of the drive aisle.

h.[G.] *Minimum Vertical Clearance*

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[H. *SMALLER PARKING SPACES FOR LOW TURNOVER USES.* REDUCED PARKING SPACE DIMENSIONS MAY BE APPROVED BY THE TRAFFIC ENGINEER PURSUANT TO SUBSECTION F.21. ABOVE.]

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- i. *Recreational Vehicle Spaces.* Parking spaces for recreational vehicles, if provided and delineated, shall be a minimum of 10 feet wide by 40 feet long.
  
- j. *Stacked [, AUTOMATED,] or Tandem Spaces with Attendant Parking.* The traffic engineer may approve reduced parking space dimensions for stacked parking spaces[,] and/or tandem spaces in an attendant (valet) parking facility. An accessible passenger loading zone shall be provided with attendant (valet) parking services at or near a primary entrance. Notice of availability of valet service shall be conspicuously posted inside and outside the primary entrance. The business owner shall enter into a parking agreement with the municipality guaranteeing continuation of the valet service.
  
- k. *Stacked or Tandem Spaces for a Residential Dwelling.* Two [required] parking spaces for any residential dwelling may be arranged in tandem with a combined space depth (vehicle projection length) of 38 feet, or stacked one above the other using a car stacker, subject to the following:
  - i. The parking [required] for the dwelling unit is arranged independently from parking serving any other dwelling unit; and
  - ii. The owner assigns the two spaces to the same dwelling.
  
- l. *Automated Parking Structures.* Automated parking structures are exempt from the parking space and aisle dimensions and vertical clearance requirements of this section 21.07.090H. 9[12]. They are instead subject to section 21.07.090M.10.
  
- m. *Administrative Adjustments.* The traffic engineer may approve a lesser parking dimension than set forth in this subsection 21.07.090H.9[12]., due to site topography, existing structures, lot configuration, or preservation of trees and vegetation, subject to a recorded parking agreement[~~—complying with 21.07.090F.2~~].

\*\*\*      \*\*\*      \*\*\*

- i. *Passenger Loading Zones.* All institutional, entertainment, and commercial uses such as schools/daycare, stadiums, and theaters

1 that have high-volume peak traffic volumes shall provide an area for  
2 drop-offs and pick-ups that meets the following requirements:

- 3  
4 1. *Passenger Loading Zone.* **[IN ADDITION TO THE  
5 REQUIRED MINIMUM NUMBER OF PARKING SPACES  
6 ESTABLISHED IN SUBSECTION 21.07.090E., T]**The traffic  
7 engineer may require one or more passenger loading zone  
8 spaces, depending on the type, intensity, and traffic patterns  
9 of the proposed use. The passenger loading zone for large  
10 commercial establishments or other intensive uses may be  
11 required by the traffic engineer to include one or more spaces  
12 dedicated to taxi cabs, ride hailing services, and/or other  
13 specialized high occupancy vehicles.
- 14  
15 2. *Passenger Loading Zone Dimensions.* Any passenger loading  
16 zone that is provided for a development shall consist of one  
17 or more passenger drop-off/pick-up spaces parallel to the  
18 driveway or circulation aisle adjacent to the building. Each  
19 space shall be a minimum of 20 feet in length and eight or  
20 more feet in width, with an access aisle at least five feet wide  
21 abutting the full length of the space. As an alternative, subject  
22 to approval of the traffic engineer, a passenger loading zone  
23 may consist of one or more parking spaces that meets the  
24 accessible parking space dimensional standards of  
25 subsection 21.07.090J.**8[7]**. However, designated passenger  
26 loading zone spaces shall not count towards **any[THE]**  
27 accessible parking space requirement.
- 28  
29 3. *Plan.* The parking facility layout, circulation, and design plan  
30 shall show the location and design of proposed passenger  
31 loading zones. For certain intensive uses, the traffic engineer  
32 may require the plan to include a traffic control plan  
33 addressing projected usage, hours of operation, peak  
34 loading/unloading time, plans for directing traffic, safety  
35 measures, and other information deemed necessary by the  
36 traffic engineer to designing a safe and well-functioning drop-  
37 off area.
- 38  
39 4. *Accessible Route.* An accessible pedestrian route to the  
40 building or facility entrance shall be provided pursuant to  
41 subsection 21.07.090J.**10[9]**., *Accessible Routes*.
- 42  
43 5. *Schools.* Passenger loading zones shall be required for  
44 schools (public or private). Length, location, and design of the  
45 passenger loading zones shall be approved by the traffic  
46 engineer.  
47

1 (Note to Code Revisor: The following subsection 21.07.090J. was not included in  
2 AO 2022-80 as introduced by the Assembly, but is included in the S-Version and  
3 amended as shown below.)

4  
5 J. Accessible Parking Spaces

- 6  
7 1. *Required Number of Accessible Parking Spaces.* A portion of  
8 the total number of parking spaces provided in each parking  
9 facility for commercial, industrial, community, multifamily, and  
10 mixed-use residential uses shall be accessible parking  
11 spaces. **These requirements are intended to reflect**  
12 **current national data on prevalence of mobility**  
13 **disabilities.** However, buildings in multifamily or mixed-use  
14 developments that are single-family, two-family, or townhouse  
15 style structures are not subject to the accessible parking  
16 space requirement. The number of accessible parking spaces  
17 shall be determined based on the total number of parking  
18 spaces provided, in accordance with table 21.07-7[8],  
19 Accessible Parking Spaces, except where otherwise stated in  
20 this section.  
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TABLE 21.07-7[8]: ACCESSIBLE PARKING SPACES		
Total Parking Spaces Provided	Total Accessible Spaces Required	Number of Accessible Spaces that shall be Van-Accessible
<u>1 to 10</u>	<u>1</u>	<u>1</u>
11 to 25	<u>2</u> [1]	<u>2</u> [1]
26 to 50	<u>4</u> [2]	<u>2</u> [1]
51 to 75	<u>6</u> [3]	<u>2</u> [1]
76 to 100	<u>8</u> [4]	<u>2</u> [1]
101 to 150	<u>11</u> [5]	<u>2</u> [1]
151 to 200	<u>14</u> [6]	<u>2</u> [1]
201 to 300	<u>14</u> [7]	2
301 to 400	<u>14</u> [8]	2
401 to 500	<u>14</u> [9]	2
501 to 1000	<u>3</u> [2] percent of total	1 for every 6 accessible spaces
1001 and over	20 plus <u>2</u> [1] for each 100 over 1000	1 for every 6 accessible spaces

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**7. Use of Required Accessible Parking. Required accessible parking facilities shall be used only for the parking of passenger automobiles of the residents, occupants, customers, visitors, or employees of the use. Accessible parking spaces and aisles shall be kept clear of obstructions, parking of equipment, and storage of goods or vehicles.**

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*(Note to code revisor: Re-number remaining subsections of 21.07.090J. Re-number all references to Table 21.07-8 to 21.07-7.)*

1 K. *Bicycle Parking* [SPACES].  
2

3 1. **Applicability. This subsection is effective beginning**  
4 **January 1, 2024, and applies to the uses in Table 21.07-8.**  
5 **Uses listed in subsection 21.07.090K.3. are exempt.**  
6

7 2. **Bicycle Parking Required.** [ALL NONRESIDENTIAL,  
8 MULTIFAMILY, AND MIXED-USE DWELLING  
9 DEVELOPMENTS WITH MORE THAN 40 PARKING  
10 SPACES REQUIRED IN TABLE 21.07-4 SHALL PROVIDE  
11 AT LEAST FOUR BICYCLE PARKING SPACES, OR A  
12 NUMBER OF BICYCLE PARKING SPACES EQUAL TO  
13 THREE PERCENT OF THE NUMBER OF REQUIRED  
14 AUTOMOBILE PARKING SPACES, WHICHEVER IS  
15 GREATER.]

16 **Table 21.07-8[14] establishes the minimum number of bicycle**  
17 **parking spaces required per principal use, and the percentage**  
18 **of these spaces required to be developed as long-term**  
19 **(employee, resident) bicycle parking, versus short-term**  
20 **(visitor) spaces. The requirements are intended to follow**  
21 **the recommendations in the Anchorage Non-Motorized**  
22 **Plan.** The required minimum number of spaces is calculated  
23 as follows:  
24

25 a. **A minimum of two bicycle parking spaces shall be**  
26 **provided for each use in Table 21.07-8[14]. For**  
27 **commercial and community uses, group living uses**  
28 **with 10 or more beds, and household living uses with**  
29 **10 or more dwellings, the two spaces shall be located**  
30 **outdoors and publicly accessible for customers and**  
31 **short-term visitors.**

32  
33 **[b. After the first 40 bicycle parking spaces, no**  
34 **additional bicycle parking is required for any**  
35 **individual non-residential use.**

36  
37 **c. Subsection 21.07.090C., Computation of Parking**  
38 **and Loading Requirements, applies with respect to**  
39 **fractions, multiple uses, and building floor area**  
40 **measurements.]**  
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<b>TABLE 21.07-8[14]: Bicycle Parking Spaces Required</b>			
<u>Use Category/Type</u>	<u>Minimum Number of Bicycle Spaces<sup>1</sup></u> <u>("sf" = square feet; "gfa" = gross floor area)</u>		<u>Minimum Percentage for Long-Term Spaces</u> <u>(for required bicycle parking in excess of 2 spaces)<sup>2</sup></u>
	<u>[Areas with Area-specific Parking Requirements in Table 21.07-7]</u>	<u>[All Other Areas]</u>	
<u>Multifamily and mixed-use dwellings</u>	<u>1 space per two dwellings</u>	<u>[1 space per 10 dwellings]</u>	<u>90 percent</u>
<u>Group living uses, Roominghouses, Homeless and transient shelters[.]</u>	<u>1 space per 5 beds</u>	<u>[1 space per 5 beds]</u>	<u>90 percent</u>
<u>Government administration &amp; civic facility, Offices, Hospitals/Healthcare facilities[.]</u>	<u>1 space per 5,000 sf gfa</u>	<u>[1 space per 10,000 sf gfa]</u>	<u>75 percent</u>
<u>Cultural Facilities, Entertainment &amp; Recreation Facilities</u>	<u>1 space per 10,000 sf gfa</u>	<u>[1 space per 20,000 sf gfa]</u>	<u>10 percent</u>
<u>Schools, Colleges[.]</u>	<u>4 spaces per classroom</u>	<u>[2 spaces per classroom]</u>	<u>25 percent</u>
<u>Restaurants, Bars</u>	<u>1 space per 3,000 sf gfa</u>	<u>[1 space per 10,000 sf gfa]</u>	<u>10 percent</u>
<u>Retail Sales, Personal Service, Repair &amp; Rental[.]</u>	<u>1 space per 5,000 [42,000] sf gfa</u>	<u>[1 space per 24,000 gfa]</u>	<u>10 percent</u>
<b>[Industrial uses]</b>	<b>[1 space per 25,000 sf. gfa]</b>	<b>[1 space per 50,000]</b>	<b>[75 percent]</b>
<u>Visitor Accommodations</u>	<u>1 space per 23,000 sf gfa</u>	<u>[1 space per 23,000 sf gfa]</u>	<u>75 percent</u>
<b>Industrial uses</b>	<b>1 space per 25,000 sf gfa</b>	<b>[1 space per 50,000]</b>	<u>75 percent</u>
<u>Uses not included above, except as provided in subsection K.3[2]. below</u>	<u>Minimum of 2 spaces</u>	<b>[Minimum of 2 spaces]</b>	<u>Not required</u>

<sup>1</sup> A minimum of 2 spaces are required for each use. After the first 40 bicycle parking spaces, no additional bicycle parking is required for any individual non-residential use.

<sup>2</sup> The first 2 required spaces are not used in calculating the percentage of required long term spaces. The number of required long-term bicycle parking spaces is calculated as a percentage of the overall number of required spaces beyond the first two required spaces.

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**3[2]. Exemptions.** The bicycle parking [THIS] requirements in subsection 1, shall not apply to:

a. Household living dwelling units with their own individual garages [BUILDINGS IN MULTIFAMILY OR MIXED-USE DEVELOPMENTS THAT ARE SINGLE-FAMILY, TWO-FAMILY, OR TOWNHOUSE STYLE STRUCTURES].

b. Airstrips.

- c. Utility Substations, Telecommunication facilities, and WECS.
- e. Natural resource extraction.
- f. Impound yards and storage yards.
- g. Any other non-residential use which has no regular employees or people on premises.

11 **4[3].** Administrative Adjustments. The director **and traffic engineer** may approve administrative relief or adjustments to the bicycle parking requirement **or bicycle facility development standards [for changes of use or modifications to existing buildings and site]**, provided the applicant demonstrates the adjustment is necessary to compensate for a practical difficulty of the site. The department shall keep record of the approved adjustment with written findings supporting the adjustment on file and available for public inspection.

22 **5.** Co-Located Bicycle Parking. **Bicycle parking spaces may be co-located for different uses on the same property if bicycle facility development standards are met. The total required bicycle parking spaces shall be the sum of the required parking for the individual uses.**

28 **6[4].** Bicycle Parking Facility Development Standards. These requirements shall apply to all required bicycle parking spaces, except as specifically provided otherwise. Only long-term parking spaces required by table 21.07-8[14] (in right-hand column) shall follow the requirements of subsection g. [BICYCLE PARKING SPACES SHALL MEET THE STANDARDS OF SUBSECTION 21.07.060F.15.]

- a. Required bicycle parking or a sign leading thereto shall be located in an area visible from a primary entrance area. Required **bicycle** parking other than long-term bicycle parking shall be located within 50 feet of a primary entrance as measured along an accessible pedestrian route.
- b. A required bicycle parking space shall be either a bike rack or bike locker. Each shall be a securely anchored, tamper-resistant structure. Bike racks shall allow the bike frame and one wheel to be locked to the rack with a standard U-type lock when both wheels are left on the bicycle. Bike racks shall support the bicycle frame in a stable position without damage to the bicycle.

Horizontal bike racks (i.e., racks not mounted to walls) shall support the bicycle at two or more points.

i. Bicycle rack tubing shall be a minimum of ¼ inch diameter and maximum of 3-inch diameter.

ii. The topmost attachable portion of bicycle racks shall be 25 inches to 40 inches in height.

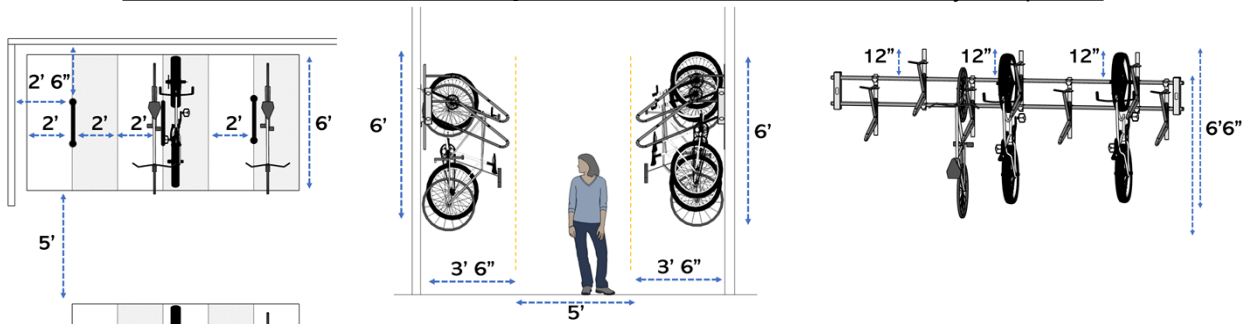
c. A required bicycle parking space shall be a minimum of six feet long and two feet wide, with the following exceptions:

i. Vertical bicycle parking spaces (securing the bicycle perpendicular to the ground) shall be a minimum of three feet six inches deep measured from the wall and six feet six inches tall. Vertical parking spaces placed side-by-side shall be a minimum of one foot six inches wide, with a vertical stagger of one foot between side-by-side spaces, as shown in the first set of figures below.

ii. Triangle bike locker layouts are exempt from the two-foot minimum width however shall have a minimum length of six feet six inches and access doors at least two feet six inches wide.

iii. Stacked bike parking spaces (stacking bicycles one tier on top of another) may meet the alternative stacked dimensions in the bottom figures provided there is a mechanically assisted lifting mechanism for mounting the bicycle on the upper tier.

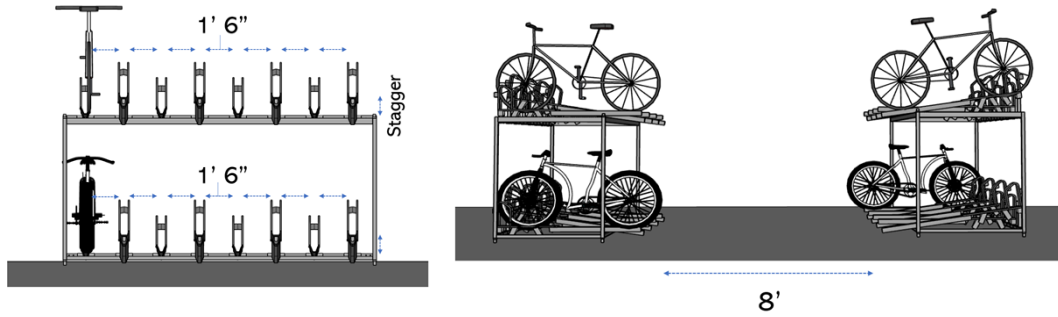
Dimensions for Horizontal/Side-by-Side and Vertical/Wall-Mounted Bicycle Spaces:



Dimensions for Stacked Bicycle Spaces:

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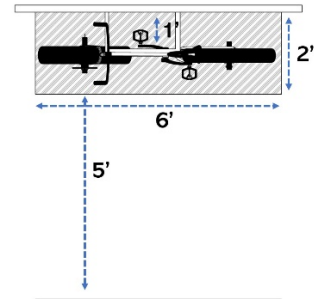
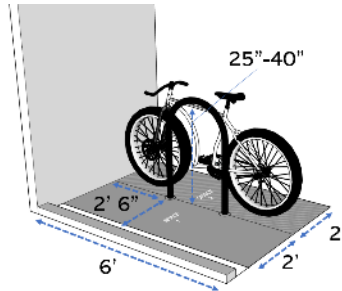
iv. Other deviations are permitted, per director approval, provided the applicant demonstrates the spaces meet the intent of the standard.

d. Bicycle parking spaces shall be designed so that the bicycle may be added or removed without having to move another bicycle.

i. There shall be a minimum of five feet of clear space inclusive of a walkway, behind the parking space to allow room for bicycle maneuvering.

ii. There shall be a minimum of two-foot six-inch clearance from all sides of the bicycle rack to walls, fences, curbs, and vehicle parking and circulation areas, or other obstructions, except for racks provided in iii.

iii. Horizontal bike parking spaces (securing the bicycle horizontal to the ground) with racks attached to walls shall have a minimum one-foot clearance from the rack to the wall, as shown in the second figure below. Vertical and stacked spaces have no required wall clearance.



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- e. Areas devoted to bicycle parking facilities shall be illuminated, hard-surfaced, and designed and maintained to be clear of obstructions, mud, and snow.
- f. Bicycle parking shall not obstruct pedestrian walkways, building access, or other use areas.
- g. A minimum of 30 percent of long-term spaces shall be level with the floor such that bicyclists do not have to lift a bicycle overhead to park it.

**7[h].** *Development Standards for Long-Term Spaces.* Long-term bicycle parking spaces are intended to accommodate bicyclists [~~residents, students, employees, and commuters~~] who typically need to store a bicycle overnight or for an entire work shift or school day. In addition to the standards in subsection 6., r[R]equired long-term spaces shall comply with the following additional standards:

- a[i]. Long-term bicycle parking shall be located in illuminated [well-lit] restricted access, lockable rooms or enclosures providing protection from theft, vandalism, and weather. Bicycle lockers, covered bicycle cages, or a secure room inside a building are common examples.
- b[ii]. Long-term bicycle parking spaces shall be located where bicyclists are not required to carry bicycles on stairs to access the parking.

1                    **c[~~iii~~]**. Long-term bicycle parking spaces for residential uses  
2                    with fewer than five dwellings may be located within  
3                    dwelling units, provided each dwelling unit has at least  
4                    one bike parking space. Bicycle parking in the dwelling  
5                    shall be located ~~[within 15 feet of the dwelling's~~  
6                    ~~primary entrance and ]~~in a closet or three-sided  
7                    alcove which includes a rack and meets the  
8                    development standards of ~~[this ]~~subsection  
9                    21.07.090K.6[4]. Long-term spaces shall not be  
10                   located on a balcony, deck, or patio area accessory to  
11                   an individual dwelling unit.

12                   \*\*\*        \*\*\*        \*\*\*

13                   (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-58, 5-20-14;  
14                   AO 2015-82, 7-28-15; AO 2015-100, 10-13-15; AO 2015-131, 1-12-16; AO  
15                   2016-3(S), 2-23-16; AO 2017-55, 4-11-17; AO 2017-176, 1-9-18; AO 2019-  
16                   132, 12-3-19; AO 2020-38, 4-28-20)

17                   \*\*\*        \*\*\*        \*\*\*

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19                   **21.07.110     Residential Design Standards.**

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21                   *(Note to Code Revisor: The S-Version's revisions to section 21.07.110, Residential*  
22                   *Design Standards, restore the current code text of section 21.07.110 that the*  
23                   *ordinance had deleted. The S-Version recommends retaining section 21.07.110*  
24                   *as currently adopted, in this ordinance.)*

25  
26                   A.        *Purpose.*  
27                   \*\*\*        \*\*\*        \*\*\*

28                   *(Note to Code Revisor: The S-Version's revisions to the following subsection*  
29                   *21.07.110A., Purpose, restore the current code text of this subsection that the*  
30                   *ordinance had deleted.)*

31  
32                   **4.        Enhance public safety by promoting lines of sight to**  
33                   **residential entries, clearly defining transitions from**  
34                   **public to more private residential outdoor spaces, and**  
35                   **designing entrances to be prominent.**

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37                   **5.        Locate active living spaces, entrances, and windows to**  
38                   **improve the physical and visual connection from**  
39                   **residences to the street, and foster opportunities for**  
40                   **casual surveillance of the street and outwardly expressed**  
41                   **proprietorship of the neighborhood.**

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43                   [4.        ENHANCE PUBLIC SAFETY BY PROMOTING LINES OF  
44                   SIGHT TO RESIDENTIAL ENTRIES, CLEARLY DEFINING  
45                   TRANSITIONS FROM PUBLIC TO MORE PRIVATE  
46                   RESIDENTIAL OUTDOOR SPACES, AND DESIGNING  
47                   ENTRANCES TO BE PROMINENT.  
48

- 1           5.    LOCATE ACTIVE LIVING SPACES, ENTRANCES, AND  
2            WINDOWS TO IMPROVE THE PHYSICAL AND VISUAL  
3            CONNECTION FROM RESIDENCES TO THE STREET,  
4            AND FOSTER OPPORTUNITIES FOR CASUAL  
5            SURVEILLANCE OF THE STREET AND OUTWARDLY  
6            EXPRESSED PROPRIETORSHIP OF THE  
7            NEIGHBORHOOD.]  
8            \*\*\*       \*\*\*       \*\*\*

9           C.    Standards for Multifamily and Townhouse Residential.

10  
11    (Note to Code Revisor: The S-Version’s revisions to the following subsection  
12    21.07.110C.1., Purpose, restore the current code text of this subsection that the  
13    ordinance had deleted.)

- 14  
15           1.    Purpose.  
16            \*\*\*       \*\*\*       \*\*\*

17           c.    Promote building placement and orientation that  
18            interfaces with the neighborhood street and  
19            engages the pedestrian, to contribute to public  
20            safety, attractive street frontages, pedestrian  
21            access, and a sense of neighborhood and  
22            community.

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24           [C.    PROMOTE BUILDING PLACEMENT AND  
25            ORIENTATION THAT INTERFACES WITH THE  
26            NEIGHBORHOOD STREET AND ENGAGES THE  
27            PEDESTRIAN, TO CONTRIBUTE TO PUBLIC  
28            SAFETY, ATTRACTIVE STREET FRONTAGES,  
29            PEDESTRIAN ACCESS, AND A SENSE OF  
30            NEIGHBORHOOD AND COMMUNITY.]

31           \*\*\*       \*\*\*       \*\*\*

32           g.    Promote a safe living environment through “eyes  
33            on the street” design elements, such as placement  
34            of windows, and transition spaces between the  
35            public realm and the more private areas of the  
36            development.

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38           [G.    PROMOTE A SAFE LIVING ENVIRONMENT  
39            THROUGH “EYES ON THE STREET” DESIGN  
40            ELEMENTS, SUCH AS PLACEMENT OF WINDOWS,  
41            AND TRANSITION SPACES BETWEEN THE PUBLIC  
42            REALM AND THE MORE PRIVATE AREAS OF THE  
43            DEVELOPMENT.]

44           \*\*\*       \*\*\*       \*\*\*

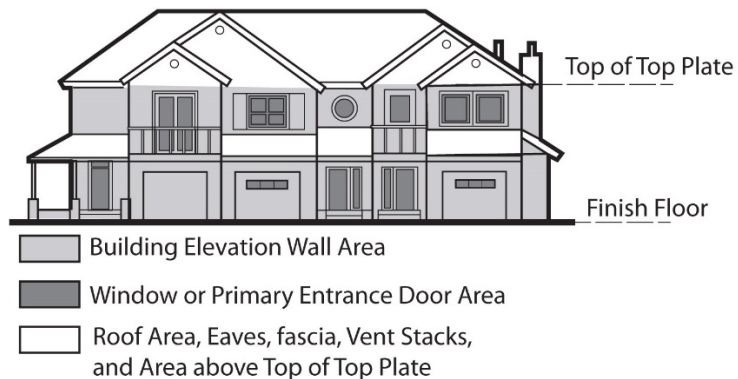
45    (Note to Code Revisor: The S-Version’s revisions to the following subsection  
46    21.07.110C.3., Windows Facing the Street, restore the current code text of this  
47    subsection that the ordinance had deleted.)

- 48  
49           3.    Windows Facing the Street.

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**a. Windows. Windows or primary entrance doors shall be provided on each building elevation facing a street or that has primary entrances to dwellings (up to a maximum of two elevations). At least 10 percent of the wall area of the building elevation shall be windows or primary entrance doors. The director may eliminate or reduce the requirement on one elevation (in instance where up to two apply) based on site specific circumstances (such as infill projects where an entry faces an adjacent property rather than the street).**

**b. Wall Area Calculation. The area of vertical wall surface measured from finished floor of each level, including garages to top of top plate. In the case of a basement wall, calculate the area of vertical wall surface measured from finished grade to the top of the finished floor above or top of top plate, whichever is applicable. Any wall area above the top plate shall not apply, including any gable ends. The façade wall area of stories below grade plane, such as below grade parking, are exempt from the measurement of wall area. The roof, including eaves, fascia, and vent stacks, is also exempt, as illustrated below.**



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**c. Energy Efficiency Exception. A reduction in required window area is permitted if demonstrated by calculation by an energy rater certified by the state of Alaska that installing the required window area will reduce the energy rating below a 5-star energy rating.**

**d. Additions and Renovations. Only the wall area affected by either an addition or a renovation shall**

**be required to comply with the opening requirements. Unaffected wall areas need not comply. Garage additions are exempt from opening requirements. The director may eliminate or reduce the requirement for window openings on additions/renovations based on project specific circumstances (such as the nature of the spaces being added, building orientation, or structural requirements).**

[3. *WINDOWS FACING THE STREET.*

A. *WINDOWS.* WINDOWS OR PRIMARY ENTRANCE DOORS SHALL BE PROVIDED ON EACH BUILDING ELEVATION FACING A STREET OR THAT HAS PRIMARY ENTRANCES TO DWELLINGS (UP TO A MAXIMUM OF TWO ELEVATIONS). AT LEAST 10 PERCENT OF THE WALL AREA OF THE BUILDING ELEVATION SHALL BE WINDOWS OR PRIMARY ENTRANCE DOORS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT ON ONE ELEVATION (IN INSTANCE WHERE UP TO TWO APPLY) BASED ON SITE SPECIFIC CIRCUMSTANCES (SUCH AS INFILL PROJECTS WHERE AN ENTRY FACES AN ADJACENT PROPERTY RATHER THAN THE STREET).

B. *WALL AREA CALCULATION.* THE AREA OF VERTICAL WALL SURFACE MEASURED FROM FINISHED FLOOR OF EACH LEVEL, INCLUDING GARAGES TO TOP OF TOP PLATE. IN THE CASE OF A BASEMENT WALL, CALCULATE THE AREA OF VERTICAL WALL SURFACE MEASURED FROM FINISHED GRADE TO THE TOP OF THE FINISHED FLOOR ABOVE OR TOP OF TOP PLATE, WHICHEVER IS APPLICABLE. ANY WALL AREA ABOVE THE TOP PLATE SHALL NOT APPLY, INCLUDING ANY GABLE ENDS. THE FAÇADE WALL



1 AREA OF STORIES BELOW GRADE PLANE, SUCH  
2 AS BELOW GRADE PARKING, ARE EXEMPT FROM  
3 THE MEASUREMENT OF WALL AREA. THE ROOF,  
4 INCLUDING EAVES, FASCIA, AND VENT STACKS,  
5 IS ALSO EXEMPT, AS ILLUSTRATED BELOW.

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7 C. *ENERGY EFFICIENCY EXCEPTION.* A REDUCTION  
8 IN REQUIRED WINDOW AREA IS PERMITTED IF  
9 DEMONSTRATED BY CALCULATION BY AN  
10 ENERGY RATER CERTIFIED BY THE STATE OF  
11 ALASKA THAT INSTALLING THE REQUIRED  
12 WINDOW AREA WILL REDUCE THE ENERGY  
13 RATING BELOW A 5-STAR ENERGY RATING.

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15 D. *ADDITIONS AND RENOVATIONS.* ONLY THE WALL  
16 AREA AFFECTED BY EITHER AN ADDITION OR A  
17 RENOVATION SHALL BE REQUIRED TO COMPLY  
18 WITH THE OPENING REQUIREMENTS.  
19 UNAFFECTED WALL AREAS NEED NOT COMPLY.  
20 GARAGE ADDITIONS ARE EXEMPT FROM  
21 OPENING REQUIREMENTS. THE DIRECTOR MAY  
22 ELIMINATE OR REDUCE THE REQUIREMENT FOR  
23 WINDOW OPENINGS ON  
24 ADDITIONS/RENOVATIONS BASED ON PROJECT  
25 SPECIFIC CIRCUMSTANCES (SUCH AS THE  
26 NATURE OF THE SPACES BEING ADDED,  
27 BUILDING ORIENTATION, OR STRUCTURAL  
28 REQUIREMENTS).]

29 \*\*\* \*\*\* \*\*

30 **4[3][4].** *Building Spacing.*

31 \*\*\* \*\*\* \*\*

32 (Note to Code Revisor: The S-Version's revisions to the following subsection  
33 21.07.110C.5., Pedestrian Access, restore the current code text of this subsection  
34 that the ordinance had deleted.)

35  
36 **5. Pedestrian Access. Walkway connections from primary**  
37 **front entrances to the street are required in accordance**  
38 **with subsection 21.07.060E.4., except that developments**  
39 **may provide one of the following alternatives instead:**

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41 **a. Primary front entrances for individual dwellings**  
42 **may connect to the street by the dwelling unit's**  
43 **individual driveway if such is provided;**

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45 **b. A parking courtyard may be provided in**  
46 **conformance with subsection 21.07.060F.17., or**  
47

1                    **c. Other methods, as approved by the director, that**  
2                    **provide safe, convenient, and adequate pedestrian**  
3                    **access.**  
4

5                    [5. *PEDESTRIAN ACCESS.* WALKWAY CONNECTIONS  
6                    FROM PRIMARY FRONT ENTRANCES TO THE STREET  
7                    ARE REQUIRED IN ACCORDANCE WITH SUBSECTION  
8                    21.07.060E.4., EXCEPT THAT DEVELOPMENTS MAY  
9                    PROVIDE ONE OF THE FOLLOWING ALTERNATIVES  
10                    INSTEAD:

11  
12                    A. PRIMARY FRONT ENTRANCES FOR INDIVIDUAL  
13                    DWELLINGS MAY CONNECT TO THE STREET BY  
14                    THE DWELLING UNIT'S INDIVIDUAL DRIVEWAY IF  
15                    SUCH IS PROVIDED;

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17                    B. A PARKING COURTYARD MAY BE PROVIDED IN  
18                    CONFORMANCE WITH SUBSECTION  
19                    21.07.060F.18., OR

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21                    C. OTHER METHODS, AS APPROVED BY THE  
22                    DIRECTOR, THAT PROVIDE SAFE, CONVENIENT,  
23                    AND ADEQUATE PEDESTRIAN ACCESS.  
24

25                    *(Note to Code Revisor: The S-Version's revisions to the following subsection*  
26                    *21.07.110C.6., Building and Site Orientation Menu, restore the current code text*  
27                    *of this subsection that the ordinance had deleted, except that a reference to*  
28                    *21.07.060F.17. is re-numbered to F.16 in subsection 21.07.110C.6.i.)*  
29

30                    **6. Building and Site Orientation Menu. Buildings shall be**  
31                    **oriented to surrounding streets, sidewalks, common**  
32                    **private open spaces, and the neighborhood public realm**  
33                    **through at least three of the following menu choices. The**  
34                    **director may reduce the requirement to two menu**  
35                    **choices if he or she determines that the primary objective**  
36                    **of the menu option is achieved or that the specific site**  
37                    **limits availability of a majority of the options.**

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39                    **a. Courtyard Housing. Arrange or configure the**  
40                    **building(s) to enclose and frame a housing**  
41                    **courtyard as described in subsection 21.07.060F.7.**

42  
43                    **b. Orientation of Living Spaces and Windows—**  
44                    **Ground Floor. Provide windows and/or entrances**  
45                    **to habitable living spaces comprising, at least 10**  
46                    **percent of the ground-floor wall area of up to two**  
47                    **building elevations facing a street or having a**  
48                    **primary front entrance. Windows in a garage door**

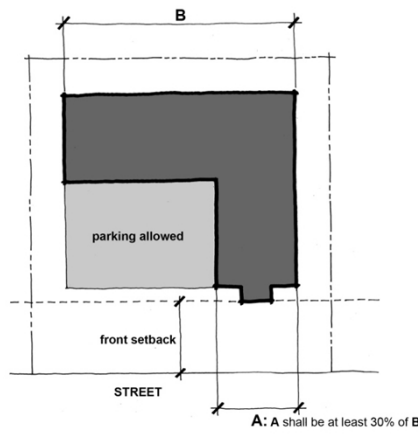


1 do not count towards the minimum area in this  
2 section.

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4 **c. *Orientation of Living Spaces and Windows—***  
5 **Overall Development. Provide windows and/or**  
6 **entrances to habitable living spaces, comprising at**  
7 **least 10 percent of the total wall area of up to two**  
8 **building elevations facing a street or having a**  
9 **primary front entrance. Windows in a garage door**  
10 **do not count towards the minimum area of this**  
11 **section.**

12  
13 **d. *Orientation of Living Spaces and Windows—***  
14 **Additional Transparency. Increase the percentage**  
15 **of the wall area comprised of windows and/or**  
16 **pedestrian entrances to 20 percent in subsections**  
17 **6.b. and/or 6.c. above.**

18  
19 **e. *Street Frontage – Parking Beside or Behind the***  
20 **Building. No more than 70 percent of a street-**  
21 **facing building elevation shall have parking**  
22 **facilities (including garages) between it and the**  
23 **street.**

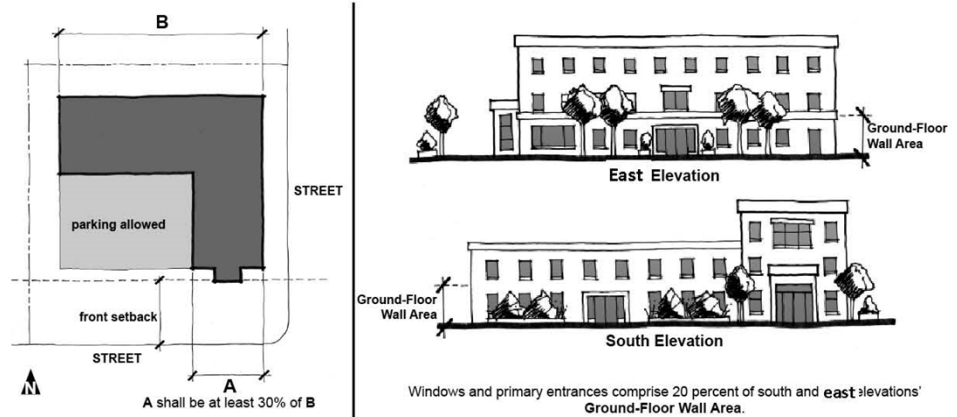


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26 **f. *Street Frontage – Limited Parking Width. As an***  
27 **alternative to 6.e. above, limit driveways and**  
28 **parking facilities to no more than 50 percent of the**  
29 **total site area between the building and a street,**  
30 **and garages to no more than 50 percent of the**  
31 **street facing building elevation.**

32  
33 **g. *Street Corner Building. On a corner lot, provide***  
34 **choices 6.e. and 6.f. on both street frontages, such**  
35 **that the building (including its habitable floor area**  
36 **with windows) is placed nearest the corner, and**  
37 **any parking facilities are located beside or behind**  
38 **the building away from the street corner.**

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**Compliance with this feature counts for meeting two.**



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- h. Limited Front-Facing Garage Width for Townhouses.** Where garages for individual townhouse-style dwelling units face the street or are on the same building elevation as the primary front entry to the dwellings, limit the garage door width to no more than 50 percent of the width of each dwelling, or up to 67 percent provided the building façade achieves one more feature than required in subsection C.7., Building Articulation Menu.
- i. Enhanced Sidewalk.** Provide an enhanced sidewalk conforming to subsection 21.07.060F.16. on at least one street frontage that is not less than 100 linear feet.
- j. Separated Walkway to the Street.** Connect all primary entrances to the street by a clear and direct walkway separated from and not routed through a parking facility.
- k. Visible Front Entries.**

  - i.** Place the primary front entrance(s) (at least one shared primary front entrance for a multifamily structure or at least 50 percent of individual unit entrances) on a street-facing building elevation, or facing a common private open space that is visible from and has direct access to the street.
  - ii.** As an alternative, place the primary front entrance(s) on a façade at an angle of up to

**90 degrees from the street, where there is an unobstructed line of sight from the street or sidewalk edge (abutting the site) to the entry door, and a sheltering roof structure no more than 12 feet above the floor of the entry as a permanent architectural feature projecting from the façade.**

**l. Enhanced Front Yard Landscaping. Provide one level higher of site or parking lot perimeter landscaping along the street frontages (up to a maximum of two frontages) than otherwise required by this title. For example, if L1 landscaping is required, provide L2 landscaping.**

**m. Site Entry Feature. Highlight and define a pedestrian and/or vehicle entrance to a development site using three or more of the following elements:**

**i. Landscape treatment with seasonal color and trees, which clearly distinguishes and highlights the site entry.**

**ii. Plaza or courtyard as described in subsection 21.07.060F6. or 7.**

**iii. Identifying building primary entrance form including a covered entry, when the primary entrance is within 40 feet of the site entrance.**

**iv. Special paving, pedestrian scale lighting, and/or bollards.**

**v. Ornamental gate and/or fence.**

**n. Director Approval. Other methods, as approved by the director, that provide appropriate building and site layouts relative to the surrounding neighborhood and streets.**

**6. BUILDING AND SITE ORIENTATION MENU. BUILDINGS SHALL BE ORIENTED TO SURROUNDING STREETS, SIDEWALKS, COMMON PRIVATE OPEN SPACES, AND THE NEIGHBORHOOD PUBLIC REALM THROUGH AT LEAST THREE OF THE FOLLOWING MENU CHOICES. THE DIRECTOR MAY REDUCE THE REQUIREMENT TO TWO MENU CHOICES IF HE OR SHE DETERMINES THAT**

1 THE PRIMARY OBJECTIVE OF THE MENU OPTION IS  
2 ACHIEVED OR THAT THE SPECIFIC SITE LIMITS  
3 AVAILABILITY OF A MAJORITY OF THE OPTIONS.  
4

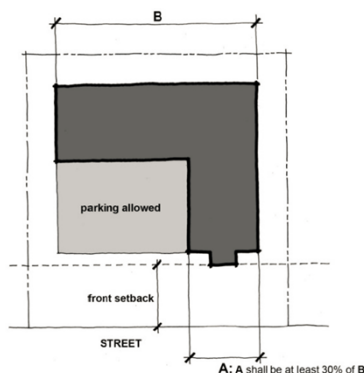
5 A. *COURTYARD HOUSING.* ARRANGE OR  
6 CONFIGURE THE BUILDING(S) TO ENCLOSE AND  
7 FRAME A HOUSING COURTYARD AS DESCRIBED  
8 IN SUBSECTION 21.07.060F.7.  
9

10 B. *ORIENTATION OF LIVING SPACES AND*  
11 *WINDOWS—GROUND FLOOR.* PROVIDE  
12 WINDOWS AND/OR ENTRANCES TO HABITABLE  
13 LIVING SPACES COMPRISING, AT LEAST 10  
14 PERCENT OF THE GROUND-FLOOR WALL AREA  
15 OF UP TO TWO BUILDING ELEVATIONS FACING A  
16 STREET OR HAVING A PRIMARY FRONT  
17 ENTRANCE. WINDOWS IN A GARAGE DOOR DO  
18 NOT COUNT TOWARDS THE MINIMUM AREA IN  
19 THIS SECTION.  
20

21 C. *ORIENTATION OF LIVING SPACES AND*  
22 *WINDOWS—OVERALL DEVELOPMENT.* PROVIDE  
23 WINDOWS AND/OR ENTRANCES TO HABITABLE  
24 LIVING SPACES, COMPRISING AT LEAST 10  
25 PERCENT OF THE TOTAL WALL AREA OF UP TO  
26 TWO BUILDING ELEVATIONS FACING A STREET  
27 OR HAVING A PRIMARY FRONT ENTRANCE.  
28 WINDOWS IN A GARAGE DOOR DO NOT COUNT  
29 TOWARDS THE MINIMUM AREA OF THIS SECTION.  
30

31 D. *ORIENTATION OF LIVING SPACES AND*  
32 *WINDOWS—ADDITIONAL TRANSPARENCY.*  
33 INCREASE THE PERCENTAGE OF THE WALL  
34 AREA COMPRISED OF WINDOWS AND/OR  
35 PEDESTRIAN ENTRANCES TO 20 PERCENT IN  
36 SUBSECTIONS 6.B. AND/OR 6.C. ABOVE.  
37

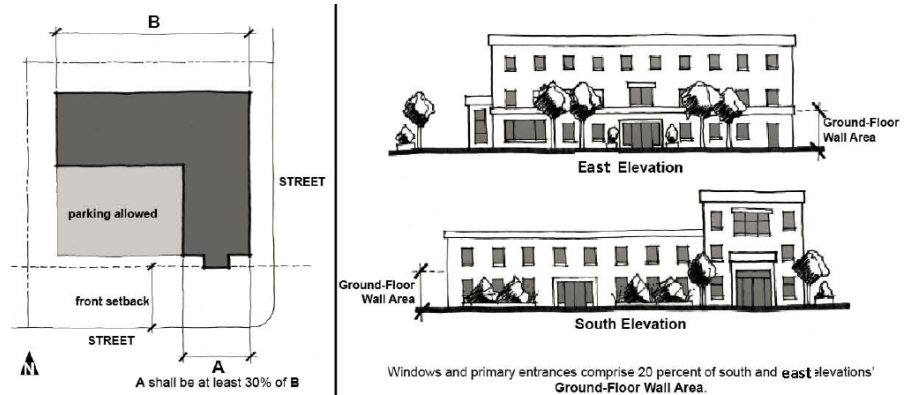
38 E. *STREET FRONTAGE – PARKING BESIDE OR*  
39 *BEHIND THE BUILDING.* NO MORE THAN 70  
40 PERCENT OF A STREET-FACING BUILDING  
41 ELEVATION SHALL HAVE PARKING FACILITIES



(INCLUDING GARAGES) BETWEEN IT AND THE STREET.]

[F. *STREET FRONTAGE – LIMITED PARKING WIDTH.* AS AN ALTERNATIVE TO 6.E. ABOVE, LIMIT DRIVEWAYS AND PARKING FACILITIES TO NO MORE THAN 50 PERCENT OF THE TOTAL SITE AREA BETWEEN THE BUILDING AND A STREET, AND GARAGES TO NO MORE THAN 50 PERCENT OF THE STREET FACING BUILDING ELEVATION.

G. *STREET CORNER BUILDING.* ON A CORNER LOT, PROVIDE CHOICES 6.E. AND 6.F. ON BOTH STREET FRONTAGES, SUCH THAT THE BUILDING (INCLUDING ITS HABITABLE FLOOR AREA WITH WINDOWS) IS PLACED NEAREST THE CORNER, AND ANY PARKING FACILITIES ARE LOCATED BESIDE OR BEHIND THE BUILDING AWAY FROM THE STREET CORNER. COMPLIANCE WITH THIS FEATURE COUNTS FOR MEETING TWO.



H. *LIMITED FRONT-FACING GARAGE WIDTH FOR TOWNHOUSES.* WHERE GARAGES FOR INDIVIDUAL TOWNHOUSE-STYLE DWELLING UNITS FACE THE STREET OR ARE ON THE SAME BUILDING ELEVATION AS THE PRIMARY FRONT ENTRY TO THE DWELLINGS, LIMIT THE GARAGE DOOR WIDTH TO NO MORE THAN 50 PERCENT OF THE WIDTH OF EACH DWELLING, OR UP TO 67 PERCENT PROVIDED THE BUILDING FAÇADE ACHIEVES ONE MORE FEATURE THAN REQUIRED IN SUBSECTION C.7., BUILDING ARTICULATION MENU.

I. *ENHANCED SIDEWALK.* PROVIDE AN ENHANCED SIDEWALK CONFORMING TO SUBSECTION 21.07.060F.17. ON AT LEAST ONE STREET

1 FRONTAGE THAT IS NOT LESS THAN 100 LINEAR  
2 FEET.

3  
4 J. *SEPARATED WALKWAY TO THE STREET.*  
5 CONNECT ALL PRIMARY ENTRANCES TO THE  
6 STREET BY A CLEAR AND DIRECT WALKWAY  
7 SEPARATED FROM AND NOT ROUTED THROUGH  
8 A PARKING FACILITY.

9  
10 K. *VISIBLE FRONT ENTRIES.*

11  
12 I. PLACE THE PRIMARY FRONT ENTRANCE(S)  
13 (AT LEAST ONE SHARED PRIMARY FRONT  
14 ENTRANCE FOR A MULTIFAMILY  
15 STRUCTURE OR AT LEAST 50 PERCENT OF  
16 INDIVIDUAL UNIT ENTRANCES) ON A  
17 STREET-FACING BUILDING ELEVATION, OR  
18 FACING A COMMON PRIVATE OPEN SPACE  
19 THAT IS VISIBLE FROM AND HAS DIRECT  
20 ACCESS TO THE STREET.

21  
22 II. AS AN ALTERNATIVE, PLACE THE PRIMARY  
23 FRONT ENTRANCE(S) ON A FAÇADE AT AN  
24 ANGLE OF UP TO 90 DEGREES FROM THE  
25 STREET, WHERE THERE IS AN  
26 UNOBSTRUCTED LINE OF SIGHT FROM THE  
27 STREET OR SIDEWALK EDGE (ABUTTING  
28 THE SITE) TO THE ENTRY DOOR, AND A  
29 SHELTERING ROOF STRUCTURE NO MORE  
30 THAN 12 FEET ABOVE THE FLOOR OF THE  
31 ENTRY AS A PERMANENT ARCHITECTURAL  
32 FEATURE PROJECTING FROM THE  
33 FAÇADE.

34  
35 L. *ENHANCED FRONT YARD LANDSCAPING.*  
36 PROVIDE ONE LEVEL HIGHER OF SITE OR  
37 PARKING LOT PERIMETER LANDSCAPING ALONG  
38 THE STREET FRONTAGES (UP TO A MAXIMUM OF  
39 TWO FRONTAGES) THAN OTHERWISE REQUIRED  
40 BY THIS TITLE. FOR EXAMPLE, IF L1  
41 LANDSCAPING IS REQUIRED, PROVIDE L2  
42 LANDSCAPING.

43  
44 M. *SITE ENTRY FEATURE.* HIGHLIGHT AND DEFINE A  
45 PEDESTRIAN AND/OR VEHICLE ENTRANCE TO A  
46 DEVELOPMENT SITE USING THREE OR MORE OF  
47 THE FOLLOWING ELEMENTS:  
48

- I. LANDSCAPE TREATMENT WITH SEASONAL COLOR AND TREES, WHICH CLEARLY DISTINGUISHES AND HIGHLIGHTS THE SITE ENTRY.
- II. PLAZA OR COURTYARD AS DESCRIBED IN SUBSECTION 21.07.060F6. OR 7.
- III. IDENTIFYING BUILDING PRIMARY ENTRANCE FORM INCLUDING A COVERED ENTRY, WHEN THE PRIMARY ENTRANCE IS WITHIN 40 FEET OF THE SITE ENTRANCE.
- IV. SPECIAL PAVING, PEDESTRIAN SCALE LIGHTING, AND/OR BOLLARDS.
- V. ORNAMENTAL GATE AND/OR FENCE.

N. *DIRECTOR APPROVAL.* OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE APPROPRIATE BUILDING AND SITE LAYOUTS RELATIVE TO THE SURROUNDING NEIGHBORHOOD AND STREETS.]

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7[4][7]. *Building Articulation Menu.*

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8[5][8]. *Northern Climate Weather Protection and Sunlight Menu.*

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(Note to Code Revisor: The S-Version's revisions to the following subsection 21.07.110C.9., Entryway Treatment, restore the current code text of this subsection that the ordinance had deleted.)

**9. Entryway Treatment. Primary front entrances shall be given emphasis and physical access as follows. A porch, landing, patio, or other semi-private outdoor entryway space with a minimum dimension of four feet shall be provided that is distinguished from adjacent areas and vehicular parking by a vertical separation or change in surface material. Examples of features that meet the intent of the section include the following:**

**a. Outdoor sheltering roof structure such as an overhang, recess, portico, or other permanent architectural feature covering at least 12 square feet.**

**b. Façade variation through wall modulation (changes in wall plane) or changes in exterior**

**finishes (color or materials) that relate to and emphasize the entry.**

**c. Entry detail elements such as double doors, entry side light or transom windows, or planter boxes.**

**d. Front steps creating a vertical separation, and/or vertical vegetation or low front fencing, to define the transition from public to more private outdoor entryway space.**

**e. Landscaping and pedestrian features, not otherwise required by this title, such as integrated planters, pedestrian-scale lighting or accent lighting, or special paving treatments.**

**f. Other methods as approved by the director that emphasize primary front entrances.**

[9. *ENTRYWAY TREATMENT.* PRIMARY FRONT ENTRANCES SHALL BE GIVEN EMPHASIS AND PHYSICAL ACCESS AS FOLLOWS. A PORCH, LANDING, PATIO, OR OTHER SEMI-PRIVATE OUTDOOR ENTRYWAY SPACE WITH A MINIMUM DIMENSION OF FOUR FEET SHALL BE PROVIDED THAT IS DISTINGUISHED FROM ADJACENT AREAS AND VEHICULAR PARKING BY A VERTICAL SEPARATION OR CHANGE IN SURFACE MATERIAL. EXAMPLES OF FEATURES THAT MEET THE INTENT OF THE SECTION INCLUDE THE FOLLOWING:

A. OUTDOOR SHELTERING ROOF STRUCTURE SUCH AS AN OVERHANG, RECESS, PORTICO, OR OTHER PERMANENT ARCHITECTURAL FEATURE COVERING AT LEAST 12 SQUARE FEET.

B. FAÇADE VARIATION THROUGH WALL MODULATION (CHANGES IN WALL PLANE) OR CHANGES IN EXTERIOR FINISHES (COLOR OR MATERIALS) THAT RELATE TO AND EMPHASIZE THE ENTRY.

C. ENTRY DETAIL ELEMENTS SUCH AS DOUBLE DOORS, ENTRY SIDE LIGHT OR TRANSOM WINDOWS, OR PLANTER BOXES.

D. FRONT STEPS CREATING A VERTICAL SEPARATION, AND/OR VERTICAL VEGETATION OR LOW FRONT FENCING, TO DEFINE THE







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**(B) The front building elevation has two or more facades that are offset by at least 16 inches. Each façade or a combination of offset facades shall be at least one third of the area of the building elevation.**

**(C) Front-facing balcony, accessible from a habitable room, at least six feet wide, that projects from a façade at least two feet and is enclosed by an open railing.**

**ii. List B:**

**(A) A primary entrance area with a covered porch or landing at least eight feet wide, incorporating visual enhancements such as gabled roof forms, roof brackets, fascia boards, side lights, and/or ornamental columns divided visually into top, middle, and bottom.**

**(B) A bay window on the front elevation at least six feet wide that extends a minimum of 12 inches outward from a façade, forming a bay or alcove in the room within.**

**(C) If the garage is more than one car wide, multiple garage doors are used.**

**iii. List C:**

**(A) Windows and primary entrance door(s) that occupy a minimum of 25 percent of the wall area of the front elevation. Windows in the garage door do not count towards the 25 percent.**



1                                    **d.    *Narrow Lot/Small Home Reduction. An overall***  
2                                    **reduction in required window and primary entrance**  
3                                    **door area to 7% may be approved when the**  
4                                    **calculated wall area is under 500 square feet. Units**  
5                                    **using this reduction shall comply with subsection**  
6                                    **21.07.110D.4.c.**  
7

8                                    **e.    *Additions and Renovations. On existing***  
9                                    **structures, only the wall area affected by either an**  
10                                   **addition or a renovation shall be required to**  
11                                   **comply with the opening requirements. Unaffected**  
12                                   **wall areas need not comply. Garage additions are**  
13                                   **exempt from opening requirements. The director**  
14                                   **may eliminate or reduce the requirement for**  
15                                   **window openings on additions/renovations based**  
16                                   **on project-specific circumstances (such as the**  
17                                   **nature of the spaces being added, building**  
18                                   **orientation, or structural requirements).**  
19

20                                   [3.    *PRIMARY ENTRANCE.*

21  
22                                   A.    A PORCH OR LANDING WITH AT LEAST 16  
23                                          SQUARE FEET SHALL BE PROVIDED AT THE  
24                                          PRIMARY ENTRANCE. THE PORCH OR LANDING  
25                                          SHALL BE COVERED BY A ROOF OF AT LEAST 16  
26                                          SQUARE FEET.

27  
28                                   B.    THE PRIMARY ENTRANCE OF EACH RESIDENCE  
29                                          AND THE WALKWAY TO THAT ENTRANCE SHALL  
30                                          BE CLEARLY VISIBLE FROM THE STREET.  
31                                          PRIMARY ENTRANCES SHALL NOT BE LOCATED  
32                                          ON THE REAR OF THE STRUCTURE.

33  
34                                   C.    A HARD-SURFACED PEDESTRIAN WALKWAY  
35                                          SHALL BE PROVIDED FROM THE STREET,  
36                                          SIDEWALK, OR DRIVEWAY TO THE PRIMARY  
37                                          ENTRANCE. ROOF DRAINAGE SHALL NOT FALL  
38                                          UPON THE WALKWAY.

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40                                   4.    *GARAGES.*

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42                                   A.    WHERE A GARAGE (WITH NO HABITABLE FLOOR  
43                                          AREA ABOVE) EXTENDS FROM THE REST OF THE  
44                                          STRUCTURE TOWARDS THE STREET, THE WIDTH  
45                                          OF THE NON-GARAGE PORTION OF THE FRONT  
46                                          BUILDING ELEVATION SHALL BE NO LESS THAN  
47                                          THE LENGTH THAT THE GARAGE EXTENDS FROM  
48                                          THE REST OF THE STRUCTURE.  
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B. GARAGE DOORS FACING THE STREET SHALL COMPRISE NO MORE THAN 67 PERCENT OF THE TOTAL WIDTH OF A DWELLING'S BUILDING ELEVATION.

C. DWELLING UNITS WITH A STREET-FACING BUILDING ELEVATION THAT IS 40 FEET WIDE OR NARROWER AND WITH GARAGE DOORS THAT FACE THE STREET SHALL FEATURE AT LEAST ONE DESIGN ELEMENT FROM EACH OF THE THREE LISTS BELOW.

I. LIST A:

(A) AT LEAST ONE DORMER THAT IS ORIENTED TOWARD THE STREET.

(B) THE FRONT BUILDING ELEVATION HAS TWO OR MORE FACADES THAT ARE OFFSET BY AT LEAST 16 INCHES. EACH FAÇADE OR A COMBINATION OF OFFSET FACADES SHALL BE AT LEAST ONE THIRD OF THE AREA OF THE BUILDING ELEVATION.

(C) FRONT-FACING BALCONY, ACCESSIBLE FROM A HABITABLE ROOM, AT LEAST SIX FEET WIDE, THAT PROJECTS FROM A FAÇADE AT LEAST TWO FEET AND IS ENCLOSED BY AN OPEN RAILING.

II. LIST B:

(A) A PRIMARY ENTRANCE AREA WITH A COVERED PORCH OR LANDING AT LEAST EIGHT FEET WIDE, INCORPORATING VISUAL ENHANCEMENTS SUCH AS GABLED ROOF FORMS, ROOF BRACKETS, FASCIA BOARDS, SIDE LIGHTS, AND/OR ORNAMENTAL COLUMNS DIVIDED VISUALLY INTO TOP, MIDDLE, AND BOTTOM.

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(B) A BAY WINDOW ON THE FRONT ELEVATION AT LEAST SIX FEET WIDE THAT EXTENDS A MINIMUM OF 12 INCHES OUTWARD FROM A FAÇADE, FORMING A BAY OR ALCOVE IN THE ROOM WITHIN.

(C) IF THE GARAGE IS MORE THAN ONE CAR WIDE, MULTIPLE GARAGE DOORS ARE USED.

III. LIST C:

(A) WINDOWS AND PRIMARY ENTRANCE DOOR(S) THAT OCCUPY A MINIMUM OF 25 PERCENT OF THE WALL AREA OF THE FRONT ELEVATION. WINDOWS IN THE GARAGE DOOR DO NOT COUNT TOWARDS THE 25 PERCENT.

(B) TRIM (MINIMUM THREE AND ONE HALF INCHES WIDE) OF A DIFFERENT COLOR FROM THE PRIMARY SIDING COLOR, SHALL OUTLINE ALL WINDOWS, DOORS, AND ROOF EDGES ON THE FRONT BUILDING ELEVATION, AND MAY OUTLINE CORNERS AND PROJECTIONS/RECESSES ON THE FRONT BUILDING ELEVATION.

(C) A MINIMUM OF TWO DIFFERENT SIDING MATERIALS AND/OR PATTERNS ARE USED ON THE FRONT BUILDING ELEVATION. DOORS AND TRIM DO NOT QUALIFY AS A TYPE OF SIDING MATERIAL.

D. THE HOUSE MAY ENCROACH INTO THE PRIMARY FRONT SETBACK BY UP TO FIVE FEET WHEN THERE IS NO GARAGE, OR WHERE THERE IS A GARAGE (ATTACHED OR DETACHED) WHERE THE FRONT WALL OF THE GARAGE IS LOCATED AT LEAST 8 FEET BEHIND THE FRONT FAÇADE OF THE HOUSE.

5. *WINDOWS.*

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- A. *MINIMUM WINDOW AREA REQUIREMENT.* WINDOWS AND PRIMARY ENTRANCE DOOR(S) SHALL OCCUPY A MINIMUM OF 10 PERCENT OF THE WALL AREA OF A BUILDING ELEVATION FACING A STREET OR REQUIRED PRIVATE COMMON OPEN SPACE (UP TO A MAXIMUM OF TWO ELEVATIONS). THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT ON ONE ELEVATION (IN INSTANCES WHERE UP TO TWO APPLY) BASED ON SITE SPECIFIC CIRCUMSTANCES, STRUCTURAL REQUIREMENTS FOR SHEAR, AND ORGANIZATION OF SPACES IN THE HOME (WINDOWS ARE NOT REQUIRED IN ROOMS NOT NORMALLY INHABITED OR IN GARAGES).
- B. *WALL AREA CALCULATION.* WALL AREA CALCULATION SHALL BE IN ACCORDANCE WITH SUBSECTION 21.07.110C.3.B.
- C. AN OVERALL REDUCTION IN REQUIRED WINDOW AREA MAY BE APPROVED IF DEMONSTRATED BY CALCULATION BY AN ENERGY RATER CERTIFIED BY THE STATE OF ALASKA THAT INSTALLING THE REQUIRED WINDOW AREA WILL REDUCE THE ENERGY RATING BELOW A 5-STAR ENERGY RATING.
- D. *NARROW LOT/SMALL HOME REDUCTION.* AN OVERALL REDUCTION IN REQUIRED WINDOW AND PRIMARY ENTRANCE DOOR AREA TO 7% MAY BE APPROVED WHEN THE CALCULATED WALL AREA IS UNDER 500 SQUARE FEET. UNITS USING THIS REDUCTION SHALL COMPLY WITH SUBSECTION 21.07.110D.4.C.
- E. *ADDITIONS AND RENOVATIONS.* ON EXISTING STRUCTURES, ONLY THE WALL AREA AFFECTED BY EITHER AN ADDITION OR A RENOVATION SHALL BE REQUIRED TO COMPLY WITH THE OPENING REQUIREMENTS. UNAFFECTED WALL AREAS NEED NOT COMPLY. GARAGE ADDITIONS ARE EXEMPT FROM OPENING REQUIREMENTS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT FOR WINDOW OPENINGS ON ADDITIONS/RENOVATIONS BASED ON PROJECT-SPECIFIC CIRCUMSTANCES (SUCH AS THE NATURE OF THE SPACES BEING ADDED,



BUILDING ORIENTATION, OR STRUCTURAL  
REQUIREMENTS).]

**E. Prohibited Structures.**

**[Reserved]**

[E. PROHIBITED STRUCTURES. [RESERVED]]

**F**(**E**)(**F**). *Site Design.*  
\*\*\* \*\*

(Note to Code Revisor: The S-Version's revisions to the following subsection 21.07.110F.3., Driveway Width, restore the current code text of this subsection that the ordinance had deleted.)

**3. Driveway Width.**

**a. Purpose.** This section limits the width of a driveway at the property line and at the street curb. The intent of these limitations is to provide adequate space for snow storage within the right-of-way, to have space for on-street parking where appropriate, and to discourage the majority of the front area of a lot from being paved and/or used for vehicle parking.

**b. Applicability**

**i.** This section applies to driveway throat width at the property line and street curb.

**ii.** Residential driveways are also subject to the municipal driveway standards currently established by the traffic engineer. Where there is a conflict, this section shall govern. Access to streets owned by the state of Alaska requires compliance with state driveway standards, as provided in subsection 21.07.090H.8.d.

**iii.** When a driveway serves both residential and nonresidential principal uses, the driveway dimensions shall be as required for the nonresidential use, unless approved otherwise by the traffic engineer.

**c. Percent of Lot Frontage.** The total width of driveway entrances to a residential lot from a street shall not exceed 40 percent of the frontage of the

1 lot, or 33 percent of the frontage if the platting  
2 authority or traffic engineer finds that conditions  
3 warrant it.

4  
5 i. A driveway for multifamily dwellings, mixed-  
6 use dwellings, or a group living use may  
7 always be at least 14 feet wide.

8  
9 ii. A driveway for a single-family, two-family, or  
10 townhouse dwelling may always be at least  
11 ten feet wide, provided the traffic engineer  
12 determines snow storage, traffic flow and  
13 safety, and the urban context are addressed,  
14 and provided townhouse driveways are  
15 attached in pairs to the maximum extent  
16 feasible.

17  
18 iii. Flag lots are exempt from the percentage  
19 limitations, but shall have a maximum  
20 driveway width of 20 feet. Abutting flag lots  
21 may share a driveway up to 24 feet wide (12  
22 feet per lot).

23  
24 d. Exceptions. The traffic engineer and the planning  
25 director may approve a departure from the  
26 standards of this section, such as a narrower  
27 driveway, if documentation prepared by a licensed  
28 professional in the state of Alaska registered with  
29 the Alaska State Board of Registration for  
30 Architects, Engineers and Land Surveyors  
31 demonstrates the driveway still meets this  
32 chapter's standards and the municipal driveway  
33 standards memo issued by the municipal traffic  
34 engineer, and that change is appropriate. Approval  
35 shall be contingent on factors such as street  
36 classification, street typology, urban context,  
37 traffic volume and speed, curb return radii, street  
38 travel lane offset from face of curb, pedestrian and  
39 bicycle facilities, snow storage, driveway  
40 configuration and length, site and project  
41 characteristics, number of vehicles expected to  
42 use the driveway, and comprehensive plan  
43 policies. The traffic engineer and planning director  
44 may also be more restrictive than the standards of  
45 this section, provided they document the rationale.

46  
47 [3. *DRIVEWAY WIDTH.*

48 A. PURPOSE. THIS SECTION LIMITS THE WIDTH OF  
49 A DRIVEWAY AT THE PROPERTY LINE AND AT

1 THE STREET CURB. THE INTENT OF THESE  
2 LIMITATIONS IS TO PROVIDE ADEQUATE SPACE  
3 FOR SNOW STORAGE WITHIN THE RIGHT-OF-  
4 WAY, TO HAVE SPACE FOR ON-STREET PARKING  
5 WHERE APPROPRIATE, AND TO DISCOURAGE  
6 THE MAJORITY OF THE FRONT AREA OF A LOT  
7 FROM BEING PAVED AND/OR USED FOR VEHICLE  
8 PARKING.

9  
10 B. *APPLICABILITY.*

11  
12 I. THIS SECTION APPLIES TO DRIVEWAY  
13 THROAT WIDTH AT THE PROPERTY LINE  
14 AND STREET CURB.

15  
16 II. RESIDENTIAL DRIVEWAYS ARE ALSO  
17 SUBJECT TO THE MUNICIPAL DRIVEWAY  
18 STANDARDS CURRENTLY ESTABLISHED  
19 BY THE TRAFFIC ENGINEER. WHERE  
20 THERE IS A CONFLICT, THIS SECTION  
21 SHALL GOVERN. ACCESS TO STREETS  
22 OWNED BY THE STATE OF ALASKA  
23 REQUIRES COMPLIANCE WITH STATE  
24 DRIVEWAY STANDARDS, AS PROVIDED IN  
25 SUBSECTION 21.07.090H.8.D.

26  
27 III. WHEN A DRIVEWAY SERVES BOTH  
28 RESIDENTIAL AND NONRESIDENTIAL  
29 PRINCIPAL USES, THE DRIVEWAY  
30 DIMENSIONS SHALL BE AS REQUIRED FOR  
31 THE NONRESIDENTIAL USE, UNLESS  
32 APPROVED OTHERWISE BY THE TRAFFIC  
33 ENGINEER.

34  
35 C. *PERCENT OF LOT FRONTAGE.* THE TOTAL WIDTH  
36 OF DRIVEWAY ENTRANCES TO A RESIDENTIAL  
37 LOT FROM A STREET SHALL NOT EXCEED 40  
38 PERCENT OF THE FRONTAGE OF THE LOT, OR 33  
39 PERCENT OF THE FRONTAGE IF THE PLATTING  
40 AUTHORITY OR TRAFFIC ENGINEER FINDS THAT  
41 CONDITIONS WARRANT IT.

42  
43 I. A DRIVEWAY FOR MULTIFAMILY  
44 DWELLINGS, MIXED-USE DWELLINGS, OR A  
45 GROUP LIVING USE MAY ALWAYS BE AT  
46 LEAST 14 FEET WIDE.

47  
48 II. A DRIVEWAY FOR A SINGLE-FAMILY, TWO-  
49 FAMILY, OR TOWNHOUSE DWELLING MAY

1 ALWAYS BE AT LEAST TEN FEET WIDE,  
2 PROVIDED THE TRAFFIC ENGINEER  
3 DETERMINES SNOW STORAGE, TRAFFIC  
4 FLOW AND SAFETY, AND THE URBAN  
5 CONTEXT ARE ADDRESSED, AND  
6 PROVIDED TOWNHOUSE DRIVEWAYS ARE  
7 ATTACHED IN PAIRS TO THE MAXIMUM  
8 EXTENT FEASIBLE.

9  
10 III. FLAG LOTS ARE EXEMPT FROM THE  
11 PERCENTAGE LIMITATIONS, BUT SHALL  
12 HAVE A MAXIMUM DRIVEWAY WIDTH OF 20  
13 FEET. ABUTTING FLAG LOTS MAY SHARE A  
14 DRIVEWAY UP TO 24 FEET WIDE (12 FEET  
15 PER LOT).

16  
17 D. *EXCEPTIONS.* THE TRAFFIC ENGINEER AND THE  
18 PLANNING DIRECTOR MAY APPROVE A  
19 DEPARTURE FROM THE STANDARDS OF THIS  
20 SECTION, SUCH AS A NARROWER DRIVEWAY, IF  
21 DOCUMENTATION PREPARED BY A LICENSED  
22 PROFESSIONAL IN THE STATE OF ALASKA  
23 REGISTERED WITH THE ALASKA STATE BOARD  
24 OF REGISTRATION FOR ARCHITECTS,  
25 ENGINEERS AND LAND SURVEYORS  
26 DEMONSTRATES THE DRIVEWAY STILL MEETS  
27 THIS CHAPTER'S STANDARDS AND THE  
28 MUNICIPAL DRIVEWAY STANDARDS MEMO  
29 ISSUED BY THE MUNICIPAL TRAFFIC ENGINEER,  
30 AND THAT CHANGE IS APPROPRIATE. APPROVAL  
31 SHALL BE CONTINGENT ON FACTORS SUCH AS  
32 STREET CLASSIFICATION, STREET TYPOLOGY,  
33 URBAN CONTEXT, TRAFFIC VOLUME AND SPEED,  
34 CURB RETURN RADII, STREET TRAVEL LANE  
35 OFFSET FROM FACE OF CURB, PEDESTRIAN AND  
36 BICYCLE FACILITIES, SNOW STORAGE,  
37 DRIVEWAY CONFIGURATION AND LENGTH, SITE  
38 AND PROJECT CHARACTERISTICS, NUMBER OF  
39 VEHICLES EXPECTED TO USE THE DRIVEWAY,  
40 AND COMPREHENSIVE PLAN POLICIES. THE  
41 TRAFFIC ENGINEER AND PLANNING DIRECTOR  
42 MAY ALSO BE MORE RESTRICTIVE THAN THE  
43 STANDARDS OF THIS SECTION, PROVIDED THEY  
44 DOCUMENT THE RATIONALE.]

45  
46 *(Note to Code Revisor: The S-Version's revisions to the following subsection*  
47 *21.07.110F.4., Alleys, restore the current code text of this subsection that the*  
48 *ordinance had deleted.)*  
49

1                   **4. Alleys.**  
2

3                   **a. The intent of this section is to promote vehicle**  
4                   **driveway access from rear alleys and reduce the**  
5                   **impacts of front yard driveways in older urban**  
6                   **neighborhoods.**

7  
8                   **b. This section applies to residential developments**  
9                   **located in the Traditional Neighborhood Design**  
10                   **areas designated on Map 2-1: Anchorage 2040**  
11                   **Land Use Plan Map, in the Anchorage 2040 Land**  
12                   **Use Plan.**

13  
14                   **c. Access to parking for residential uses shall be**  
15                   **from the alley when the site abuts an alley, except**  
16                   **that street access is permitted in any of the**  
17                   **following situations:**

18  
19                   **i. Access to a townhouse dwelling on a corner**  
20                   **lot may be from the street frontage having**  
21                   **the secondary front setback or the alley.**

22  
23                   **ii. Due to the relationship of the alley to the**  
24                   **street system and/or the proposed housing**  
25                   **density of the development, the traffic**  
26                   **engineer determines that use of the alley for**  
27                   **parking access would be a significant traffic**  
28                   **impact or safety hazard.**

29  
30                   **iii. The traffic engineer determines that**  
31                   **topography or other natural feature or**  
32                   **physical barrier makes alley access**  
33                   **infeasible.**

34  
35                   **iv. The alley is not improved and traffic**  
36                   **engineer determines that improvement is**  
37                   **not feasible.**

38  
39                   **v. A single-family dwelling, two-family**  
40                   **dwelling, or townhouse dwelling with two**  
41                   **units, with alley access may have a garage**  
42                   **or driveway that faces the street if the garage**  
43                   **door is no wider than 10 feet and the**  
44                   **driveway no wider than 12 feet at any point.**

45  
46                   **d. In situations where a group of lots front an entire**  
47                   **block on one side of a street between two**  
48                   **intersections, abut a mid-block alley, and are being**  
49                   **developed together, then parking access to the**

**structures shall be from the alley, and building(s) may encroach into the front setback by up to five feet.**

**e. If a new development includes alleys, the lot depth requirement is reduced by half the width of the alley and the lot area requirement is reduced by 12 percent for those lots that abut an alley. Vehicular access to all dwelling units on lots abutting alleys shall be from the alley, and vehicular access to such units from the street is prohibited.**

[4. ALLEYS.

A. THE INTENT OF THIS SECTION IS TO PROMOTE VEHICLE DRIVEWAY ACCESS FROM REAR ALLEYS AND REDUCE THE IMPACTS OF FRONT YARD DRIVEWAYS IN OLDER URBAN NEIGHBORHOODS.

B. THIS SECTION APPLIES TO RESIDENTIAL DEVELOPMENTS LOCATED IN THE TRADITIONAL NEIGHBORHOOD DESIGN AREAS DESIGNATED ON MAP 2-1: ANCHORAGE 2040 LAND USE PLAN MAP, IN THE ANCHORAGE 2040 LAND USE PLAN.

C. ACCESS TO PARKING FOR RESIDENTIAL USES SHALL BE FROM THE ALLEY WHEN THE SITE ABUTS AN ALLEY, EXCEPT THAT STREET ACCESS IS PERMITTED IN ANY OF THE FOLLOWING SITUATIONS:

I. ACCESS TO A TOWNHOUSE DWELLING ON A CORNER LOT MAY BE FROM THE STREET FRONTAGE HAVING THE SECONDARY FRONT SETBACK OR THE ALLEY.

II. DUE TO THE RELATIONSHIP OF THE ALLEY TO THE STREET SYSTEM AND/OR THE PROPOSED HOUSING DENSITY OF THE DEVELOPMENT, THE TRAFFIC ENGINEER DETERMINES THAT USE OF THE ALLEY FOR PARKING ACCESS WOULD BE A SIGNIFICANT TRAFFIC IMPACT OR SAFETY HAZARD.

III. THE TRAFFIC ENGINEER DETERMINES THAT TOPOGRAPHY OR OTHER NATURAL

1 FEATURE OR PHYSICAL BARRIER MAKES  
2 ALLEY ACCESS INFEASIBLE.

3  
4 IV. THE ALLEY IS NOT IMPROVED AND  
5 TRAFFIC ENGINEER DETERMINES THAT  
6 IMPROVEMENT IS NOT FEASIBLE.

7  
8 V. A SINGLE-FAMILY DWELLING, TWO-FAMILY  
9 DWELLING, OR TOWNHOUSE DWELLING  
10 WITH TWO UNITS, WITH ALLEY ACCESS  
11 MAY HAVE A GARAGE OR DRIVEWAY THAT  
12 FACES THE STREET IF THE GARAGE DOOR  
13 IS NO WIDER THAN 10 FEET AND THE  
14 DRIVEWAY NO WIDER THAN 12 FEET AT  
15 ANY POINT.

16  
17 B. IN SITUATIONS WHERE A GROUP OF LOTS FRONT  
18 AN ENTIRE BLOCK ON ONE SIDE OF A STREET  
19 BETWEEN TWO INTERSECTIONS, ABUT A MID-  
20 BLOCK ALLEY, AND ARE BEING DEVELOPED  
21 TOGETHER, THEN PARKING ACCESS TO THE  
22 STRUCTURES SHALL BE FROM THE ALLEY, AND  
23 BUILDING(S) MAY ENCROACH INTO THE FRONT  
24 SETBACK BY UP TO FIVE FEET.

25  
26 C. IF A NEW DEVELOPMENT INCLUDES ALLEYS, THE  
27 LOT DEPTH REQUIREMENT IS REDUCED BY HALF  
28 THE WIDTH OF THE ALLEY AND THE LOT AREA  
29 REQUIREMENT IS REDUCED BY 12 PERCENT FOR  
30 THOSE LOTS THAT ABUT AN ALLEY. VEHICULAR  
31 ACCESS TO ALL DWELLING UNITS ON LOTS  
32 ABUTTING ALLEYS SHALL BE FROM THE ALLEY,  
33 AND VEHICULAR ACCESS TO SUCH UNITS FROM  
34 THE STREET IS PROHIBITED.]

35 \*\*\* \*\*

36 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-132, 11-5-  
37 14; AO 2015-36, 5-14-15; AO 2015-100, 10-13-15; AO 2016-34(S), 4-12-  
38 16; AO 2016-136, 11-15-16; AO 2017-160, 12-19-17; AO 2018-59. 7-31-  
39 2018; AO 2019-132, 12-3-19; AO 2020-38, 4-28-20)

40  
41 **Section 3[4].** Anchorage Municipal Code Section 21.11.070, Development and  
42 Design Standards, is hereby amended to read as follows (*the remainder of the*  
43 *chapter is not affected and therefore not set out*):

44  
45 **21.11.070 Development and Design Standards.**

46 \*\*\* \*\*

47 F. **Off-Street Parking and Loading.**

48 \*\*\* \*\*

3. *Off-street parking loading design standards.* If off-street parking and/or loading is provided, it shall comply with all standards for off-street parking and loading in section 21.07.090. [, EXCEPT THAT:] *Small and compact parking space dimensions specific to the DT districts are provided in section 21.07.090H.9[12].*

A. PERMANENT PARKING NOT LOCATED WITHIN A SETBACK FROM PROJECTED RIGHTS OF WAY MAY UTILIZE THE ALTERNATIVE PARKING ANGLE, STALL, AND AISLE DIMENSIONS OUTLINED IN TABLE 21.11-9. AN ALTERNATIVE PARKING SITE PLAN SHALL BE SUBMITTED AND APPROVED AS PART OF THE APPLICABLE LAND USE PERMIT PROCESS.

B. PERMANENT PARKING NOT LOCATED WITHIN A SETBACK FROM PROJECTED RIGHTS OF WAY MAY UTILIZE THE COMPACT PARKING ANGLE, STALL, AND AISLE DIMENSIONS OUTLINED IN TABLE 21.11-10, PROVIDED THE PARKING AREA IS USED EXCLUSIVELY FOR EMPLOYEE PARKING FOR PERIODS IN EXCESS OF FOUR CONSECUTIVE HOURS, AND NO MORE THAN 30 PERCENT OF THE TOTAL NUMBER OF SPACES IS DESIGNED FOR COMPACT CARS. AN ALTERNATIVE PARKING SITE PLAN SHALL BE SUBMITTED AND APPROVED AS PART OF THE APPLICABLE LAND USE PERMIT PROCESS.]

*(Note to code revisor: Delete the following tables 21.11-9 and 21.11-10.)*

[TABLE 21.11-9: ALTERNATIVE PARKING ANGLE, STALL, AND AISLE DIMENSIONS]						
[PARKING ANGLE (DEGREES)]	[SPACE WIDTH]	[SPACE DEPTH (VEHICLE PROJECTION)]	[AISLE WIDTH 1-WAY]	[TYPICAL PARKING BAY WIDTH (MODULE)]	[INTERLOCK REDUCTION]	[OVERHANG ALLOWANCE]
[45]	[8' 4"]	[17' 4"]	[12' 3"]	[46' 11"]	[2' 0"]	[2' 0"]
[50]	[8' 4"]	[18' 0"]	[12' 9"]	[48' 9"]	[1' 10"]	[2' 1"]
[60]	[8' 4"]	[18' 10"]	[14' 3"]	[51' 11"]	[1' 4"]	[2' 3"]
[70]	[8' 4"]	[19' 2"]	[16' 1"]	[54' 5"]	[0' 10"]	[2' 5"]
[75]	[8' 4"]	[19' 0"]	[17' 6"]	[55' 6"]	[0' 8"]	[2' 6"]
[90]	[8' 4"]	[18' 0"]	[22' 6"]	[58' 6"]	[N/A]	[2' 8"]

[\*THE 90-DEGREE PARKING ANGLE DIMENSIONS ASSUME A TWO-WAY TRAFFIC FLOW.]



[TABLE 21.11-10: COMPACT PARKING ANGLE, STALL, AND AISLE DIMENSIONS]

[PARKING ANGLE (DEGREES)]	[SPACE TYPE]	[SPACE WIDTH]	[SPACE DEPTH (VEHICLE PROJECTION)]	[AISLE WIDTH 1-WAY]	[TYPICAL PARKING BAY WIDTH (MODULE)]	[INTERLOCK REDUCTION]	[OVERHANG ALLOWANCE]
[45]	[CO]	[7' 7"]	[15' 2"]	[10' 9"]	[41' 1"]	[1' 6"]	[1' 6"]
	[STD]	[8' 4"]	[18' 4"]	[13' 0"]	[49' 8"]	[2' 0"]	[2' 3"]
[50]	[CO]	[7' 7"]	[15' 8"]	[11' 2"]	[42' 6"]	[1' 4"]	[1' 7"]
	[STD]	[8' 4"]	[19' 2"]	[13' 6"]	[51' 0"]	[2' 0"]	[2' 4"]
[60]	[CO]	[7' 7"]	[16' 4"]	[12' 6"]	[45' 2"]	[1' 0"]	[1' 8"]
	[STD]	[8' 4"]	[20' 0"]	[15' 0"]	[55' 0"]	[1' 6"]	[2' 6"]
[70]	[CO]	[7' 7"]	[16' 5"]	[14' 1"]	[46' 11"]	[0' 8"]	[1' 10"]
	[STD]	[8' 4"]	[20' 4"]	[17' 0"]	[57' 8"]	[1' 0"]	[2' 8"]
[75]	[CO]	[7' 7"]	[16' 6"]	[16' 4"]	[49' 11"]	[0' 6"]	[1' 10"]
	[STD]	[8' 4"]	[20' 2"]	[18' 0"]	[58' 4"]	[0' 9"]	[2' 9"]
[90*]	[CO]	[7' 7"]	[15' 6"]	[19' 0"]	[50' 0"]	[N/A]	[2' 0"]
	[STD]	[8' 4"]	[19' 0"]	[23' 0"]	[61' 0"]	[N/A]	[3' 0"]

[CO: COMPACT CAR.] [STD: STANDARD CAR.]

[\*THE 90-DEGREE PARKING ANGLE DIMENSIONS ASSUME A TWO-WAY TRAFFIC FLOW.]

\*\*\* \*\*

(AO 2020-38, 4-28-20; AO 2020-93, 10-1-20)

**Section 4[5].** Anchorage Municipal Code Chapter 21.15, Rules of Construction and Definitions, is hereby amended to read as follows (*the remainder of the chapter is not affected and therefore not set out*):

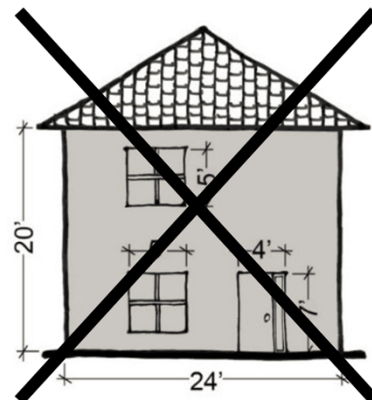
**21.15.020 Rules of Construction and Interpretation.**

\*\*\* \*\*

(Note to code revisor: the new subsection 21.15.020P. added by the original AO is deleted by this S-version)

**[P.] Measurement of Windows and Entrances as a Percentage of Building Wall Area. The percentage of building elevation wall area that is window and entrance openings is determined by dividing the total square footage of the windows and primary entrances on the building elevation by the gross square footage of the building elevation wall area (except for parts of the building exempted in 3.), and then multiplying the resulting quotient by 100.**

**1. Illustrated Example. In the illustration, the building elevation wall area is 20 feet of height x 24 feet of length = 480**



1 square feet. The window and primary entrance area is the  
2 area of the two windows (5 x 5 feet x 2) plus the area of  
3 the primary entrance and its sidelight (4 x 7 feet) = 78  
4 square feet. The percentage of the building elevation wall  
5 area is found by dividing 78 by 480 then multiplying by  
6 100, to yield 16%.

7  
8 2. *Measuring Window and Primary Entrance Area.* “Window  
9 area” is defined in section 21.15.040, Definitions. Primary  
10 entrance area includes the entrance opening and door  
11 frame but excludes trim, molding, and other features  
12 beyond the door frame. “Primary entrance” is defined in  
13 21.15.040, Definitions.

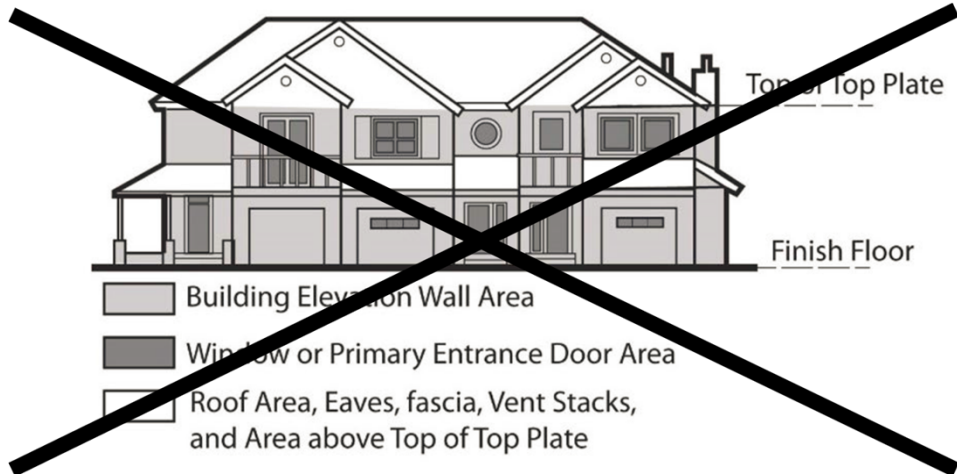
14  
15 3. *Exempt Parts of the Building Elevation Wall Area.* The  
16 building elevation wall area is measured as the exterior  
17 wall plane surface area from finished grade to the top of  
18 the wall on the building except that, for the purposes of  
19 measuring windows and entrances as a percentage of  
20 building wall area, the following parts of the building are  
21 not included in the wall area calculation:

22  
23 a. *Wall area above the topmost story’s top plate, such*  
24 as gable ends of a roof enclosing a cold attic, or  
25 parapet walls.

26 b. *Roof surfaces, eaves, fascia, vent stacks,*  
27 mechanical equipment and rooms, rooftop access  
28 enclosures, and other roof appurtenances.

29  
30 c. *Wall area of stories below grade plane, such as*  
31 below grade parking, up to the finished floor of the  
32 story above grade plane.

33  
34 d. *Parts of a street-facing building elevation below*  
35 the grade plane of the abutting sidewalk (or edge  
36 of street pavement where there is no sidewalk).]  
37



\*\*\*      \*\*\*      \*\*\*  
 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13)  
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**21.15.040 Definitions.**

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**Car Share Program**

A membership program that offers an alternative to car ownership under which persons or entities that become members are permitted to use vehicles from a fleet on an hourly basis, or in smaller intervals. Vehicles are available for pick-up by eligible members 24 hours per day. Automobile insurance and maintenance are covered by the car share program and individual members pay for usage and mileage fees.

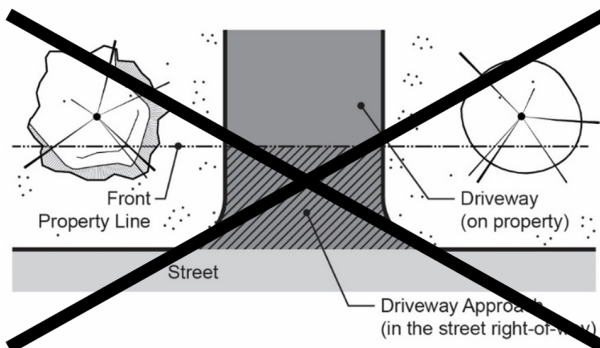
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**[Complete Street See Street, Complete.]**

\*\*\*      \*\*\*      \*\*\*

**[Driveway Approach**

**The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The driveway approach extends from the street edge to the front property line.]**



\*\*\*      \*\*\*      \*\*\*

**Parking Cash-Out**

A program offered by an employer that provides employees with a choice of foregoing any subsidized/free parking for a cash payment equivalent to

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25  
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1 the cost of the parking space to the employer[ALLOWING COMMUTERS  
2 TO CHOOSE CASH INSTEAD OF A FREE PARKING SPACE. FOR  
3 EXAMPLE, A COMMUTER WHO IS OFFERED A PARKING CASH-OUT  
4 MIGHT BE ABLE TO CHOOSE TO RECEIVE \$50 PER MONTH IN CASH  
5 IF THEY USE AN ALTERNATIVE MODE BESIDES A SINGLE-  
6 OCCUPANCY VEHICLE].

7  
8 [PARKING DISTRICT, MUNICIPALLY RECOGNIZED]  
9 [AN AREA RECOGNIZED OR DESIGNATED BY THE MUNICIPAL  
10 ASSEMBLY IN WHICH A CENTRAL AUTHORITY SUCH AS AN AREA  
11 IMPROVEMENT DISTRICT OR A PARKING AUTHORITY SUPPLIES  
12 ONE OR MORE CENTRALIZED PARKING FACILITIES, MANAGES THE  
13 PARKING SUPPLY AND/OR IMPLEMENTS OTHER PARKING  
14 MANAGEMENT STRATEGIES TO SERVE THE AREA, RATHER THAN  
15 EACH INDIVIDUAL DEVELOPMENT SITE HAVING TO SUPPLY ALL OF  
16 ITS OWN PARKING.]

17 \*\*\* \*\*

### 18 **Parking, Unbundled**

19 Parking spaces that are leased or sold separately from the rental or  
20 purchase fees for the dwelling units or building floor area rather than  
21 included in the rental or purchase fees, such that potential tenants or buyers  
22 have the option of renting or buying a dwelling unit or building floor area at  
23 a lower price with fewer or no parking spaces. The rental or purchase of a  
24 parking space is at the discretion and direct cost of the tenant or buyer  
25 [PARKING RENTED AND SOLD SEPARATELY FROM BUILDING  
26 SPACE, SO OCCUPANTS ONLY PAY FOR THE AMOUNT OF PARKING  
27 THEY WANT].

28 \*\*\* \*\*

### 29 **Pedestrian Feature**

30 A permanent object that provides pedestrians with increased convenience,  
31 comfort, and utility, and which is publicly accessible and not limited to a  
32 tenant or establishment such as seating for a restaurant. Pedestrian  
33 features include:

- 34 • Seating such as benches accommodating several people;
- 35 • Secondary/informal seating opportunities such as steps, pedestals,  
36 low walls, or edges of fountains, accommodating several people;
- 37 • A space for standing with objects to lean against, such as bollards,  
38 short fences, or irregular building facades, accommodating several  
39 people;
- 40 • A tree or raised planter;
- 41 • A work of art such as a water feature, sculpture, cultural exhibit, or  
42 clock feature;

43 [~~• Pedestrian-scale lighting;~~]

44 [~~• A cross-country ski rack that can accommodate a ski lock and  
45 is located at building entrances within 500 feet of a ski  
46 trailhead;~~]

- 47 • A winter city [~~urban design~~] feature such as a wind screen, or  
48 outdoor stove or space heater; or

- Other object supporting pedestrian utility, such as a gazebo or kiosk.

### **Pedestrian Movement Zone**

The middle portion of an enhanced **[street]** sidewalk **[(21.07.060G.19.), comprising its walkway clear width]** located between the sidewalk's street interface and building interface zones. The pedestrian **[movement]** zone provides for the primary function of sidewalks<sub>1</sub>[,] and is kept clear of any obstructions to pedestrian movement.

\*\*\*      \*\*\*      \*\*\*

### **Ride Hailing Services**

A business that matches passengers with for-hire personal vehicles with drivers using software applications. Ride hailing services are different from other for-hire vehicles with drivers, such as taxicabs, in that they are not hailed from the street.

\*\*\*      \*\*\*      \*\*\*

[PRIMARY PEDESTRIAN WALKWAY

A WALKWAY MEETING THE STANDARDS OF SUBSECTION 21.07.060F.4.]

\*\*\*      \*\*\*      \*\*\*

### **[Street, Complete**

**A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities, and for all users to safely move through the transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available on the municipal website.]**

\*\*\*      \*\*\*      \*\*\*

### **Walkway Clear Width**

That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed<sub>1</sub>**[(, continuous (i.e., without interruption or being split by obstructions),]** and kept clear for pedestrian movement.

\*\*\*      \*\*\*      \*\*\*

### **Walkway, Primary Pedestrian**

**See Primary Pedestrian Walkway.**

[WALKWAY, PRIMARY PEDESTRIAN

SEE PRIMARY PEDESTRIAN WALKWAY.]

\*\*\*      \*\*\*      \*\*\*

(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-132, 11-5-14; AO 2015-82, 7-28-15; AO 2015-100, 10-13-15; AO 2015-138, 1-12-16; AO 2015-133(S), 2-23-16; AO 2015-142(S-1), 6-21-16; AO 2016-3(S), 2-23-16; AO 2016-144(S), 12-20-16; AO 2017-55, 4-11-17; AO 2017-75, 5-9-2017; AO 2018-12, 2-27-18; AO 2018-67(S-1), 10-9-18; AO 2018-92, 10-23-18; AO 2019-132, 12-2-19; AO 2020-38, 4-28-20)

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**Section 5[6].** Anchorage Municipal Code of Regulations Section 21.20.007, Schedule of Fees - Miscellaneous Fees, is hereby amended to read as follows (*the remainder of the chapter is not affected and therefore not set out*):

**21.20.007 Schedule of Fees – Miscellaneous Fees**

The following fees shall be paid for the services described:

\*\*\* \*\*

*** **		
H.	New and revised parking layouts:	
	1. Site plan review only	\$415.00
	2. Complete site plan review:	
	a. 1[0]-10 spaces	\$415.00 plus \$16.50 per space
	b. 11—50 spaces	\$515.00 plus \$12.50 per space
	c. 51—200 spaces	\$620.00 plus \$8.25 per space
	d. 201—1,000 spaces	\$1,035.00 plus \$2.05 per space
	e. 1,001 + spaces	\$1,240.00 plus \$1.05 per space
I.	<u>Agreements with the Municipality</u>	
	Parking <u>or</u> [AND] access agreement[S]	\$235.00 per agreement
	<b><u>[Discretionary parking reductions review by Traffic Engineer (This fee does not apply to non-discretionary parking reductions listed in table 21.07-9.)]</u></b>	<b><u>[\$135.00/hour (two-hour minimum fee)]</u></b>
<b>J[H]</b> [I].	Sign permits:	
*** **		

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(GAAB 21.05.090; AO No. 77-407; AR No. 78-12; AR No. 79-55; AO No. 82-49; AR No. 83-96; AR No. 83-289(S); AR No. 86-63; AR No. 86-263; AR No. 87-315; AO No. 87-154(S); AR No. 90-151; AR No. 93-327(S), § 1, 2-22-94; AO No. 2001-145(S-1), § 23, 12-11-01; AO No. 2003-97, § 3, 9-30-03; AO No. 2003-152S, § 25, 1-1-04; AO No. 2004-1, § 4, 1-1-03; AR No. 2006-137, § 1, 6-6-06; AR No. 2008-134, § 4, 7-29-08; AO No. 2013-100, § 16, 1-1-14; AO No. 2018-100(S) , § 15, 1-1-19; AO No. 2019-116(S), § 11, 1-1-20)

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Authority—Anchorage Municipal Code 3.40, 21.20.050; AO No. 77-407.

**Section 6[7].** This ordinance shall become effective 60 calendar days after passage and approval by the Assembly.

PASSED AND APPROVED by the Anchorage Assembly this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Chair

ATTEST:

\_\_\_\_\_  
Municipal Clerk

(Planning and Zoning Commission Case No. 2022-0026)