

**Draft Assembly Ordinance**

***Title 21 Text Amendment to Off-Street Parking and Site  
Access Regulations***

**Public Hearing Draft**

**PZC Case No. 2022-0026**

***Anchorage 2040 Land Use Plan  
Implementation Actions 4-3 and 4-6***

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Submitted by: Chair of the Assembly at  
the Request of the Mayor  
Prepared by: Planning Department  
For reading: \_\_\_\_\_

**ANCHORAGE, ALASKA  
AO NO. 2022-\_\_\_**

1 **AN ORDINANCE OF THE ANCHORAGE MUNICIPAL ASSEMBLY AMENDING**  
2 **ANCHORAGE MUNICIPAL CODE TITLE 21 CHAPTERS 21.04, ZONING**  
3 **DISTRICTS; 21.07, DEVELOPMENT AND DESIGN STANDARDS; 21.11,**  
4 **DOWNTOWN; 21.15, RULES OF CONSTRUCTION AND DEFINITIONS, AND**  
5 **AMENDING ANCHORAGE MUNICIPAL CODE OF REGULATIONS SECTION**  
6 **21.20.007, SCHEDULE OF FEES, IN ORDER TO AMEND THE OFF-STREET**  
7 **PARKING AND SITE ACCESS DEVELOPMENT STANDARDS AND ACHIEVE**  
8 **COMPREHENSIVE PLAN GOALS FOR INFILL AND REDEVELOPMENT,**  
9 **HOUSING, AND ACCESSIBLE LAND USE.**

10  
11 (Planning and Zoning Commission Case No. 2022-0026)  
12

13 **WHEREAS**, the *Anchorage 2040 Land Use Plan (2040 Plan)* assesses the  
14 housing and employment needs of current and future Anchorage residents and  
15 includes goals, policies, and actions to address these needs; and  
16

17 **WHEREAS**, Goal 3 of the *2040 Plan* establishes Anchorage's commercial centers  
18 and corridors as the places to accommodate new business growth and mixed-use  
19 housing opportunities through infill and redevelopment in a more efficient, walkable  
20 development pattern; and  
21

22 **WHEREAS**, Goal 4 of the *2040 Plan* establishes Anchorage's neighborhoods as  
23 the places to provide a range of additional housing opportunities, meeting the  
24 housing needs of residents of all income levels and household types; and  
25

26 **WHEREAS**, off-street parking and driveway access standards are often the  
27 costliest and most land-consuming zoning requirements for multi-unit housing,  
28 mixed-use, and business developments; and  
29

30 **WHEREAS**, minimum parking requirements, where they exceed forecast parking  
31 utilization, are a barrier to the development of a range of housing types and  
32 businesses; and  
33

34 **WHEREAS**, wider than necessary site access driveway and vehicle circulation  
35 aisle requirements combined with inadequate treatment of pedestrian and bicyclist  
36 site access needs also impact the cost and range of housing types and business  
37 developments, and choices for travel mode; and  
38  
39

1 **WHEREAS**, Action 4-3 of the *2040 Plan* calls for amendments to Title 21 to allow  
2 parking reductions by right for residential uses and to offer greater parking  
3 reductions in key development areas; and  
4

5 **WHEREAS**, Action 4-6 of the *2040 Plan* calls for amendments to Title 21 for  
6 internal site circulation for vehicles and private lanes for compact infill housing; and  
7

8 **WHEREAS**, the *2040 Plan* as well as neighborhood and district plans recognize  
9 traditional urban neighborhoods and transit-supportive development corridors in  
10 certain parts of the Anchorage Bowl as having stronger street grid patterns, greater  
11 access to public transit and sidewalks, lower rates of car ownership and parking  
12 utilization, and shorter distances between trip destinations, which merit alternative  
13 parking, driveway, and form-based regulations tailored to their urban contexts; and  
14

15 **WHEREAS**, the *Metropolitan Area Transportation Solutions (AMATS) 2040*  
16 *Metropolitan Transportation Plan* sets forth policies and actions under Goal 3 to  
17 develop an efficient multi-modal transportation system to reduce congestion,  
18 promote accessibility, and improve system reliability, including Action 3E-5 to  
19 pursue parking strategies that support multi-modal transportation options; and  
20

21 **WHEREAS**, the *AMATS Non-Motorized Plan (2022)* calls for short-term and long-  
22 term bicycle parking spaces and provides recommendations for their design and  
23 location; and  
24

25 **WHEREAS**, neighborhood and district plans support amending Title 21 parking  
26 and site access regulations, including but not limited to the *Spenard Corridor Plan*  
27 Policies 5.28, 5.29, 5.30, and 5.32 for flexible parking requirements, compact  
28 parking design, parking management solutions, and shared parking to create  
29 efficiencies to further redevelopment and investment goals; and the *Fairview*  
30 *Neighborhood Plan* Strategy 3.2 to reduce required on-site parking; and  
31

32 **WHEREAS**, amending Title 21 to streamline approvals for administrative parking  
33 reductions from the minimum number of required spaces and expand the menu of  
34 available parking reduction strategies, will provide more flexibility, reduce costs,  
35 and facilitate developments with efficient, compact site plans that balance site  
36 access for motorists, pedestrians, and bicyclists in context with the surrounding  
37 neighborhood; and  
38

39 **WHEREAS**, amending Title 21 to replace five existing area-specific administrative  
40 parking reductions with by-right lower minimum parking requirements in urban  
41 neighborhood contexts near Downtown and transit-supportive development  
42 corridors will allow for more efficient land use, more flexibility to meet market  
43 demand, and more choice for parking management strategies in these key areas;  
44 and  
45

1 **WHEREAS**, amending multifamily residential driveway access and circulation  
2 requirements will facilitate development within urban contexts that is true to  
3 neighborhood character and desired outcomes; and  
4

5 **WHEREAS**, the Planning Department received and incorporated public comments  
6 on the policy direction before writing the proposed Title 21 text amendments,  
7 received and incorporated comments on the drafts of the proposed Title 21  
8 amendments, maintained a webpage covering the proposed Title 21 amendments  
9 that included an online questionnaire, and project information and presentations;  
10 and  
11

12 **WHEREAS**, the Planning Department received and incorporated feedback from  
13 dozens of consultations and public meetings with the general public, private-sector  
14 development experts, design/architectural professionals, business organizations,  
15 neighborhoods, public agencies, and municipal committees and commissions;  
16 now, therefore,  
17

18 **THE ANCHORAGE ASSEMBLY ORDAINS:**  
19

20 **Section 1.** Anchorage Municipal Code Chapter 21.04, Zoning Districts, is  
21 hereby amended to read as follows (*the remainder of the chapter is not affected*  
22 *and therefore not set out*):  
23

24 **21.04.020 Residential Districts**

25 \*\*\* \*\*

\*\*\*

26 H. *R-3A: Residential Mixed-Use District.*

27 \*\*\* \*\*

\*\*\*

28 2. District-Specific Standards

29 \*\*\* \*\* \*\*

30 [E. *REDUCED PARKING RATIOS.* DEVELOPMENT IN THE R-  
31 3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
32 THE MINIMUM NUMBER OF PARKING SPACES, AS  
33 PROVIDED IN SUBSECTION 21.07.090F.6.]  
34

35 e[F]. *Enhanced street sidewalk* [OPTION]. An enhanced *street*  
36 sidewalk [ENVIRONMENT] may be provided in lieu of  
37 required sidewalks and side perimeter landscaping, as  
38 provided in 21.07.060G.19[F.17].  
39

40 f[G]. *Building height increases.*

41 \*\*\* \*\* \*\*

42 g[H]. *Neighborhood protections.*

43 \*\*\* \*\* \*\*

44 J. *R-4A: Residential Mixed-Use District.*

45 \*\*\* \*\* \*\*

46 2. District-Specific Standards

1                   \*\*\*       \*\*\*       \*\*\*

2           [D.    **REDUCED PARKING RATIOS.** DEVELOPMENT IN THE R-  
3           4A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
4           THE MINIMUM NUMBER OF PARKING SPACES, AS  
5           PROVIDED IN SUBSECTION 21.07.090F.6.]

6                   \*\*\*       \*\*\*       \*\*\*

7           (AO 2012-124(S), 2-26-13; AO No. 2015-100, § 1, 10-13-15; AO No. 2017-  
8           176, § 3, 1-9-18; AO No. 2019-58, § 2, 5-7-19)

9  
10   **21.04.030   Commercial districts.**

11   \*\*\*       \*\*\*       \*\*\*

12   G.    *Standards for Mixed-Use Development in the B-1A and B-1B*  
13    *Districts.*

14           \*\*\*       \*\*\*       \*\*\*

15           [5.    **REDUCED PARKING RATIOS.** THE DEVELOPMENT IS  
16           ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT  
17           OF THE MINIMUM NUMBER OF PARKING SPACES, AS  
18           PROVIDED IN SUBSECTION 21.07.090F.6.]

19  
20           5[6]. *Enhanced street sidewalk* [OPTION]. An enhanced *street*  
21           sidewalk [ENVIRONMENT] may be provided in lieu of  
22           required sidewalks and side perimeter landscaping, as  
23           provided in 21.07.060G.19[F.17].

24  
25           [7.    **BUILDING PLACEMENT AND ORIENTATION.** BUILDINGS  
26           SHOULD BE PLACED AND ORIENTED TO THE STREET,  
27           LINING SIDEWALKS AND PUBLIC SPACES WITH  
28           FREQUENT SHOPS, ENTRANCES, WINDOWS WITH  
29           INTERIOR VIEWS, AND ARTICULATED GROUND-LEVEL  
30           FACADES. THE FOLLOWING STANDARDS APPLY:

31  
32           A.    BUILDINGS SHALL HAVE VISUAL ACCESS  
33           WINDOWS AND/OR PRIMARY ENTRANCES ON  
34           STREET-FACING BUILDING ELEVATIONS (UP TO  
35           A MAXIMUM OF TWO ELEVATIONS) FOR AT  
36           LEAST 15 PERCENT OF THE NON-RESIDENTIAL  
37           GROUND FLOOR WALL AREA. QUALIFYING  
38           WINDOWS SHALL BE NO MORE THAN FOUR FEET  
39           ABOVE FINISHED GRADE. WINDOWS SHALL  
40           COMPRISE AT LEAST TEN PERCENT OF THE  
41           WALL AREA OF THE UPPER FLOOR BUILDING  
42           ELEVATION (ABOVE THE GROUND FLOOR).  
43           EXTERIOR WALL AREAS OF BUILDING  
44           MECHANICAL ROOMS ARE EXEMPT. AN  
45           ELEVATION THAT IS MORE THAN 150 FEET AWAY  
46           FROM THE FACING STREET SHALL BE EXEMPT,

1 UNLESS IT IS THE ONLY APPLICABLE  
2 ELEVATION.

3  
4 B. EITHER:

5 I. PROVIDE AT LEAST ONE PRIMARY  
6 ENTRANCE WITHIN 60 FEET OF A STREET  
7 SIDEWALK, OR 90 FEET FOR BUILDINGS  
8 OVER 25,000 SQUARE FEET OF GROSS  
9 FLOOR AREA, AND CONNECTED TO THE  
10 STREET BY A CLEAR AND DIRECT  
11 WALKWAY; OR

12  
13 II. PROVIDE A PROMINENT AND INVITING  
14 PRIMARY ENTRANCE THAT IS VISIBLE  
15 FROM THE STREET, CONNECTED BY A  
16 DIRECT WALKWAY TO THE STREET, AND  
17 HIGHLIGHTED BY TWO OF THE  
18 FOLLOWING:

19  
20 (A) PORTICO, OVERHANG, CANOPY, OR  
21 SIMILAR PERMANENT FEATURE  
22 PROJECTING FROM THE WALL;

23 (B) RECESSED AND/OR PROJECTED  
24 ENTRANCE THAT COVERS AT LEAST  
25 80 SQUARE FEET;

26 (C) ARCHES, PEAKED ROOF FORMS,  
27 TERRACING PARAPETS, OR OTHER  
28 CHANGE OF BUILDING ROOFLINE;

29 (D) CHANGES IN SIDING MATERIAL, OR  
30 DETAIL FEATURES SUCH AS  
31 TILEWORK, TO SIGNIFY THE  
32 ENTRANCE; OR

33 (E) ENTRANCE PLAZA, PATIO, OR  
34 SIMILAR COMMON PRIVATE OPEN  
35 SPACE.

36  
37 C. BUILDINGS SHALL COMPLY WITH THE MAXIMUM  
38 SETBACKS ESTABLISHED IN SECTION 21.06.020,  
39 TABLES OF DIMENSIONAL STANDARDS, AND  
40 SUBSECTION 21.06.030C.5. THE MAXIMUM  
41 SETBACK MAY BE REDUCED OR ELIMINATED  
42 WITH THE CONCURRENCE OF THE DIRECTOR.]

43  
44 H. *Standards for Mixed-Use Development in the B-3 District.*  
45 \*\*\* \*\*

1 [3. *BUILDING PLACEMENT AND ORIENTATION.* BUILDINGS  
2 ARE SUBJECT TO THE BUILDING PLACEMENT AND  
3 ORIENTATION STANDARDS FOR MIXED-USE  
4 DEVELOPMENT IN THE B-1A/B-1B DISTRICTS IN  
5 SUBSECTION 21.04.030G.7. ABOVE.  
6

7 4. *REDUCED PARKING RATIOS.* THE DEVELOPMENT IS  
8 ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT  
9 OF THE MINIMUM NUMBER OF PARKING SPACES, AS  
10 PROVIDED IN SUBSECTION 21.07.090F.6.]  
11

12 3[5]. *Enhanced street sidewalk* [OPTION]. An enhanced *street*  
13 sidewalk [ENVIRONMENT] may be provided in lieu of  
14 required sidewalks and side perimeter landscaping, as  
15 provided in 21.07.060G.19[F.17].  
16

16 \*\*\* \*\*

17 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO No. 2017-58, § 1,  
18 4-11-17; AO No. 2019-11, § 2, 2-12-19)  
19

19 \*\*\* \*\*

20 **21.04.070 Overlay Zoning Districts.**

21 \*\*\* \*\*

22 **C. Commercial Center Overlay.**

23 \*\*\* \*\*

24 [4. *REDUCED PARKING RATIOS.* INSTEAD OF THE FIVE  
25 PERCENT REDUCTION PROVIDED IN SUBSECTIONS  
26 21.04.030G. AND H., ANY USE IS ELIGIBLE FOR A  
27 REDUCTION OF UP TO 10 PERCENT OF THE MINIMUM  
28 NUMBER OF PARKING SPACES, AS PROVIDED IN  
29 SUBSECTION 21.07.090F.6.]  
30

31 4[5]. *Uses prohibited.*

32 \*\*\* \*\*

33 5[6]. *Dimensional standards.*

34  
35 [A. THE MINIMUM FRONT SETBACK IS REDUCED TO  
36 FIVE FEET FOR BUILDING ELEVATIONS THAT  
37 COMPLY WITH 21.04.030G.7., BUILDING  
38 PLACEMENT AND ORIENTATION.]  
39

40 [B.] Uses in the B-1B district are exempt from the gross  
41 floor area limitations of subsection 21.04.030C.2.b.,  
42 provided that an individual use occupies no more than  
43 60,000 square feet of gross floor area.  
44

45 6[7]. *Building entrances.*

46 \*\*\* \*\*



1                    7[8].    *Sidewalks and walkways.*  
2                    \*\*\*            \*\*\*            \*\*\*

3                    (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13)

4  
5    **Section 2.**    Anchorage Municipal Code Chapter 21.07, Development and Design  
6 Standards, is hereby amended by adding a new Section 21.07.015, Neighborhood  
7 and Development Contexts, to read as follows (*the remainder of the chapter is not*  
8 *affected and therefore not set out*):  
9

10    **21.07**                    **Development and Design Standards**  
11    \*\*\*                    \*\*\*

12    **21.07.015**           **Neighborhood Development Context Areas.**  
13

14            A.    Purpose.    This section establishes Neighborhood Development  
15 Context Areas that reflect the existing and desired characteristics of  
16 Anchorage's older, urban neighborhoods as identified in the  
17 Comprehensive Plan. The Neighborhood Development Context  
18 Areas are distinguished from one another and suburban parts of the  
19 Anchorage Bowl by their physical and functional characteristics such  
20 as: street, alley, and block patterns; building placement; how  
21 buildings and structures relate to surrounding buildings and streets;  
22 the diversity, distribution, and intensity of land uses; and availability  
23 of transportation access alternatives such as pedestrian facilities and  
24 public transportation. The areas mapped in this section provide the  
25 basis for development regulations tailored to the unique  
26 characteristics and strengths of these neighborhoods and fulfill their  
27 role as recommended in the Comprehensive Plan. These areas are  
28 provided relief from some generally applicable development and  
29 design standards in this chapter 21.07 because of their unique  
30 characteristics.

31  
32            B.    Applicability  
33

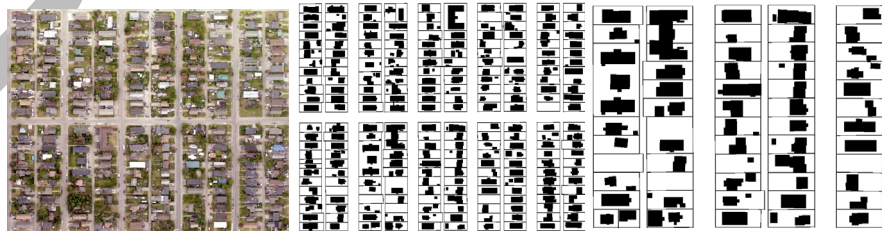
- 34                    1.    The neighborhood development context areas in this section  
35                    apply as a basis for area-specific development standards in  
36                    this chapter 21.07.  
37  
38                    2.    The neighborhood development context areas in this section  
39                    shall not be used as a basis to change the allowed uses or  
40                    review and approval procedures of the underlying zoning  
41                    district, district-specific dimensional standards, or any other  
42                    standards of this title besides the development and design  
43                    standards of chapter 21.07.  
44  
45                    3.    The department shall provide and maintain maps showing the  
46                    boundaries of the neighborhood development context areas

1 at the parcel scale, in this title and in a publicly accessible  
2 location.  
3

4 C. *Neighborhood Development Context Areas Established.* The  
5 following neighborhood development context areas are established  
6 and delineated in maps 21.07-1 through 21.07-4 in subsection  
7 21.07.015D. Each subsection below is organized to describe  
8 general character: street, block, and access patterns; building  
9 placement and location; and mix of uses. Descriptions are intent  
10 statements providing a basis for area-specific development  
11 standards elsewhere in this chapter 21.07.  
12

13 1. *Traditional Urban Neighborhood Context.*

14 The traditional urban neighborhood context includes  
15 Anchorage’s original urban neighborhoods, such as  
16 Government Hill, South Addition, Fairview, and Mountain  
17 View. These neighborhoods are identified by the  
18 Comprehensive Plan, Land Use Plan Map as Traditional  
19 Neighborhood Design areas. Properties within the Downtown  
20 (DT) zoning districts are not included because they are  
21 addressed in chapter 21.11. Traditional urban neighborhood  
22 contexts feature a regular street grid development pattern of  
23 city blocks surrounded by streets, which provide pedestrian  
24 and vehicular connections at frequent, regular intervals.  
25 Block and lot sizes are small and block shapes are consistent  
26 and often the same size. Sidewalks are present on most  
27 streets, except in limited parts of southern South Addition and  
28 in western Government Hill. There is a consistent presence  
29 of alleys. Buildings and residences typically have moderate  
30 front setbacks and orient to the street, with landscaping or  
31 pedestrian access comprising a majority of the frontage.  
32 Parking tends to be located to the side or rear of buildings.  
33 There is a balance of pedestrian, bicycle, public transit, and  
34 automobile reliance.



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2. *Edge Urban Neighborhood Context.*

The edge urban neighborhood context includes properties generally identified in the Comprehensive Plan, Land Use Plan Map, as traditional neighborhood design areas in Anchorage's early post-war era (1950s-1960s) neighborhoods, such as Spenard, North Star, Midtown, Rogers Park, Airport Heights, Russian Jack Park, and University Area. Edge urban neighborhoods feature a variety of interconnected street systems with smaller lot and block sizes than the more suburban parts of the Anchorage Bowl. Some edge urban areas feature a regular street grid development pattern of city blocks and others provide a more relaxed and irregular street grid. There is an inconsistent presence of alleys and local street sidewalks. Buildings typically have moderate to somewhat deeper front setbacks. Buildings tend to orient toward the public street, although some front setbacks are deep enough to allow for a mix of landscaping and parking. Some edge urban neighborhood context areas include transit-supportive development corridors where the Municipality has invested in elevated levels of public transportation service and enhanced pedestrian facilities.



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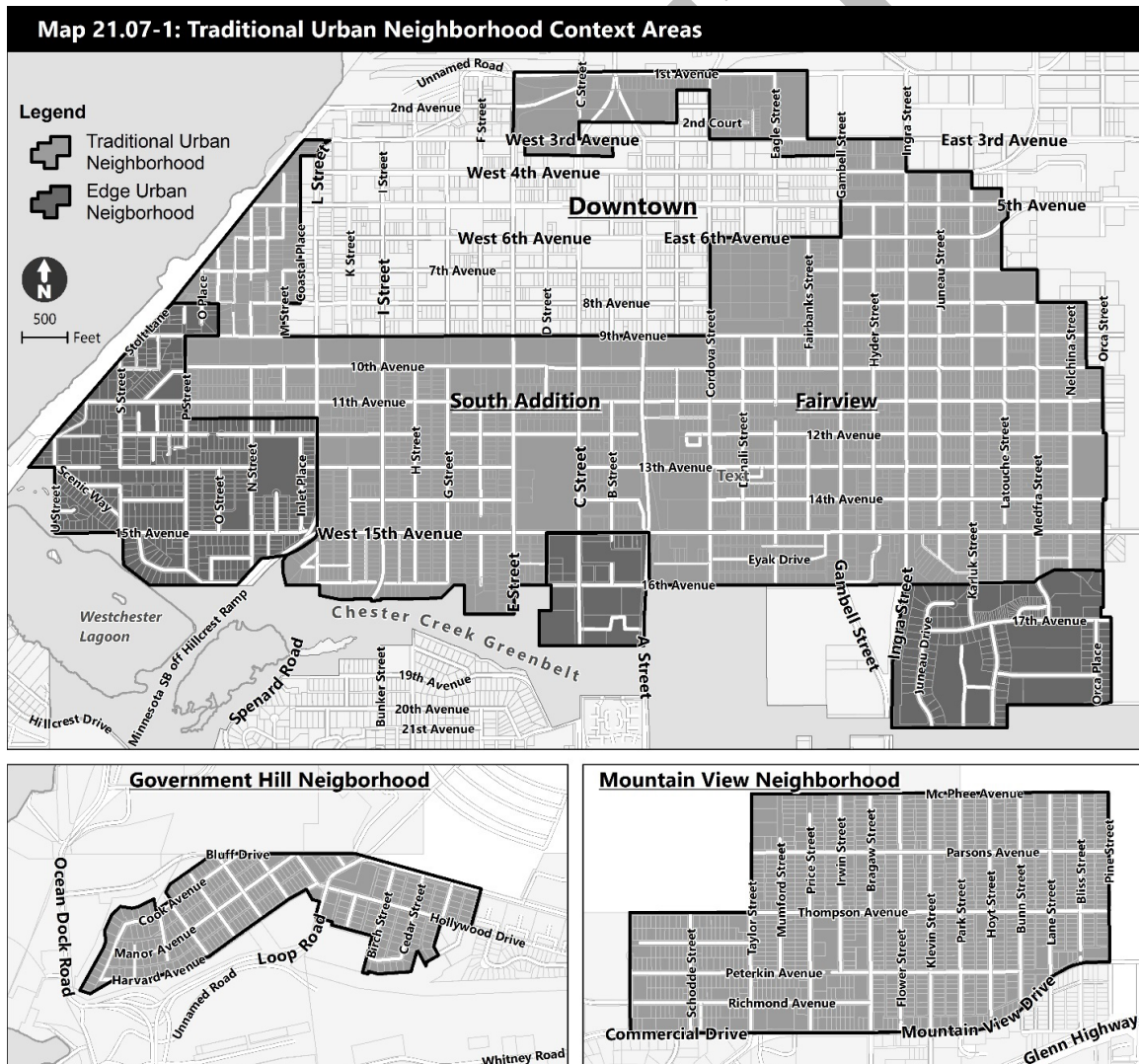
3. *Transit-Supportive Development Corridors.*  
Transit-supportive development corridors designated by the Comprehensive Plan extend through and outward from the edge urban neighborhood contexts. These corridors are where pedestrian facilities and frequent public transportation service interact with a compact, pedestrian-friendly pattern of commercial, residential, and mixed-use development within walking distance of the public transit route. These corridors have elevated levels of municipal public transportation service with frequent headways between buses and higher ridership demand. These transit routes connect local and regional town centers, city centers, and other service/employment centers, such as the UMED District. Transit-supportive development corridors are a half-mile wide, extending ¼-mile (approximately 5 to 10 minutes walking distance) on each side the public transit route.

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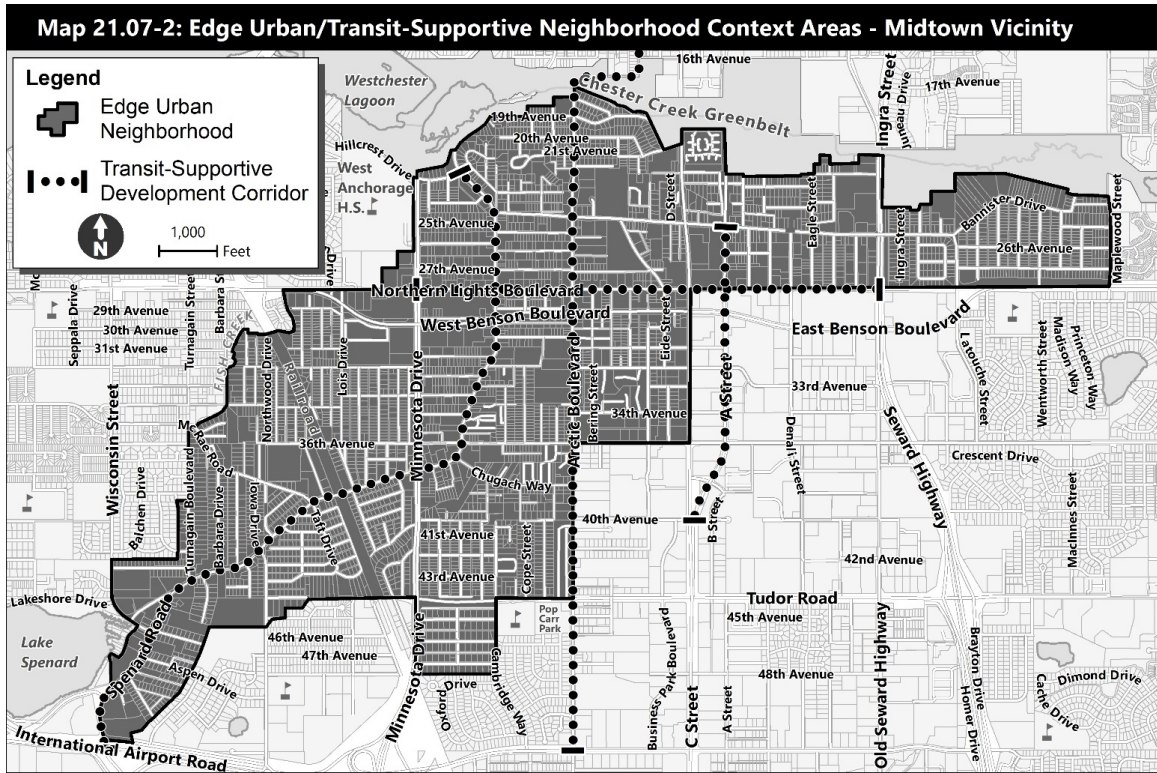
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D. Neighborhood Context Area Boundaries.

1. The boundaries of the neighborhood development context areas are established in maps 21.07-1 through 21.07-4.
2. The transit-supportive development corridors on maps 21.07-2, 21.07-3, and 21.07-4 shall include all properties wholly or partially within ¼ mile of the street right-of-way centerline of the linear transit corridor feature shown on the map, except that: where a transit-supportive development corridor overlaps with a traditional urban or edge urban neighborhood context area, the traditional urban or edge urban neighborhood regulations of this title shall govern.



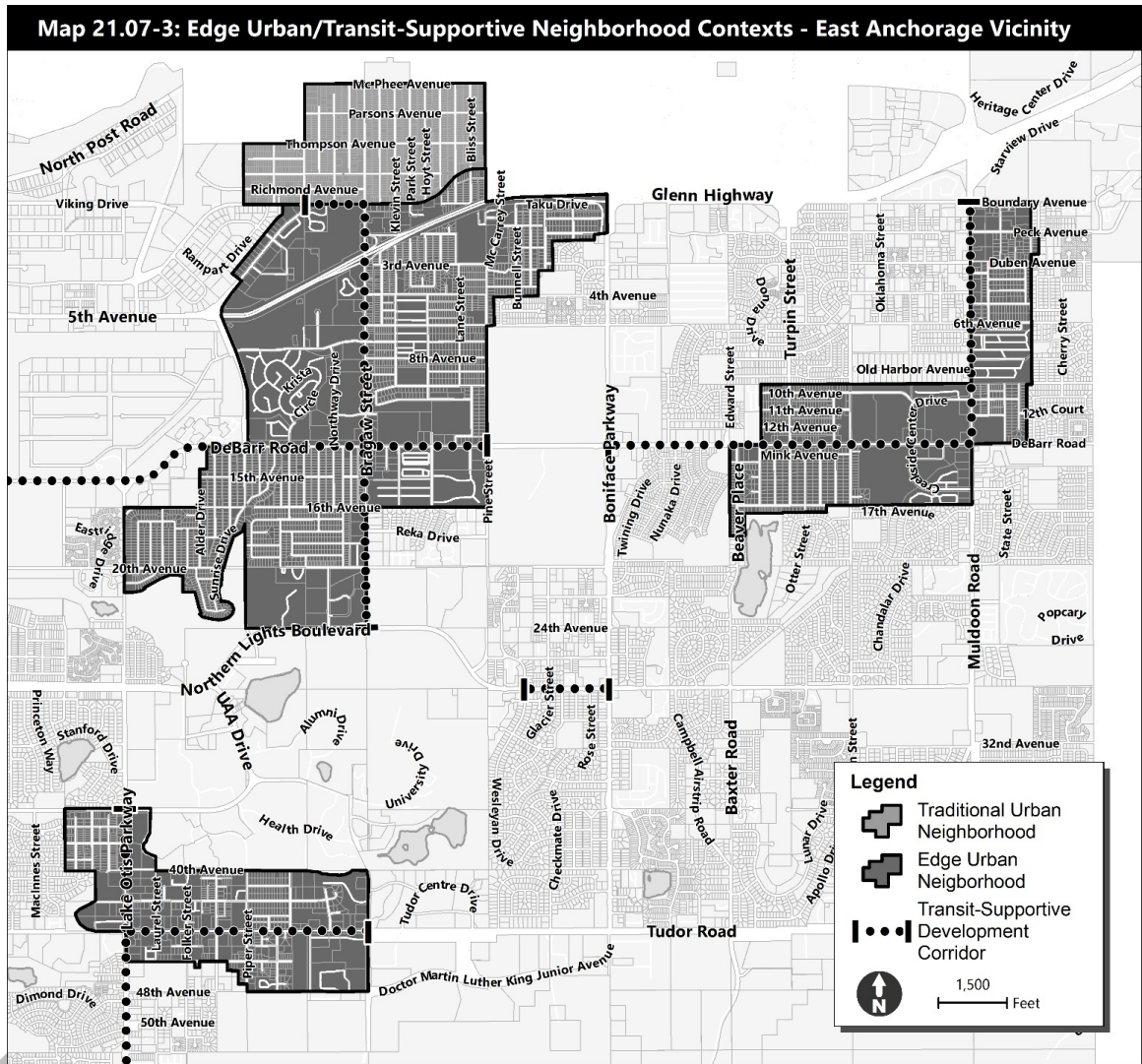
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1 **Section 3.** Anchorage Municipal Code Chapter 21.07, Development and Design  
2 Standards, is hereby amended to read as follows (*the remainder of the chapter is*  
3 *not affected and therefore not set out*):  
4

5 **21.07 Development and Design Standards**

6 \*\*\* \*\*

7 **21.07.060 Transportation and Connectivity**

8 \*\*\* \*\*

9 E. *Standards for Pedestrian Facilities.*

10 \*\*\* \*\*

11 2. *Sidewalks.*

12  
13 a. All sidewalks shall be designed to comply with the  
14 standards of the Design Criteria Manual (DCM) and  
15 Municipality of Anchorage Standard Specifications  
16 (MASS) and shall be improved in accordance with  
17 subsection 21.08.050H.  
18

19 b. Street improvement projects and new streets in  
20 subdivisions i[]n all class A zoning districts except for  
21 industrial districts[,] shall install sidewalks [SHALL BE  
22 INSTALLED ]on both sides of all streets (local,  
23 collector, arterial, public or private, including loop  
24 streets). Where indicated in the comprehensive plan, a  
25 pathway may replace a sidewalk on one side. Street  
26 improvement projects i[]n industrial zoning districts  
27 shall install[,] a sidewalk [SHALL BE INSTALLED ]on  
28 one side of all local streets, and on both sides of local  
29 streets if the new sidewalks would connect to existing  
30 sidewalks on both ends and the needed sidewalk  
31 length is no greater than one quarter mile.  
32 \*\*\* \*\*

33 f. Development on lots along existing streets in class A  
34 zoning districts shall install sidewalks on all lot  
35 frontages abutting streets in the following situations:  
36

37 i. In R-4, R-4A, commercial [DISTRICTS], and  
38 DT zoning districts.  
39 \*\*\* \*\*

40 4. *On-site pedestrian walkways.*

41 \*\*\* \*\*

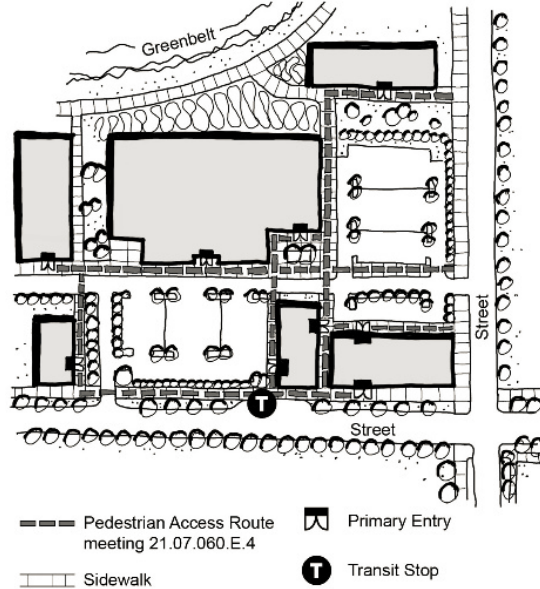
42 b. *On-site pedestrian connections.*

43 \*\*\* \*\*

44 ii. All primary building entrances on a site shall be  
45 connected to the street by a convenient system  
46 of walkways. This includes multiple primary

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entrances into one building, and primary entrances in separate buildings on a site.



(Note to code revisor: replace existing illustration with illustration above.)

\*\*\* \*\*

iv. The primary front entrance of a residential dwelling shall be connected to the street by a walkway as provided in i. through ii. above, or by the dwelling's individual driveway, or by a shared parking courtyard meeting 21.07.060G.20.

c. Walkway clear width and improvements.

i. Walkway clear width. The minimum width of a required pedestrian walkway shall be five feet of unobstructed clear width, [EXCLUDING VEHICULAR OVERHANG,] except where otherwise stated in this title. A walkway that provides access to no more than four residential dwelling units may provide an unobstructed clear width of three feet.

ii. Walkways shall be improved in accordance with subsection 21.08.050H.

\*\*\* \*\*

1            F.      *Pedestrian frontage standard.*

2  
3            1.      *Purpose.* The pedestrian frontage standard promotes building  
4            placement and orientation toward neighborhood streets and  
5            sidewalks, facilitates pedestrian and bicycle access, supports  
6            lower parking utilization in urban neighborhood areas, and  
7            encourages alternative modes of transportation. Objectives  
8            include:

9  
10           a.      Define public streets and their associated frontage  
11           spaces as positive, useful features around which to  
12           organize and orient buildings in a manner that  
13           promotes pedestrian activity and connects to multiple  
14           modes of transportation.

15  
16           b.      Provide pedestrian access to building entries that is  
17           clearly defined, safe, and inviting for people of all  
18           abilities, and minimizes conflicts between pedestrians  
19           and vehicle parking.

20  
21           c.      Place active living spaces, entrances, and windows on  
22           street-facing building facades to improve the physical  
23           and visual connection from residences to the street.

24  
25           d.      Promote a safe, secure pedestrian and neighborhood  
26           environment through “eyes on the street” design  
27           elements, such as placement of windows, that foster  
28           opportunities for casual surveillance of the street and  
29           outwardly expressed proprietorship of the  
30           neighborhood.

31  
32           2.      *Applicability.* Subsection 3. applies to development in the  
33           traditional urban and edge urban neighborhood context areas  
34           established in 21.07.015, and to developments that receive  
35           reductions to required parking in 21.07.090F. Subsection 4.  
36           applies to development in the other areas of the municipality.  
37           Exceptions and exemptions from this section 21.07.060F.  
38           include:

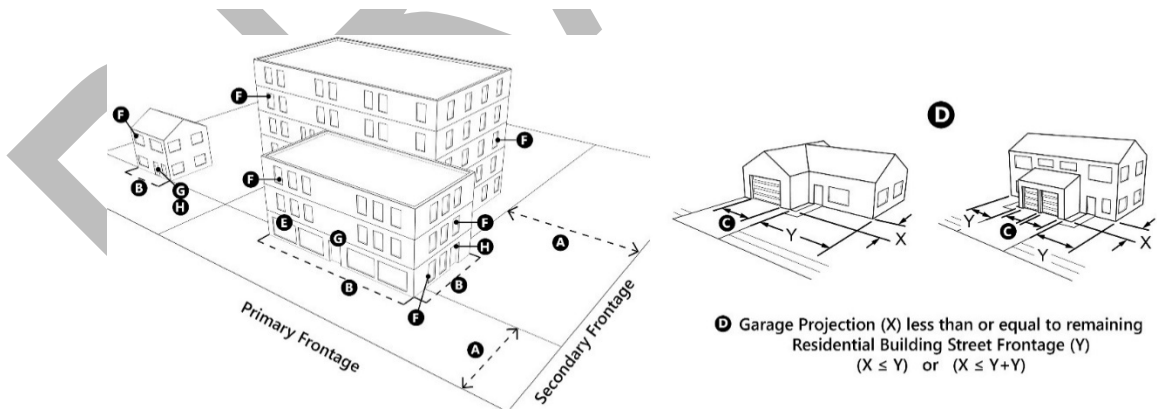
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40           a.      Changes of use and other developments that comprise  
41           building modifications of less than 50 percent of the  
42           total improvement value of the building(s) on the site.

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44           b.      Development in Girdwood, Downtown (DT), and  
45           Industrial zoning districts.

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- c. Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of 20,000 square feet or larger, or in Class B zoning districts.
- d. Non-residential uses without habitable floor area, such as utility substations.
- e. The director shall reduce the window area requirement by the amount the applicant demonstrates is needed to comply with a 5-star energy rating or an affordable housing (21.15.040) financing institution's energy rating requirements.
- f. The director shall approve administrative relief provided the applicant demonstrates the adjustment is necessary to compensate for some practical difficulty of the site.
- g. The Alternative Equivalent Compliance procedure set forth in subsection 21.07.010D. may be used to propose alternative means of complying with the intent of this section.

3. Standard for urban neighborhood contexts and parking reductions. The standards of table 21.07-2 apply to the primary frontage and at least one secondary frontage.



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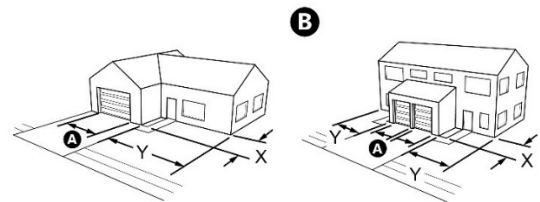
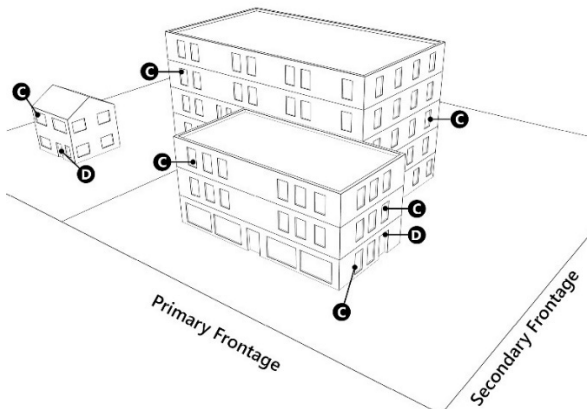
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**Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts and Parking Reductions**

<u>Building and Parking Placement</u> <sup>1</sup>	<u>Primary Frontage</u>	<u>Secondary Frontage</u>
<u>A. Maximum front setback (ground-floor only)</u> <sup>1,4</sup>	<u>30 feet in Traditional Urban, and 60 feet in other areas</u>	<u>90 feet</u>
<u>B. Minimum percentage of the width of ground-floor street-facing building elevation(s) required to comply with max. setback in A</u> <sup>1,4</sup>	<u>50%</u>	<u>50%</u>
<u>C. Residential garage entrance as a percentage of ground-floor street-facing building elevation width (maximum allowed)</u>	<u>50% in Traditional Urban, and 67% in other areas</u>	<u>67%</u>
<u>D. Maximum distance a garage may project out in front of the rest of the street-facing residential building elevation</u>	<u>No more than the width of the non-garage portion of the street-facing building elevation</u>	
<u>Building Orientation and Access</u>	<u>Primary Frontage</u>	<u>Secondary Frontage</u>
<u>E. Minimum percentage of a non-residential ground-floor, street-facing building elevation required to be windows or primary entrances</u> <sup>2,3</sup>	<u>30% in Traditional Urban, and 20% in other areas</u>	<u>15%</u>
<u>F. Minimum percentage of residential and upper-floor non-residential street-facing elevation required to be windows or primary entrances</u> <sup>3</sup>	<u>15% in Traditional Urban, and 10% in other areas</u>	<u>10%</u>
<u>G. At least one primary entrance located within max. setback in A</u> <sup>4</sup>	<u>Required</u>	<u>Not Required</u>
<u>H. Front primary entrances for residential dwellings meet 21.07.060G.15., Covered, Visible Residential Entrance</u>	<u>Required</u>	
<u>I. Minimum number of pedestrian amenities from 21.07.060G. in addition to 21.07.060G.15.</u>	<u>2 pedestrian amenities required per multifamily, mixed-use, townhouse, or group housing development</u>	
<sup>1</sup> Generally applicable standards and exceptions for maximum setbacks are provided in 21.06.030C.5. <sup>2</sup> Non-residential ground-floor windows used to meet these percentages shall be visual access windows (21.15.040) and have a sill height of no more than four feet above finished grade. Ground-floor wall area is defined in 21.15.040. <sup>3</sup> Rules of measurement for calculating window area as a percentage of building wall area are provided in 21.15.020P. <sup>4</sup> Development in the R-4A zoning district is exempt from the maximum setback related standards A, B, and G.		

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**4. Standard for other areas and developments in the municipality. The standards of table 21.07-3 apply to the primary frontage and at least one secondary frontage.**



**B** Garage Projection (X) less than or equal to remaining Residential Building Street Frontage (Y)  
 $(X \leq Y)$  or  $(X \leq Y+Y)$

<b>Table 21.07-3: Pedestrian Frontage Standard – Other Areas and Developments</b>		
<u>Building and Parking Placement</u>	<u>Primary Frontage</u>	<u>Secondary Frontage</u>
<u>A. Residential garage entrance as a percentage of ground-floor street-facing building elevation width (maximum allowed)</u>	67%	75%
<u>B. Maximum distance a garage may project out in front of the rest of a street-facing residential building elevation</u>	<u>No more than the width of the non-garage portion of the street-facing building elevation</u>	
<u>Building Orientation and Access</u>	<u>Primary Frontage</u>	<u>Secondary Frontage</u>
<u>C. Minimum percentage of residential street-facing building elevation wall areas required to be windows or primary entrances <sup>1</sup></u>	<u>Window(s) required, with no minimum percentage</u>	
<u>D. Front primary entrances to residential dwellings meet 21.07.060G.15., Covered, Visible Residential Entrance.</u>	<u>Required</u>	
<u>E. Minimum number of pedestrian amenities from 21.07.060G. in addition to 21.07.060G.15.</u>	<u>1 pedestrian amenity required per multifamily, mixed-use, townhouse, or group housing development</u>	
<sup>1</sup> Rules of measurement for calculating window area as a percentage of building wall area are provided in 21.15.020P.		

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G[F]. Pedestrian amenities menu.

1. *Purpose.* The purpose of this section is to define and provide standards for pedestrian amenities that may be required, [OR ]included in a menu of choices to meet a requirement, or listed as a special feature that can count toward a bonus incentive anywhere in this title. For example, another section of this title may list a pedestrian amenity as a special feature for which bonus floor area or a reduction in required parking may be granted. The standards contained in this section give predictability for applicants, decision-makers, and the community for the minimum acceptable standards for pedestrian amenities. It also ensures the amenities will improve and enhance the community to the benefit of all, and respond to the northern latitude climate. This title provides flexibility to encourage and allow for creativity and unique situations through the alternative equivalent compliance and minor modifications process.
2. *Applicability.* Pedestrian amenities shall meet the minimum standards of this section in order to be credited toward a requirement, menu choice, or [AS A SPECIAL FEATURE BONUS ]incentive of this title, except where specifically provided otherwise in this title. The alternative equivalent compliance procedure set forth in subsection 21.07.010D. may be used to propose alternative means of complying with the standards of this section 21.07.060G[F]. The standards of

1                    this section apply only to pedestrian amenities that are  
2                    counted toward a requirement, menu choice, or incentive  
3                    under this title. It does not apply to amenities that do not  
4                    receive credit for requirements in title 21.  
5

6                    [3. WALKWAY. A WALKWAY IS A SURFACE THAT  
7                    CONNECTS TWO POINTS FOR PEDESTRIAN USE, AS  
8                    DEFINED IN CHAPTER 21.15. A WALKWAY MAY BE IN A  
9                    PUBLICLY DEDICATED PEDESTRIAN EASEMENT.  
10                    EXAMPLES INCLUDE PEDESTRIAN CONNECTIONS  
11                    WITHIN ONE DEVELOPMENT SITE, MID-BLOCK,  
12                    BETWEEN SUBDIVISIONS, OR LEADING FROM STREETS  
13                    TO PUBLIC AMENITIES, SUCH AS SCHOOLS OR PARKS.]  
14

15                    A. A WALKWAY SHALL HAVE A MINIMUM  
16                    UNOBSTRUCTED CLEAR WIDTH OF FIVE FEET,  
17                    EXCEPT WHERE OTHERWISE STATED IN THIS  
18                    TITLE. A WALKWAY THAT PROVIDES ACCESS TO  
19                    NO MORE THAN FOUR RESIDENTIAL DWELLING  
20                    UNITS MAY HAVE AN UNOBSTRUCTED CLEAR  
21                    WIDTH OF THREE FEET.  
22

23                    B. WALKWAYS SHALL BE IMPROVED IN  
24                    ACCORDANCE WITH SUBSECTION 21.08.050H.]  
25

DRAFT

1  
2 3[4]. Enhanced on-site [PRIMARY PEDESTRIAN] walkway. An  
3 enhanced on-site[PRIMARY PEDESTRIAN] walkway is  
4 intended to provide an option for applicants to receive credit  
5 for exceeding the minimum development standards for  
6 walkways and improving pedestrian convenience, comfort,  
7 and safety on the site. Enhanced on-site[PRIMARY  
8 PEDESTRIAN] walkways provide additional width[AN  
9 UNOBSTRUCTED CLEAR WIDTH OF AT LEAST EIGHT  
10 FEET] for pedestrian movement [WITH ADDITIONAL SPACE  
11 INCORPORATING FEATURES ALONG THE WALKWAY  
12 SUCH AS STOREFRONT SIDEWALK SPACE, ROOM FOR  
13 RESIDENTIAL STOOPS OR BUILDING FOUNDATION  
14 PLANTINGS,] and peripheral space that accommodates  
15 landscaping, furniture, and utilities. [AS ESTABLISHED  
16 GENERALLY IN SUBSECTION F.1 AND F. 2 ABOVE, THE  
17 STANDARDS OF THIS SUBSECTION APPLY ONLY  
18 WHERE THE SPECIFIC TERM "PRIMARY PEDESTRIAN  
19 WALKWAY" IS LISTED AS A REQUIREMENT, MENU  
20 CHOICE, OR SPECIAL FEATURE THAT COUNTS  
21 TOWARD A BONUS. THIS SUBSECTION IS NOT A  
22 GENERALLY APPLICABLE REQUIREMENT FOR OTHER  
23 LARGE WALKWAYS.]

24  
25 a. [A PRIMARY PEDESTRIAN WALKWAY SHALL BE  
26 DEVELOPED AS A CONTINUOUS PEDESTRIAN  
27 ROUTE EXTENDING FOR AT LEAST 50 FEET.]

28  
29 [B.] An enhanced on-site [A PRIMARY PEDESTRIAN]  
30 walkway shall have a pedestrian movement zone with  
31 a continuous,[N] unobstructed walkway clear width of  
32 at least eight feet. Where adjacent to a ground-floor  
33 building elevation it shall also have a [SIDEWALK  
34 STOREFRONT OR] building interface zone a minimum  
35 of two feet in width for building foundation landscaping  
36 or [THREE FEET IN WIDTH OF SIDEWALK] space for  
37 opening doors or seating and transition pedestrian  
38 spaces. In addition to the pedestrian movement zone  
39 and any building interface zone, the enhanced on-site  
40 walkway shall have a buffer space of at least  
41 two[FOUR] feet in width where abutting motor vehicle  
42 parking lots, circulation aisles, or driveways. [SHALL  
43 BE INCORPORATED AS PART OF THE WALKWAY  
44 WHEN ABUTTING ANY STREET OR VEHICLE  
45 AREA,] The buffer space shall [TO] accommodate  
46 [STREET TREES,]landscaping beds, fencing or



1                    bollards, light poles, utilities, benches, and other  
2                    furnishings[OBJECTS TO BE KEPT CLEAR OF THE  
3                    WALKWAY].  
4

5                    b[C]. A minimum of two pedestrian features as defined by  
6                    this title (21.15.040) shall be provided along the  
7                    enhanced walkway, with at least one for every 50 feet  
8                    of the walkway length [AT LEAST ONE PEDESTRIAN  
9                    FEATURE AS DEFINED BY THIS TITLE SHALL BE  
10                    PROVIDED FOR EVERY 50 FEET OF LENGTH  
11                    ALONG A PRIMARY PEDESTRIAN WALKWAY].  
12

13                    [D. A PRIMARY PEDESTRIAN WALKWAY SHALL BE  
14                    ILLUMINATED WITH PEDESTRIAN SCALE  
15                    LIGHTING.]  
16

17                    c[E]. Enhanced on-site [A PRIMARY PEDESTRIAN]  
18                    walkways shall provide continuous, direct[LY]  
19                    connections from building primary entrance(s) to  
20                    surrounding public streets and sidewalks[,] and be  
21                    publicly accessible or available to all residents of the  
22                    development [AT ALL TIMES].  
23

24                    *(Note to code revisor: renumber subsections 5-13 accordingly.)*

25                    \*\*\*        \*\*\*        \*\*\*

26                    [15. BICYCLE PARKING FACILITIES.  
27

28                    A. REQUIRED BICYCLE PARKING OR A SIGN  
29                    LEADING THERETO SHALL BE LOCATED IN AN  
30                    AREA VISIBLE FROM A PRIMARY ENTRANCE  
31                    AREA AND NO FARTHER FROM A PRIMARY  
32                    ENTRANCE THAN THE CLOSEST MOTOR VEHICLE  
33                    PARKING SPACE, NOT INCLUDING DESIGNATED  
34                    ACCESSIBLE PARKING, CARPOOL, OR VANPOOL  
35                    SPACES. IT MAY ALSO BE LOCATED INSIDE THE  
36                    BUILDING SERVED, IN A LOCATION THAT IS  
37                    EASILY ACCESSIBLE FOR BICYCLES.  
38

39                    B. A REQUIRED BICYCLE PARKING SPACE SHALL  
40                    INCLUDE A SECURELY FIXED STRUCTURE THAT  
41                    ALLOWS THE BICYCLE WHEEL AND FRAME TO BE  
42                    LOCKED TO THE FACILITY, AND THAT SUPPORTS  
43                    THE BICYCLE FRAME IN A STABLE POSITION  
44                    WITHOUT DAMAGE TO THE BICYCLE, OR SHALL  
45                    BE IN A BICYCLE LOCKER, LOCKABLE BICYCLE  
46                    ENCLOSURE, OR LOCKABLE ROOM.

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- C. A REQUIRED BICYCLE PARKING SPACE SHALL BE A MINIMUM OF SIX FEET LONG AND TWO FEET WIDE.
- D. THE SURFACING OF BICYCLE PARKING FACILITIES SHALL BE DESIGNED AND MAINTAINED TO BE CLEAR OF MUD AND SNOW.
- E. BICYCLE PARKING SHALL NOT OBSTRUCT PEDESTRIAN WALKWAYS, BUILDING ACCESS, OR USE AREAS.]

14. Separated walkway to the street. The development shall connect the building primary entrances to the street with a clear and direct walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles. The minimum clear width portion of the walkway shall be separated from the parking facility by at least five feet.

15. Covered, visible residential entrance. A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings from the street. The entrance shall meet the following standards:

- a. The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.
- b. The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle parking by vertical separation or a change in surfacing material.
- c. The building entrance shall also be visible (via an unobstructed line of sight) from a street or face a common private open space (21.07.030) that is visible from a street.

16. Enhanced primary entrance. An enhanced building entry is intended to provide an option for applicants to receive credit for a prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary entrance shall incorporate at least three of the following features:

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- a. Outdoor sheltering roof feature projecting from the building façade such as an overhang, portico, canopy, marquee with an inside dimension of at least 16 square feet;
  - b. Recessed and/or projected entrance or other building wall modulation with projections or recesses in the building wall plane;
  - c. Changes in the building’s main roofline such as arches, peaked roof forms, or terracing parapets;
  - d. Changes in siding material or exterior finishes, or façade detail features such as tilework that emphasize the entrance;
  - e. Entrance plaza, patio, or similar common private open space;
  - f. Landscaping not otherwise required by this title, such as integrated planters, landscape accent lighting, or special paving treatments; or
  - g. One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.
17. Site Entry Feature. Highlight and define a pedestrian entrance to a development site using three or more of the following elements:
- a. Landscape treatment with seasonal color and trees, which clearly distinguishes and highlights the site entry.
  - b. Plaza or courtyard as described in subsection 21.07.060G.5.
  - c. Identifying building primary entrance form including a covered entry, when the primary entrance is within 50 feet of the site entrance.
  - d. Special paving, pedestrian scale lighting, and/or bollards.

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2 e. Ornamental gate and/or fence.  
3

4 18[16]. Pedestrian-interactive *building* [USE]. A pedestrian-  
5 interactive building [USE] is intended to provide interior  
6 [GROUND FLOOR] spaces that [STRONGLY] engage the  
7 sidewalk with street-facing windows and entrances, and  
8 [THAT FEATURE] activities and services that support  
9 neighborhood residents, and generally contribute to the  
10 activity level and quality of the pedestrian[-ORIENTED]  
11 environment of the neighborhood or district. The standards  
12 that follow apply where the term “pedestrian-interactive  
13 building[USE]” is listed in this title as a requirement, special  
14 feature for a bonus, or a menu choice.  
15

16 [A. A PEDESTRIAN-INTERACTIVE USE SHALL BE ANY  
17 OF THE FOLLOWING USES THAT ARE PERMITTED  
18 IN THE DISTRICT: RETAIL AND PET SERVICES;  
19 FINANCIAL INSTITUTION PROVIDING BANKING  
20 SERVICES OPEN TO THE PUBLIC WITH AT LEAST  
21 ONE EMPLOYEE ON SITE; FOOD OR BEVERAGE  
22 SERVICE; PERSONAL SERVICE; CULTURAL  
23 FACILITY; OR THE FRONTAGE OF ENTRYWAYS  
24 OR STAIRWAYS THROUGH WHICH SUCH USES  
25 ARE PRINCIPALLY ACCESSED;  
26

27 B. RETAIL SALES USES THAT ARE PERMITTED IN  
28 THE DISTRICT SHALL BE CONSIDERED  
29 PEDESTRIAN-INTERACTIVE USES, EXCEPT FOR  
30 THE FOLLOWING TYPES OF RETAIL SALES USES:  
31 FUELING STATION; BUILDING MATERIALS STORE.

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C. THE FOLLOWING USES SUPPORTING RESIDENTIAL NEIGHBORHOOD AND HOUSING DEVELOPMENT ARE ALSO CONSIDERED PEDESTRIAN-INTERACTIVE USES WHEN PERMITTED IN THE DISTRICT: RESIDENTIAL DWELLINGS WITH INDIVIDUAL FRONT ENTRIES ALONG THE STREET; ELEMENTARY SCHOOL; MIDDLE OR HIGH SCHOOL; HEALTH SERVICES; CHILD CARE CENTER.]

a[D]. A pedestrian-interactive building [USE] shall provide a primary entrance facing the street. Entrances at building corners facing a street may be used to satisfy this requirement.

b[E]. A pedestrian-interactive building [USE] shall contain habitable floor area at least 24 feet deep extending along a minimum of 50 percent of the [FULL] length of the ground-floor, street-facing building elevation in Urban Neighborhood Contexts, and 30 percent elsewhere [ALLOWING FOR,]. The habitable floor area may include pedestrian [AND VEHICLE] entrances, entry lobbies or atriums, and stairwells.

[F. A PEDESTRIAN-INTERACTIVE USE SHALL COMPLY WITH SUBSECTION 21.06.030C.5., MAXIMUM SETBACKS, BUT THE EXCEPTIONS OF SUBSECTION 21.06.030C.5.D. SHALL NOT BE AVAILABLE.]

c[G]. Street-facing [GROUND-FLOOR] wall areas of a non-residential [PEDESTRIAN-INTERACTIVE] use shall be 50[67] percent visual access windows or primary entrances on the ground floor, and 20 percent windows above the ground floor. [, EXCEPT THAT SUCH]

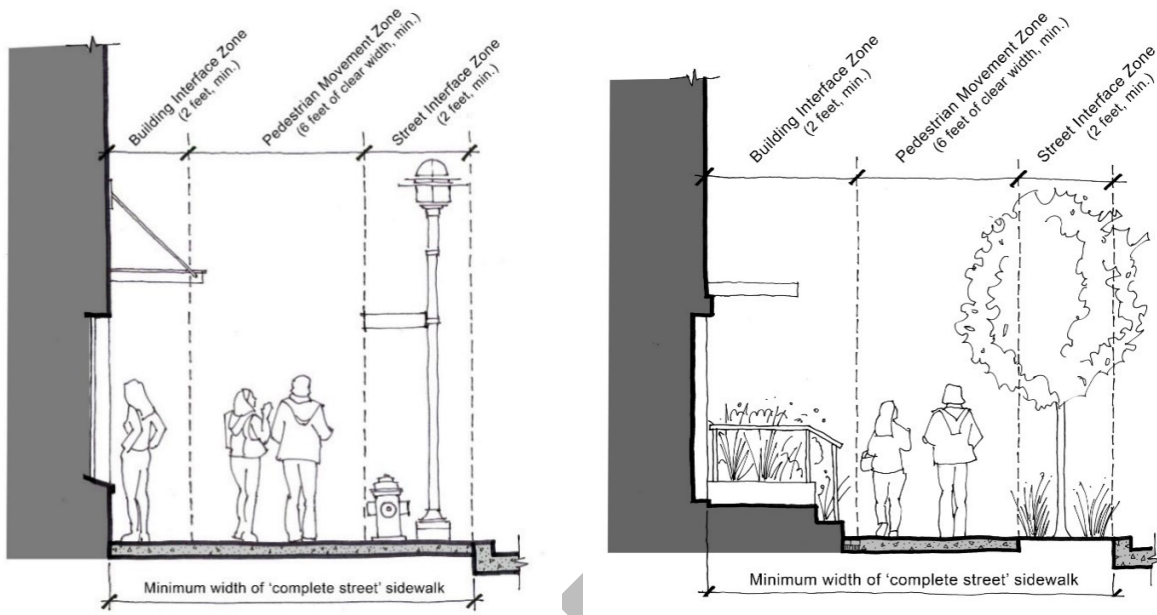
d. Street-facing wall areas for residential uses [DWELLINGS] shall be at least 20 percent [VISUAL ACCESS] windows or primary entrances in Urban Neighborhood Contexts and 10 percent in other areas.

e[H]. Where a building has three or more street frontages, these criteria apply along only two of the frontages.

19[17]. *Enhanced street sidewalk [OPTION].*

2 An enhanced street sidewalk is intended to provide an option  
3 for applicants to receive credit for exceeding the minimum  
4 development standards for sidewalks. An enhanced street  
5 sidewalk promotes sidewalk widening and streetscape  
6 enhancements to support higher levels of pedestrian activity,  
7 comfort, and safety in the district [AND ACCESS IN MIXED-  
8 USE DEVELOPMENTS]. An enhanced street sidewalk  
9 [ENVIRONMENT] with “complete[MAIN] street” (21.15.040)  
10 style amenities may be provided in lieu of required site  
11 perimeter landscaping [WHERE IT IS LOGICAL TO  
12 SUPPORT A PEDESTRIAN ZONE] as determined through an  
13 administrative site plan review, and subject to the following:

- 14
- 15 a. The enhanced street sidewalk cross-section [WIDTH]  
16 shall be at least 12 feet wide, and include a pedestrian  
17 movement zone, building interface zone, and street  
18 interface zone (21.15.040). The pedestrian movement  
19 zone shall have a clear width of at least six feet. The  
20 street interface zone shall be at least two feet wide from  
21 back-of-curb, and four feet wide along major arterials.  
22 Where a building adjoins the sidewalk, there shall be a  
23 building interface zone at least two feet wide [AT  
24 LEAST PART OF THE DEVELOPMENT’S  
25 FRONTAGE ALONG THE ENHANCED SIDEWALK  
26 SHALL FEATURE A PRINCIPAL BUILDING WITH A  
27 20-FOOT MAXIMUM SETBACK IN COMPLIANCE  
28 WITH SUBSECTION 21.06.030C.5].  
29



*Enhanced Street Sidewalks in [A]Commercial and Residential Settings*

*(Note to code revisor: replace existing illustration with illustrations above.)*

[I. A PUBLIC USE EASEMENT SHALL BE RECORDED FOR ANY PART OF THE DESIGNATED SIDEWALK TO BE LOCATED WITHIN THE SUBJECT PARCEL.

II. PHYSICAL OBSTRUCTIONS WITHIN THE SIDEWALK'S BUILDING INTERFACE ZONE, SUCH AS LANDSCAPING, ENTRY STOOPS, OR SEATING, SHALL EXTEND NO MORE THAN TWO FEET INTO THE MINIMUM REQUIRED 12 FOOT WIDTH, SO THAT AT LEAST TEN FEET REMAIN.]

b. The enhanced street sidewalk shall provide at least two-thirds the number of trees and shrubs that would otherwise have been required for site perimeter landscaping.

c. The enhanced street sidewalk may be placed wholly or in part within a right-of-way, subject to approval of the traffic engineer and municipal engineer.

i. The enhanced street sidewalk shall be subject to the applicable requirements of title 24,

1 including sections 24.30.020., *Permit to use*  
2 *Public Places*, and 24.90, *Encroachment*  
3 *Permit*.

4  
5 [II. IMPROVEMENTS WITHIN THE PUBLIC  
6 RIGHT-OF-WAY SHALL BE CONSISTENT  
7 WITH THE DCM AND MASS.]  
8

9 ii[III]. Existing improvements that meet the standards  
10 of the enhanced street sidewalk may be counted  
11 towards the requirements of this section, subject  
12 to approval by the director.  
13

14 iii[IV]. The owner shall maintain landscaping and  
15 amenities for the enhanced street sidewalk  
16 within the right-of-way[, AND COMPLY WITH  
17 THE PROVISIONS FOR REMOVAL OF SNOW  
18 AND ICE IN AMC 24.80.090, 100, AND 110].  
19

20 iv[V]. Where the right-of-way is not adequate or  
21 cannot be configured to accommodate the  
22 enhanced street sidewalk, then the  
23 development shall be set back from the street  
24 frontage as necessary to accommodate part of  
25 the improvements within the property. A public  
26 use easement shall be recorded for any part of  
27 the designated sidewalk width to be located  
28 within the subject parcel.  
29

30 20[18]. Parking courtyard.

31 \*\*\* \*\*

32 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2015-82, 7-28-15;  
33 AO 2015-100, 10-13-15; AO 2017-55, 4-11-17; AO 2020-38, 4-28-20)



1 **21.07.090 Off-Street Parking and Loading**

2  
3 A. *Purpose.* This section establishes off-street parking and loading  
4 requirements to accommodate anticipated parking utilization[AS A  
5 NECESSARY PART OF THE DEVELOPMENT AND USE OF  
6 LAND, TO ENSURE THE SAFE AND ADEQUATE FLOW OF  
7 TRAFFIC IN THE PUBLIC STREET SYSTEM], and to ensure that  
8 parking facilities[LOTS] are designed to perform in a safe, efficient  
9 manner. It is also the intent of this section to attenuate the adverse  
10 visual, environmental, and economic impacts of parking lots, and to  
11 balance the provision of adequate off-street parking with other  
12 modes of site access in order to achieve area-wide objectives for  
13 [TO ACHIEVE] a compact and efficient land use pattern, affordable  
14 housing, pedestrian-friendly environments, and use of multiple  
15 modes of transportation. Specific objectives of this section  
16 [PURPOSES] include to:

- 17  
18 1. Ensure that off-street parking, loading, and access demands  
19 will be met without impacting adjoining and [ADVERSELY  
20 AFFECTING OTHER ]nearby streets, properties, land uses,  
21 and neighborhoods;
- 22  
23 2. Provide for safe and orderly circulation and parking in parking  
24 and loading facilities, and minimize conflicts between  
25 pedestrians and vehicles;
- 26  
27 3. Encourage the efficient use of land and discourage [AVOID]  
28 the encumbrance of more space than is necessary for  
29 anticipated parking utilization;
- 30  
31 4. Recognize, through parking reductions and lower parking  
32 requirements, the parking efficiencies of mixed-use  
33 development, mixed-income housing, walkable development  
34 patterns, proximity to public transportation, and other area-  
35 specific neighborhood;
- 36  
37 5. Recognize, through area-specific exemptions from parking  
38 requirements, the parking efficiencies of public parking  
39 facilities, on-street parking management, and alternative  
40 travel mode incentives;
- 41  
42 6[4]. Improve the visual appearance of public street corridors by  
43 encouraging buildings and other attractive site features to  
44 become more prominent relative to parking lots and  
45 driveways;
- 46

1                   7.     Manage the width and frequency of driveways along street  
2                   frontages to provide adequate space for—and reduce  
3                   conflicts with—snow clearing and snow storage in the right-  
4                   of-way, on-street parking (where appropriate), and pedestrian  
5                   facilities.  
6

7                   8[5]. Provide for better pedestrian movement and encourage  
8                   alternative modes of transportation consistent with cleaner air  
9                   and water, greater transportation choice, and efficient infill  
10                  and redevelopment by reducing [THE] expanses of parking  
11                  [THAT MUST BE TRAVERSED BETWEEN  
12                  DESTINATIONS];  
13

14                  [6.     SUPPORT A BALANCED TRANSPORTATION SYSTEM  
15                  THAT IS CONSISTENT WITH CLEANER AIR AND WATER,  
16                  GREATER TRANSPORTATION CHOICES, AND  
17                  EFFICIENT INFILL AND REDEVELOPMENT;] and  
18

19                  9.     Facilitate bicycle use by providing safe, secure, and  
20                  convenient bike parking through minimum requirements for  
21                  the type of bike parking facilities and the amount of bicycle  
22                  spaces; and  
23

24                  10[7]. Allow flexibility in addressing vehicle parking, loading, and  
25                  access, including [PROVIDING FOR] reductions [AND  
26                  ALTERNATIVES] to [MINIMUM] parking requirements and  
27                  mechanisms to manage parking needs while promoting  
28                  development, reinvestment, and other community goals.  
29

30                  B.     *Applicability.*

31                         1.     *Generally.*

32                                 a.     The off-street parking and loading standards of this  
33                                 section 21.07.090 shall apply to all development in the  
34                                 municipality, including changes of use.  
35

36                                 b.     Except for the off-street loading requirements of  
37                                 subsection 21.07.090G., all other requirements of this  
38                                 section shall apply to Girdwood, unless specifically  
39                                 preempted in chapter 21.09.  
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c. Except when specifically exempted, the requirements of this section shall apply to all temporary parking lots and parking facilities[LOTS] that are a principal use on a site.

2. *Expansions, Relocations, and Enlargements.* A site to which a building is relocated shall provide [THE] required parking and loading spaces per tables 21.07-7, 21.07-8, and 21.07-11. An expansion or enlargement that is an increase in the floor area or other measure of off-street parking and loading requirements shall provide required parking and loading [SPACES AS REQUIRED] for the increase, except as provided otherwise in section 21.07.090F., *Parking Reductions*.

3. *Use of Required Parking Spaces.* Required parking spaces shall be available for the parking of passenger automobiles by residents, occupants, customers, visitors, or employees of the use. Required parking spaces shall[MAY] not be assigned, leased, or rented in any way to a use on another site, or to anyone who is not a resident, occupant, customer, guest, or employee, except for shared parking situations as provided in subsection 21.07.090F.5.[SEE SUBSECTION 21.07.090F.16. ALSO,] R[R]equired parking spaces shall not be used for the parking of equipment or for storage of goods or [INOPERABLE] vehicles.

4. *Regulation of Parking Space Use.* The providers of required off-street parking spaces may reasonably control the users thereof by means that may include, but are not limited to, restricting all parking to the users of the facility; parking lot attendants; control gates; tow-away areas; areas for exclusive use by employees, tenants or staff; areas restricted for use by customers or visitors; and imposing time limitations on users. Fees may be charged for the use of required parking, however required parking shall not in any way be made unavailable for the use served, except as provided otherwise in this title.[SUBJECT TO APPROVAL OF THE TRAFFIC ENGINEER. PRIOR TO APPROVAL OF THE PERMIT THE TRAFFIC ENGINEER MAY REVIEW ALL METHODS OF CONTROL AND MAY DISAPPROVE OF ANY RESTRICTION SUCH AS FEES THAT ADVERSELY AFFECTS THE PURPOSE OF THIS SECTION.] The municipality may enforce any approved parking plan or

1 restrictions through any of the code enforcement provisions  
2 set forth in chapter 21.14, Enforcement.  
3

- 4 5. *Parking Nonconformities.* When a site is out of compliance as  
5 to the number of required or allowed parking spaces, section  
6 21.13.060, *Characteristics of Use*, applies.  
7

8 C. *Computation of Parking and Loading Requirements.*

9 \*\*\*

10 5. *Additional computation standards.*

11 \*\*\*

12 c. *Areas that Count Toward Minimum but Not Maximum*  
13 *Parking Requirements.* For the purpose of calculating  
14 parking requirements, the following types of parking  
15 spaces shall not count against the maximum parking  
16 requirement, but shall count toward the minimum  
17 requirement:

- 18
- 19 i. Accessible parking spaces;
  - 20
  - 21 ii. Parking spaces set aside for passenger  
22 loading [ZONES SPACES ]including taxicab  
23 stands and ride hailing service spaces, provided  
24 that such spaces are not required by the traffic  
25 engineer pursuant to subsection 21.07.090I.;
  - 26
  - 27 iii. Vanpool and carpool parking spaces;
  - 28
  - 29 iv. Stacked, tandem, and full-time attendant (valet)  
30 parking spaces in conformance with subsection  
31 21.07.090H.12.;
  - 32
  - 33 v. User-paid parking such as unbundled parking;
  - 34
  - 35 vi. Parking spaces with electric vehicle supply  
36 equipment (EVSE) for EV charging;
  - 37
  - 38 vii[IV]. Parking spaces provided as the required parking  
39 for a use on another parcel through a  
40 municipally approved shared parking or off-site  
41 parking agreement; and
  - 42
  - 43 viii[V]. Parking structures, underground parking, and  
44 parking within, above, or beneath the building(s)  
45 it serves.  
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- d. Reductions to Required Parking Not Applicable to Accessible Parking Spaces or Passenger Loading. The following types of parking spaces shall not be reduced by the area-specific parking requirements or exemptions established in 21.07.090E.2. or the parking reductions and alternatives in 21.07.090F.:
- i. Required accessible (ADA) parking spaces pursuant to subsection 21.07.090J.
- ii. Required passenger loading spaces pursuant to subsection 21.07.090I.

D. *Parking Lot Layout and Design Plan.*

\*\*\*      \*\*\*      \*\*\*

2. *Minimum Plan Requirements.*

- a. The parking facility layout, circulation, and design plan shall be prepared by a design professional and stamped by a professional registered with the Alaska State Board of Registration for Architects, Engineers, and Land Surveyors, except that parking lots with fewer than 20 parking spaces shall be exempt.
- b. The parking facility layout, circulation, and design plan shall include:
  - i. Location of permanent or temporary snow storage areas with calculations per 21.07.040F.;
  - ii. Location of required landscaping areas, refuse screening, and fences;
  - iii. Location of required pedestrian sidewalks and walkways per 21.07.060E. Show dimensions for sidewalk widths and grades with spot elevations;
  - iv. Off-street parking and loading calculation for all uses located on the site per tables 21.07-7[4] and 21.07-11[6];
  - v. Parking space and loading berth locations. Include number of spaces provided, typical space dimensions, and grades per 21.07.090H.;

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vi. Where loading facilities or on-site refuse collection are provided, show commercial truck circulation and truck turning radii when required by the traffic engineer;

vii[VI]. Show all parking facility[AREA] circulation patterns including location of curbed end islands at end of parking rows. Provide dimensions for the width of the circulation aisles, and show grades with spot elevations per 21.07.090H.;

viii[VII]. Location of accessible parking spaces and access aisles per 21.07.090J.;

ix[VIII]. Location of passenger loading zones and spaces [IF REQUIRED ]per 21.07.090I.;

x. Emergency access, fire lanes, and refuse collection;

xi[IX]. Vehicle drive-throughs, vehicle queueing spaces and dimensions including vehicle-to-vehicle separation if required per 21.07.090 L.

xii[X]. Number, location and dimensions of bicycle parking spaces if required per 21.07.090K., including bicycle parking access routes;

xiii[XI]. Driveways to streets and alleys. Provide dimensions for throat width/depth, landing grades, and driveway slope including spot elevations. Show sight distance triangles per the Municipal Driveway Standards;[.]

xiv[XII]. On site traffic control signage and locations. Provide a sign summary table that indicates Sign ID, MUTCD/ATMS sign designation, description, and the direction the sign is facing;[.]

xv[XIII]. Required parking lot lighting locations, lighting calculations and glare statement, except that parking lots with fewer than 10 parking spaces serving three- and four-unit multifamily uses are exempt from providing a lighting engineering plan; and

xvi[XIV]. Location of significant drainage elements such as manholes, catch basin, and drainage swales.

- c. The traffic engineer shall ensure that provisions have been made for minimum interference with street traffic flow and safe interior vehicular and pedestrian circulation, transit, and parking.

E. *Off-Street Parking Requirements*

1. *Minimum Number of Spaces Required.* [UNLESS OTHERWISE EXPRESSLY STATED IN THIS TITLE, O]off-street parking spaces shall be provided in accordance with this section 21.07.90E.[TABLE 21.07-4, OFF-STREET PARKING SPACES REQUIRED, AND SUBSECTION E.2. BELOW]. Reductions [, EXEMPTIONS] and alternatives to the requirements of this section [REQUIRED MINIMUM NUMBER OF PARKING SPACES] are provided in [SUBSECTION] 21.07.090F., Parking Reductions and Alternatives [BELOW].
2. Area-specific Parking Requirements. Lower parking requirements for specific areas that have less parking utilization and more alternative transportation options are set forth in table 21.07-7.[MINIMUM OF THREE PARKING SPACES. WHERE A USE IS REQUIRED TO PROVIDE OFF-STREET PARKING AND THE AMOUNT SPECIFIED IN TABLE 21.07-4 WOULD RESULT IN FEWER THAN THREE SPACES BEING REQUIRED FOR THE USE, THE USE SHALL PROVIDE AT LEAST THREE PARKING SPACES INCLUDING ONE VAN-ACCESSIBLE PARKING SPACE PURSUANT TO SUBSECTION 21.07.090J. WHERE THERE ARE MULTIPLE USES LOCATED ON A SITE, THE USES MAY SHARE THE ACCESSIBLE SPACE AS LONG AS THE REQUIREMENTS OF SUBSECTION 21.07.090J.1. ARE MET. PARKING REDUCTIONS IN SUBSECTION 21.07.090F. SHALL ALSO COMPLY WITH THIS SUBSECTION E.2. THE MINIMUM OF THREE PARKING SPACES SHALL NOT APPLY TO USES IN THE DT DISTRICTS, RESIDENTIAL HOUSEHOLD LIVING USES, COMMUNITY GARDENS, PARKS AND OPEN SPACE, UTILITY SUBSTATIONS, OR FUELING STATIONS AND FOOD AND BEVERAGE KIOSKS THAT ARE EXCLUSIVELY FOR DRIVE-THROUGH CUSTOMERS.]

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<b>TABLE 21.07-7: AREA-SPECIFIC PARKING REQUIREMENTS</b>		
<u>Areas</u>	<u>Applicability</u>	<u>Minimum Spaces Required</u>
<u>Downtown (DT) zoning districts</u>	<u>All Developments</u>	<u>No off-street parking is required, as provided in 21.11.070F.</u>
<u>Traditional Urban Neighborhood Context (Section 21.07.015D., Map 21.07-1.)</u>	<u>Residential Uses east of C Street</u>	<u>70% of the minimum spaces required in table 21.07-8.</u>
	<u>All Other Developments</u>	<u>80% of the minimum spaces required in table 21.07-8.</u>
<u>Edge Urban Neighborhood Context (Section 21.07.015D., Maps 21.07-1 thru -3.)</u>	<u>Residential Uses</u>	<u>80% of the minimum spaces required in table 21.07-8.</u>
	<u>All Other Developments</u>	<u>90% of the minimum spaces required in table 21.07-8.</u>
<u>Transit-Supportive Development Corridors outside of Edge Urban Context areas (Section 21.07.015D., Maps 21.07-2 thru -4.)</u>	<u>All Developments</u>	<u>90% of the minimum spaces required in table 21.07-8.</u>
<u>Open Option Parking Districts (21.07.090E.7.)</u>	<u>All Developments</u>	<u>No off-street parking is required, subject to subsection 21.07.090E.7.</u>
<u>Girdwood</u>	<u>See section 21.09.070L. for area-specific parking requirements in Girdwood.</u>	

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3. Use-specific parking requirements. All development in the Municipality not addressed in the area-specific parking requirements in table 21.07-7 shall provide off-street parking spaces in accordance with table 21.07-8:

<b>TABLE 21.07-8[4]: OFF-STREET PARKING REQUIREMENTS BY LAND USE [SPACES REQUIRED]</b> (“du” = dwelling unit; “sf” = square feet; “gfa” = gross floor area)			
	<b>Use Type</b>	<b>Minimum Spaces Required</b>	<b>See Loading Subsection 21.07.090G.</b>
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4[3]. Uses not listed or that have no specific requirement.  
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5[4]. Maximum number of spaces permitted.  
 \*\*\*      \*\*\*      \*\*\*



1           6[5]. *Parking location. Except as provided in subsection*  
2           *21.07.090F., all required parking shall be on the same lot as*  
3           *the use served. [HOWEVER, REQUIRED PARKING MAY BE*  
4           *ON AN ABUTTING OR ADJACENT LOT PROVIDED THE*  
5           *ZONING DISTRICT IN WHICH THE LOT IS LOCATED*  
6           *ALLOWS FOR OFF-STREET PARKING AS A PERMITTED*  
7           *PRINCIPAL USE, SITE PLAN REVIEW USE, OR*  
8           *CONDITIONAL USE; IN WHICH CASE THERE SHALL BE A*  
9           *PARKING AGREEMENT WHICH MEETS THE*  
10           *REQUIREMENTS OF SUBSECTION F.1. BELOW.]*

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12           7. *Open Option Parking District*

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14           a. *Purpose.* This subsection provides for the  
15           establishment of Open Option Parking Districts. Open  
16           Option Parking Districts remove minimum off-street  
17           parking requirements within specified areas and  
18           replace them with street management strategies within  
19           the public rights-of-way, as well as parking demand  
20           reduction strategies on larger developments. Property  
21           owners, developers, and businesses decide how much  
22           off-street parking to provide on their properties based  
23           on site-specific operations and activities. The public  
24           right-of-way gets managed separately, to ensure on-  
25           street parking, street maintenance and snow removal,  
26           and safe pedestrian and traffic movement. The Open  
27           Option approach is intended to result in more efficient  
28           use of urban land, more economic development and  
29           housing opportunities, and greater transportation  
30           choices.

31  
32           b. *Applicability.* Open Option Parking Districts may be  
33           established in the urban neighborhood development  
34           contexts and transit-supportive development corridors  
35           delineated in section 21.07.015.

36  
37           c. *Minimum Area Requirements.* The designation of an  
38           Open Option Parking District shall be considered only  
39           for whole city blocks (surrounded by public streets or  
40           public lands), or whole street face blocks (all the  
41           properties with primary frontage on a length of street  
42           between two intersections). Open Option Parking  
43           Districts may include multiple whole city blocks and/or  
44           street face blocks. The total area of the parcels within  
45           an Open Option Parking District shall be at least 1.75  
46           acres (excluding rights-of-way).

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d. Procedure for Designating an Open Option Parking District.

i. Application Submittal. Any person, community council, or the municipality may initiate the designation of an Open Option Parking District. An application may be obtained from the planning department. The applicant shall select one of the two approval procedure options provided in ii. and iii.

ii. Property Owner Approval Option. The applicant shall demonstrate that fifty-one percent of the property owners in the proposed Open Option Parking District support the designation in writing through a letter or a signed petition to the department. Upon receiving the application, the department shall notify all property owners in the proposed designated area and provide 30 days for comment. The Traffic Engineer and Director shall then evaluate the proposal and act as the decision making body subject to the approval criteria in d. below.

iii. Assembly Approval Option. Upon receiving the application, the Planning Department shall notify all property owners in the designated area and provide 30 days to comment. The Traffic Engineer and Director shall then evaluate the proposal using the approval criteria in d. below, and forward a recommendation to the Anchorage Assembly. The Assembly shall hold a public hearing and take final action as the decision making body.

iv. Agency Review. The Planning Department shall request comment by ROW agencies and utilities, including the traffic engineering, right-of-way enforcement, public works, the anchorage parking authority, municipal and street maintenance, fire department, police department, and the public transportation department.

1 v. Documentation of Approved Open Option  
2 Parking Districts. The Planning Department  
3 shall maintain a publicly available map of  
4 designated Open Option Parking Districts online  
5 and at the department.  
6

7 e. Approval Criteria for Designating an Open Option  
8 Parking District. The traffic engineer and director shall  
9 evaluate each proposed Open Option Parking District  
10 based on the following approval criteria:  
11

12 i. On-Street Parking Management. There shall be  
13 a plan for on-street parking management  
14 address parking utilization, congestion, and  
15 availability, with the approval of agencies  
16 assigned.  
17

18 ii. ROW Management Plan. There shall be a plan  
19 for coordinating on-street parking with street  
20 maintenance, snow removal, refuse collection;  
21 public transportation, and other street-based  
22 operations, approved by the requisite public  
23 agencies.  
24

25 iii. Consistency with Comprehensive Plan. The  
26 proposal shall conform to applicable  
27 neighborhood, district, or other area-specific or  
28 function plans  
29

30 f. Parking and TDM Requirements for Developments in  
31 Open Option Parking Districts  
32

33 i. No off-street parking spaces are required for  
34 developments, except that accessible (ADA)  
35 parking spaces required by 21.07.090J. shall be  
36 provided.  
37

38 ii. Development projects that result in 10 or more  
39 dwelling units, 10 or more bedrooms of group  
40 living, new construction of 10,000 or more  
41 square feet of non-residential gross floor area,  
42 or substantial building renovations involving a  
43 change of use of 50,000 or more square feet of  
44 non-residential gross floor area, shall provide  
45 disincentives for off-street parking utilization and  
46 incentives for alternative means of site access,

1 by providing parking reduction strategies from  
2 section 21.07.090F., Table 21.07-9. Structured  
3 parking, industrial uses, and affordable housing  
4 units that meet 21.07.110F., are exempt from  
5 this requirement.  
6

7 (A) The parking reduction strategies  
8 selected from section 21.07.090F.1.,  
9 Table 21.07-9 shall amount to a total  
10 reduction of at least 10 percent from what  
11 would otherwise be the minimum parking  
12 requirement were it not for the Open  
13 Option Parking District, using the  
14 “Reduction Amount” column in Table  
15 21.07-9. The total reduction required  
16 shall increase an additional 1 percent for  
17 each 10 off-street parking spaces above  
18 20 parking spaces in the development.  
19

20 (B) The requirements of 21.07.090F. for  
21 receiving parking reductions apply.  
22

23 (C) The parking reductions in section F. of  
24 Table 21.07-9 do not count. However, the  
25 applicant may propose strategies not  
26 included in Table 21.07-9, subject to  
27 21.07.090F.8, Discretionary Parking  
28 Reductions.  
29

30 F. *Parking reductions and alternatives.*  
31

32 1. Parking reductions allowed. This section 21.07.090F. allows  
33 administrative reductions to [THE TRAFFIC ENGINEER AND  
34 DIRECTOR MAY APPROVE REDUCTIONS AND  
35 ALTERNATIVES TO PROVIDING] the number of off-street  
36 parking spaces required by section 21.07.090E. [TABLE  
37 21.07-4, AND/OR TO THE CIRCULATION AND  
38 DIMENSIONAL STANDARDS OF SUBSECTIONS H.8. AND  
39 H.9., IN ACCORDANCE WITH THE FOLLOWING  
40 STANDARDS.] Developments are allowed to apply the  
41 reductions shown in Table 21.07.-9, provided the following:  
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43 a. The development meets the additional requirements  
44 set forth in Table 21.07-9;  
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- b. The property owner enters into a parking agreement with the Municipality of Anchorage as provided in 21.07.090F.2.;
- c. Multiple parking reductions are calculated as provided in F.3.;
- d. Pedestrian access improvements are provided as set forth in 21.07.090F.4.; and
- e. Proposals for larger percentage reductions than shown, that modify any of the provisions for the reductions shown, or that propose other types of parking reductions from those in in table 21.07-9 are subject to a discretionary review and approval by the traffic engineer and director as provided in subsection 21.07.090F.8., Discretionary Parking Reductions.

**Table 21.07-9: Parking Reductions and Alternatives**

<u>Type of Reduction</u>	<u>Applicability</u>	<u>Additional Requirements</u>	<u>Reduction Amount</u>
<b>A. Shared Vehicle Programs:</b> Participation in one or more of the shared vehicle programs below may substitute for required parking spaces, provided that information about the programs is available in a location visible to all residents and employees.			
<u>Carpool Program</u>	<u>Non-residential uses</u>	<u>The employer or property owner sponsors a carpool program that is available to all employees and provides designated carpool parking spaces signed for exclusive use by the carpool program.</u>	<u>Each carpool space may count as two spaces toward meeting the minimum number of required parking spaces, up to a 2% reduction in the number of required parking spaces.</u>
<u>Rideshare Program</u>	<u>Non-residential uses</u>	<u>The employer or property owner sponsors a rideshare program that is available to all employees and provides designated rideshare parking spaces that meet the passenger loading space dimensional standards of 21.07.090I.2. and are signed for exclusive use by the rideshare program.</u>	<u>Each rideshare space may count as six spaces toward meeting the minimum number of required parking spaces, up to a 5% reduction in the number of required parking spaces.</u>
<u>Car-Share Program</u>	<u>Residential uses located in the Neighborhood Development Contexts<sup>1</sup></u>	<u>The property owner sponsors memberships to an active car-share program for all households or group living residents on the site and provides designated car-share spaces that are signed for exclusive use.</u>	<u>Each carshare space may count as five spaces toward meeting the minimum number of required parking spaces, up to a 10% reduction in the number of required parking spaces.</u>
<u>Transit Pass Benefits</u>	<u>Any use located in the Neighborhood Development Contexts<sup>1</sup></u>	<u>The property owner sponsors public transit passes cost-free to all employees or residents.</u>	<u>10% reduction in the number of required parking spaces.</u>
***	***	*** (table continued)	

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<b>Table 21.07-9: Parking Reductions and Alternatives</b>			
<u>Type of Reduction</u>	<u>Applicability</u>	<u>Additional Requirements</u>	<u>Reduction Amount</u>
***	***	*** (table continued from previous page)	
<b>B. Pedestrian Amenities:</b> Developments that provide improved conditions for walking and bicycling are eligible for reductions in the minimum number of required parking spaces, as provided below.			
<u>Additional Bicycle Parking</u>	<u>Non-residential use, group living use, and any household living use with four or more dwelling units.</u>	<u>The development provides more than the minimum number of required bicycle parking spaces. Each bicycle space meets the standards of 21.07.090K.4.</u>	<u>Each four bicycle parking spaces count as one automobile parking space, with a minimum allowed reduction of one space and a maximum of a 10% reduction in the number of required automobile parking spaces.</u>
<u>Enhanced On-Site Walkway</u>	<u>Any use in the Neighborhood Development Contexts<sup>1</sup></u>	<u>The development provides an enhanced on-site walkway per 21.07.060G.3.</u>	<u>2% reduction in the number of required parking spaces</u>
<u>Enhanced Street Sidewalk</u>	<u>Any use in the Neighborhood Development Contexts<sup>1</sup></u>	<u>The development provides an enhanced street sidewalk per 21.07.060G.19.</u>	<u>2% reduction in the number of required parking spaces</u>
<u>Transit Stop or Shelter</u>	<u>Any use in the Neighborhood Development Contexts<sup>1</sup></u>	<u>Based on a determination of need by the public transportation department, the development provides a public use easement or transit stop improvements per 21.07.060G.7.</u>	<u>2% reduction in the number of required parking spaces</u>
<u>Other Pedestrian Amenities</u>	<u>Any use in the Neighborhood Development Contexts<sup>1</sup></u>	<u>The development provides one or more additional pedestrian amenities from section 21.07.060G, not otherwise required by this title.</u>	<u>1% reduction in the number of required parking spaces for each pedestrian amenity.</u>
<b>C. Parking Pricing:</b> Developments that offer the parking pricing strategies below are eligible for reductions in the minimum number of required parking spaces.			
<u>Parking Cash-out</u>	<u>Non-residential uses</u>	<u>The use implements a parking cash-out program as defined in 21.15.040 and informs all employees of the program. The cash-out value of the parking space is allowed to be up to one-year in duration.</u>	<u>10% reduction in the number of required parking spaces.</u>
<u>Unbundled Parking</u>	<u>Household living uses and non-residential uses</u>	<u>All parking accessory to the use is unbundled parking as defined in 21.15.040, by which the parking spaces are leased or sold separately from the rental or purchase fees for the dwelling units or building space.</u>	<u>10% reduction in the number of required parking spaces.</u>
***	***	*** (table continued)	

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<b>Table 21.07-9: Parking Reductions and Alternatives</b>			
<u>Type of Reduction</u>	<u>Applicability</u>	<u>Additional Requirements</u>	<u>Reduction Amount</u>
***	***	*** (table continued from previous page)	
<b>D. Housing:</b> Housing units with characteristics that reduce parking utilization are eligible for reductions in the minimum number of required parking spaces, as provided below.			
<u>Affordable Rental Housing</u>	<u>Household living uses</u>	<u>Rental housing units that meet the standards of 21.07.110G., <i>Standards for Affordable Housing</i>, are rented at rates affordable to lower income households.</u>	<u>Each affordable dwelling unit is eligible for a 25% reduction in the number of required parking spaces.</u>
<u>Senior Housing</u>	<u>Residential uses</u>	<u>Housing that meets the definition of senior housing (21.15.040) and is solely occupied by persons 62 years or older.</u>	<u>Each senior housing unit is eligible for a 25% reduction in the number of required parking spaces.</u>
<b>E. Shared and Off-site Parking Facilities:</b> Properties that utilize off-site parking facilities or shared parking between uses are eligible for reductions to the minimum number of required parking spaces, as provided below.			
<u>Shared Parking</u>		<u>See section 21.07.090F.5.</u>	
<u>Off-site Parking</u>		<u>See section 21.07.090F.6.</u>	
<u>District Parking</u>		<u>See section 21.07.090F.7.</u>	
<b>F. Prioritization of Other Goals:</b> Developments with the following public benefit features are eligible for a reduction in the minimum number of parking spaces, as follows:			
<u>Land Banking</u>	<u>Any development that sets aside an area to provide for the future construction of deferred parking spaces.</u>	<u>The area set aside is landscaped with site enhancement landscaping or pedestrian amenities. The applicant provides an alternate site plan for approval that accommodates the deferred parking, landscaping, pedestrian facilities, and other site elements that would be required by this title without the land banking.</u>	<u>The development may set aside the land area that would otherwise be needed in order to provide up to 25% of the number of required parking spaces.</u>
<u>Adaptive Reuse of Older Buildings</u>	<u>Any use located in the Neighborhood Development Contexts<sup>1</sup>, except not drive-through service or vehicle-related uses.</u>	<u>The development is a building expansion, alteration, or change of use, in an existing building that was originally permitted prior to June 13, 1978. The development does not convert housing units to non-residential uses.</u>	<u>Exemption from the first 10 percent increase in the total number of spaces required on the development site, for a maximum allowed exemption of five parking spaces. A parking reduction for adaptive reuse shall be used only once per individual building.</u>
<u>Historic and Cultural Landmark Preservation</u>	<u>Any use that involves preservation of a landmark listed in the Anchorage local landmarks register.</u>	<u>The development does not decrease the number of parking spaces that existed on the site as of [effective date of this ordinance] to less than the number otherwise required by this title.</u>	<u>25% reduction in the number of required parking spaces.</u>
<sup>1</sup> Neighborhood Development Contexts include the Traditional Urban Neighborhood, Edge Urban Neighborhood, and Transit-Supportive Development Context areas delineated in section 21.15.015.			

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2[1]. Parking agreements. All parking reductions [OR ALTERNATIVE SHALL] require a written parking agreement between the property owner(s) and the municipality, subject to the following standards: [EXCEPT WHERE EXPRESSLY STATED OTHERWISE.]

a. Recordation. The municipality shall record the parking agreement at the district recorder's office as a

1 covenant that runs with the land and is binding on the  
2 owner and all successors and assigns for as long as  
3 the required number of off-street parking spaces is not  
4 provided as a result of the parking reduction[ OR  
5 ALTERNATIVE. ALL PARTIES INVOLVED IN THE  
6 PARKING REDUCTION OR ALTERNATIVE SHALL  
7 PARTICIPATE IN THE PARKING AGREEMENT].  
8 Recordation of the agreement shall take place before  
9 issuance of an entitlement that is contingent  
10 upon[REQUIRING] a parking reduction[ OR  
11 ALTERNATIVE].

12  
13 b. *Content.* The format (template) [AND CONTENT] of  
14 the parking agreement shall be provided by the  
15 municipality, as approved by the traffic engineer and  
16 director. The parking agreement[IT] shall guarantee  
17 installation and maintenance of any required  
18 improvements by the property owner, and[/OR] the  
19 owner's continued participation in any parking  
20 management strategy required for a parking reduction.  
21 The parking agreement shall be accompanied by a site  
22 plan showing the parking and facilities required for the  
23 parking reduction.

24  
25 c. *Additional Content for Agreements Subject to*  
26 *Discretionary Review.* Discretionary parking reductions  
27 subject to 21.07.090F.8. shall be preceded by a formal  
28 letter requesting the reduction for concurrence by the  
29 traffic engineer and director. The letter shall include  
30 justification for the parking reduction, including any  
31 parking demand study if prescribed by the traffic  
32 engineer. The parking agreement shall include a  
33 contingency plan, and shall guarantee [ASSURE]  
34 future implementation of the[A] contingency plan by the  
35 property owner if so ordered by the traffic engineer.  
36 The contingency plan may include strategies such as:

37  
38 i. A deferred parking site plan [INSTALLATION  
39 OF PARKING,];

40  
41 ii. P[P]ayment to the municipality for the full cost of  
42 providing the required parking;[,]

43  
44 iii. I[T]ransportation demand management  
45 programs;[,]; or



1                   iv. Q[O]ther parking management strategies  
2                   identified in the parking reductions or  
3                   alternatives [OF THIS SECTION].  
4

5                   d[C]. Termination. The municipality may terminate the  
6                   parking agreement if it is demonstrated that the parking  
7                   reduction does not support the parking utilization for  
8                   the site, or if the development is in violation of the  
9                   parking agreement. The traffic engineer or planning  
10                   director may require a contingency plan per AMC  
11                   21.07.090F.1.c. and a new parking agreement, or the  
12                   site may be required to provide all parking per AMC  
13                   21.07.090E. If for any reason the parking agreement  
14                   terminates, owners and all successors and assigns  
15                   who are parties to the parking agreement shall comply  
16                   with all provisions of this title governing the required  
17                   number of off-street parking spaces.  
18

19                   3[2]. Calculation of parking reductions.  
20

21                   a. Calculation of multiple reductions. A development may  
22                   be eligible for more than one parking reduction listed in  
23                   this section 21.07.090F.[MULTIPLE REDUCTIONS  
24                   FROM THE REQUIRED NUMBER OF PARKING  
25                   SPACES.] The total impact of multiple parking  
26                   reductions on a development's parking requirement  
27                   shall be the sum of the individual  
28                   reductions[CALCULATED AS BEING  
29                   MULTIPLICATIVE AND NOT ADDITIVE WHERE A  
30                   DEVELOPMENT IS ELIGIBLE FOR MORE THAN  
31                   ONE. FOR EXAMPLE, IF ONE REDUCTION IS 20  
32                   PERCENT, AND A SECOND REDUCTION IS AN  
33                   ADDITIONAL 15 PERCENT, THEIR COMBINED  
34                   REDUCTION SHALL BE CALCULATED AS 80  
35                   PERCENT TIMES 85 PERCENT EQUALS 68  
36                   PERCENT, FOR A 32 PERCENT AGE POINT TOTAL  
37                   REDUCTION, RATHER THAN ADDING 20 PERCENT  
38                   PLUS 15 PERCENT EQUALS 35 PERCENT. THIS IS  
39                   BECAUSE THE 15 PERCENT REDUCTION APPLIES  
40                   TO A BASE THAT IS ALREADY REDUCED 20  
41                   PERCENT].  
42

43                   b. Maximum Nondiscretionary Reduction. A request for a  
44                   greater than 25 percent reduction in the required  
45                   amount of parking from one or more reductions listed  
46                   in table 21.07-9 shall be subject to discretionary review

1 by the traffic engineer and director, except that the  
2 following reductions listed in table 21.07-9 are not  
3 subject to this percentage limitation:

4  
5 i. Housing (Table 21.07-9).

6  
7 ii. Land Banking (Table 21.07-9).

8  
9 iii. Shared parking (21.07.090F.5.).

10  
11 iv. Off-site parking (21.07.090F.6).

12  
13 c[B]. Rounding of Fractional[MINIMUM] Reductions  
14 [CREDIT OF ONE SPACE]. Rounding of fractional  
15 numbers shall occur only after subtracting the parking  
16 reduction(s) from the minimum number of required  
17 parking spaces, as provided in 21.07.090C.1[IF THE  
18 TOTAL APPROVED REDUCTION FROM THE  
19 REQUIRED NUMBER OF PARKING SPACES FOR A  
20 DEVELOPMENT IS CALCULATED TO BE A  
21 REDUCTION OF LESS THAN ONE PARKING  
22 SPACE, IT SHALL BE CREDITED AS A REDUCTION  
23 OF ONE PARKING SPACE].

24  
25 4[3]. Pedestrian Access Improvements Required [QUALIFYING  
26 SITE DEVELOPMENT]. Developments shall improve  
27 pedestrian access as provided below, in order to be eligible  
28 for parking reductions. [USES SHALL PROVIDE THE  
29 FOLLOWING ENHANCEMENTS TO BE ELIGIBLE FOR ANY  
30 REDUCTIONS IN THE NUMBER OF REQUIRED PARKING  
31 SPACES, EXCEPT WHERE STATED OTHERWISE. THE  
32 QUALIFYING SITE CRITERIA SHALL NOT BE REQUIRED  
33 FOR THE FOLLOWING PARKING REDUCTIONS AND  
34 ALTERNATIVES IN THIS SUBSECTION 21.07.090F:  
35 DOWNTOWN ANCHORAGE PARKING EXEMPTION, LAND  
36 BANKING, STACKED AND TANDEM PARKING, OR  
37 SMALLER PARKING SPACES FOR LOW-TURNOVER  
38 USES.] Industrial uses, public safety facilities, transportation  
39 facilities, and utility facilities are exempt from this section  
40 21.07.090F.5 [THE QUALIFYING SITE DEVELOPMENT  
41 CRITERIA].

42  
43 a. Administrative Relief and Adjustment. The traffic  
44 engineer and director may approve administrative relief  
45 or adjustments to the standards of this subsection F.4.  
46 for changes of use or modifications to existing buildings

1 and sites, as part of the review and approval of a  
2 parking reduction, provided the applicant  
3 demonstrates the adjustment is necessary to  
4 compensate for some practical difficulty of the site, or  
5 some unusual aspect of the site not shared by  
6 landowners in general. The justification for the  
7 administrative adjustment shall be recorded as an  
8 appendix to the parking reduction agreement in  
9 21.07.090F.2.

10  
11 b[A]. Pedestrian Frontage Standard [STREET ORIENTED  
12 BUILDING]. Developments shall meet the standards of  
13 section 21.07.060F., Pedestrian Frontage Standard,  
14 except where not applicable pursuant to 21.07.060F.2.  
15 [FOR BUILDINGS CONSTRUCTED AFTER  
16 JANUARY 1, 2014, PRIMARY ENTRANCES AND/OR  
17 WINDOWS PROVIDING VISUAL ACCESS SHALL  
18 COMPRISE AT LEAST 15 PERCENT OF THE AREA  
19 OF ANY STREET FACING BUILDING ELEVATION.  
20 FOR NONRESIDENTIAL USES, WINDOWS  
21 PROVIDING VISUAL ACCESS AND/OR PRIMARY  
22 ENTRANCES SHALL COMPRISE AT LEAST 50  
23 PERCENT OF THE LENGTH AND 25 PERCENT OF  
24 THE GROUND-FLOOR WALL AREA OF ANY  
25 STREET FACING BUILDING ELEVATION.]

26  
27 c[B]. Walkway and Sidewalk Access [TO THE STREET].  
28 Developments shall comply with subsections  
29 21.07.060E.2., Sidewalks if applicable, and  
30 21.07.060E.4., On-Site Pedestrian Walkways. [A  
31 WALKWAY MEETING THE REQUIREMENTS OF  
32 SECTION 21.07.060 SHALL CONNECT AT LEAST  
33 ONE PRIMARY ENTRANCE TO A STREET. THE  
34 DIRECTOR AND THE TRAFFIC ENGINEER MAY  
35 WAIVE THIS REQUIREMENT IN SITUATIONS WITH  
36 EXISTING STRUCTURES WHERE IT IS  
37 DEMONSTRATED THAT THE ADDITION OF A  
38 WALKWAY WILL CAUSE A REDUCTION IN  
39 PARKING AND/OR LANDSCAPING BELOW  
40 REQUIRED LEVELS, OR WHERE THE WORK  
41 REQUIRED TO ADD A WALKWAY IS OUT OF  
42 PROPORTION WITH THE WORK BEING DONE TO  
43 EFFECT A CHANGE OF USE.]

44  
45 d[C]. Bicycle Parking [PARKING FACILITY LOCATION].

1                                   Developments shall comply with subsection  
2                                   21.07.090K., *Bicycle Parking Spaces*. [FOR  
3 BUILDINGS CONSTRUCTED AFTER JANUARY 1,  
4 2014, PARKING FACILITIES INCLUDING  
5 DRIVEWAYS SHALL COMPRISE NO MORE THAN  
6 50 PERCENT OF THE AREA BETWEEN THE  
7 STREET PROPERTY LINE AND THE STREET  
8 FACING BUILDING ELEVATION, AND GARAGE  
9 DOORS SHALL COMPRISE NO MORE THAN 50  
10 PERCENT OF THE LENGTH OF THE STREET  
11 FACING BUILDING ELEVATION. THESE  
12 REQUIREMENTS APPLY TO NO MORE THAN TWO  
13 STREET FRONTAGES.]

14  
15                   [D.    *PRIVATE OPEN SPACE.* FOR RESIDENTIAL  
16 DEVELOPMENTS THAT ARE REQUIRED TO  
17 PROVIDE PRIVATE OPEN SPACE, AN ADDITIONAL  
18 40 SQUARE FEET OF PRIVATE OPEN SPACE THAT  
19 MEETS THE REQUIREMENTS OF SECTION  
20 21.07.030 SHALL BE PROVIDED FOR EACH  
21 REDUCTION OF ONE PARKING SPACE IN  
22 DEVELOPMENTS BUILT AFTER JANUARY 1, 2014.  
23 THIS SHALL BE COMMON PRIVATE OPEN SPACE  
24 IN THE CASE OF MULTIFAMILY AND MIXED-USE  
25 DWELLINGS.

26  
27                   E.    *CROSS-ACCESS TO ADJACENT PROPERTIES.*  
28 THE DIRECTOR AND THE TRAFFIC ENGINEER  
29 MAY DETERMINE THERE IS POTENTIAL FOR  
30 DRIVEWAY OR WALKWAY CROSS-ACCESS TO  
31 ABUTTING PROPERTIES AND MAY REQUIRE A  
32 CROSS-ACCESS FACILITY AND/OR EASEMENT  
33 WITHIN THE SUBJECT PROPERTY TO THE SITE  
34 BOUNDARY. VEHICULAR CROSS-ACCESS MAY  
35 ONLY BE REQUIRED IN COMMERCIAL DISTRICTS.]

36  
37                   [4.    *DOWNTOWN.* USES LOCATED IN DT DISTRICTS ARE  
38 EXEMPT FROM PROVIDING OFF-STREET PARKING  
39 SPACES. HOWEVER, IF PARKING IS PROVIDED, ALL  
40 OTHER STANDARDS OF THIS SECTION SHALL APPLY IN  
41 THE DT DISTRICTS, EXCEPT WHERE SPECIFICALLY  
42 STATED OTHERWISE. PARKING AGREEMENTS AND  
43 QUALIFYING SITE CRITERIA SHALL NOT BE REQUIRED  
44 FOR THIS EXEMPTION.

45  
46                   5.    *RESIDENCES IN WALKING DISTANCE TO DOWNTOWN.*

1 RESIDENTIAL HOUSEHOLD USES LOCATED NORTH OF  
2 15TH AVENUE, WEST OF ORCA STREET, EAST OF L  
3 STREET, AND SOUTH OF SHIP CREEK ARE ELIGIBLE  
4 FOR A REDUCTION OF UP TO 25 PERCENT OF THE  
5 MINIMUM NUMBER OF REQUIRED PARKING SPACES.  
6

7 6. *DISTRICTS THAT PROMOTE A MIX OF USES.*  
8

9 A. USES LOCATED IN THE R-3A AND R-4A DISTRICTS  
10 ARE ELIGIBLE FOR A REDUCTION OF UP TO 10  
11 PERCENT OF THE MINIMUM NUMBER OF  
12 REQUIRED PARKING SPACES.  
13

14 B. USES LOCATED IN THE B-1A DISTRICT ARE  
15 ELIGIBLE FOR A REDUCTION OF UP TO 10  
16 PERCENT OF THE MINIMUM NUMBER OF  
17 REQUIRED PARKING SPACES, IF THE B-1A  
18 DISTRICT ABUTS RESIDENTIAL DISTRICTS ON  
19 THE MAJORITY OF ITS PERIMETER, AND HAS A  
20 CONTIGUOUS AREA OF NO MORE THAN ONE  
21 ACRE, EXCLUDING RIGHTS-OF-WAY. IN  
22 ADDITION, CERTAIN DEVELOPMENTS IN THE B-1A  
23 DISTRICT ARE ELIGIBLE FOR A SEPARATE  
24 PARKING REDUCTION AS SPECIFIED IN THE  
25 MIXED-USE DEVELOPMENT OR OVERLAY  
26 DISTRICT STANDARDS OF CHAPTER 21.04. SUCH  
27 REDUCTIONS SHALL BE REVIEWED AND  
28 ADMINISTERED UNDER THIS SECTION 21.07.090F.  
29

30 C. CERTAIN DEVELOPMENTS IN THE B-1B AND B-3  
31 DISTRICTS ARE ELIGIBLE FOR A REDUCTION THE  
32 MINIMUM NUMBER OF REQUIRED PARKING  
33 SPACES, AS SPECIFIED IN THE MIXED-USE  
34 DEVELOPMENT STANDARDS OR OVERLAY  
35 DISTRICT STANDARDS OF CHAPTER 21.04. SUCH  
36 REDUCTIONS SHALL BE REVIEWED AND  
37 ADMINISTERED UNDER THIS SECTION  
38 21.07.090F.]  
39

40 7. *RESIDENCES IN CENTER CITY NEIGHBORHOODS.*  
41

42 A. RESIDENTIAL HOUSEHOLD USES LOCATED IN  
43 CENTER CITY NEIGHBORHOODS ARE ELIGIBLE  
44 FOR A REDUCTION OF UP TO 10 PERCENT OF  
45 THE MINIMUM NUMBER OF REQUIRED PARKING  
46 SPACES.

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B. FOR THE PURPOSES OF THIS PROVISION, THE CENTER CITY AREA IS BOUNDED TO THE NORTH BY JOINT BASE ELMENDORF-RICHARDSON, TO THE SOUTH BY TUDOR ROAD, TO THE EAST BY INGRA STREET AND THE SEWARD HIGHWAY, AND TO THE WEST BY MINNESOTA DRIVE. ANY PART OF FAIRVIEW, SOUTH ADDITION, GOVERNMENT HILL, OR MOUNTAIN VIEW COMMUNITY COUNCIL IS ALSO IN THE ELIGIBLE AREA.

C. THIS REDUCTION RECOGNIZES PROXIMITY TO EMPLOYMENT CENTERS, CHARACTERISTICS SUCH AS TRADITIONAL STREET GRIDS AND DEVELOPMENT PATTERNS, HOUSEHOLD CHARACTERISTICS, EMPHASIS ON WALKABLE NORTHERN CITY ENVIRONMENTS, AND LOWER PARKING DEMAND IN THESE AREAS.

8. *USES ADJACENT TO TRANSIT SERVICE.* A USE IS ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT OF THE MINIMUM NUMBER OF REQUIRED PARKING SPACES IF IT IS LOCATED WITHIN 800 FEET OF THE STREET RIGHT-OF-WAY CENTERLINE OF ANY MUNICIPAL PUBLIC TRANSIT ROUTE, SUBJECT TO APPROVAL BY THE TRAFFIC ENGINEER AND THE DIRECTOR. THE PUBLIC TRANSPORTATION DEPARTMENT MAY REQUIRE A PUBLIC USE EASEMENT OR TRANSIT STOP AND/OR TRANSIT SHELTER IMPROVEMENTS IF THE SUBJECT PROPERTY ABUTS AN EXISTING OR PLANNED TRANSIT STOP. IF THE PUBLIC TRANSPORTATION DEPARTMENT REQUIRES SUCH AN EASEMENT OR IMPROVEMENTS, THEN THE USE IS ELIGIBLE FOR AN ADDITIONAL REDUCTION OF TWO PERCENT OR ONE MORE PARKING SPACE, WHICHEVER IS GREATER.

9. *RIDESHARE PROGRAMS.* A NONRESIDENTIAL USE IS ELIGIBLE TO SUBSTITUTE PARTICIPATION IN MUNICIPAL CARPOOL OR VANPOOL RIDESHARE PROGRAM UP TO A MAXIMUM OF FIVE PERCENT OF THE MINIMUM NUMBER OF REQUIRED PARKING SPACES. [THE LAND AREA THAT WOULD OTHERWISE BE NEEDED IN ORDER TO PROVIDE THE REQUIRED NUMBER OF PARKING SPACES SHALL BE SET ASIDE ON

1 THE SITE TO PROVIDE FOR THE FUTURE  
2 CONSTRUCTION OF A PARKING LOT IN CONFORMANCE  
3 WITH SUBSECTION 21.07.090F.12., LAND BANKING.  
4

5 A. CARPOOL. EVERY DESIGNATED CARPOOL  
6 SPACE MAY COUNT AS 1.8 SPACES TOWARD  
7 MEETING THE MINIMUM NUMBER OF REQUIRED  
8 PARKING SPACES. THE CARPOOL SPACES SHALL  
9 BE THOSE CLOSEST TO THE PRIMARY  
10 ENTRANCE OR ELEVATOR, BUT NOT CLOSER  
11 THAN ACCESSIBLE SPACES OR THOSE SIGNED  
12 FOR EXCLUSIVE CUSTOMER/VISITOR USE.  
13 SIGNS SHALL BE POSTED INDICATING THESE  
14 SPACES ARE RESERVED FOR CARPOOL USE.  
15 THE TRAFFIC ENGINEER SHALL CONSULT WITH  
16 THE PUBLIC TRANSPORTATION DEPARTMENT IN  
17 PROVIDING CARPOOL SPACES AND THE  
18 LOCATION OF CARPOOL PARKING.  
19

20 B. VANPOOL. FOR EVERY VANPOOL PURCHASED  
21 OR LEASED BY THE APPLICANT FOR EMPLOYEE  
22 USE OPERATED THROUGH THE MUNICIPAL  
23 RIDESHARE PROGRAM, THE NUMBER OF  
24 REQUIRED PARKING SPACES SHALL BE  
25 REDUCED BY UP TO SIX SPACES. THE TRAFFIC  
26 ENGINEER MAY REQUIRE A SAFE AND  
27 CONVENIENT DESIGNATED VANPOOL  
28 PASSENGER LOADING ZONE.  
29

30 10. TRANSIT PASS BENEFITS. A USE IN WHICH THE OWNER  
31 OR EMPLOYER OFFERS TRANSIT PASSES COST-FREE  
32 TO ALL EMPLOYEES OR RESIDENTS IS ELIGIBLE FOR A  
33 PARKING REDUCTION OF UP TO 5 PERCENT OF THE  
34 MINIMUM NUMBER OF REQUIRED PARKING SPACES.  
35 THE USE SHALL BE LOCATED WITHIN 800 FEET OF THE  
36 STREET RIGHT-OF-WAY CENTERLINE OF ANY  
37 MUNICIPAL TRANSIT ROUTE. THE PUBLIC  
38 TRANSPORTATION DEPARTMENT MAY REQUIRE A  
39 PUBLIC USE EASEMENT OR TRANSIT STOP AND/OR  
40 TRANSIT SHELTER IMPROVEMENTS IF THE SUBJECT  
41 PROPERTY ABUTS AN EXISTING OR PLANNED TRANSIT  
42 STOP. IF THE PUBLIC TRANSPORTATION DEPARTMENT  
43 REQUIRES SUCH AN EASEMENT OR IMPROVEMENTS,  
44 THEN THE USE IS ELIGIBLE FOR AN ADDITIONAL  
45 REDUCTION OF TWO PERCENT OR ONE MORE  
46 PARKING SPACE, WHICHEVER IS GREATER.

1 11. *PARKING CASH-OUTS.* A USE IS ELIGIBLE FOR A  
2 REDUCTION OF UP TO 10 PERCENT OF THE MINIMUM  
3 NUMBER OF REQUIRED PARKING SPACES IF IT  
4 IMPLEMENTS A PARKING CASH-OUT PROGRAM BY  
5 WHICH COMMUTERS ARE PROVIDED THE OPTION TO  
6 CHOOSE BETWEEN FREE PARKING AND ITS  
7 EQUIVALENT CASH VALUE FOR USING AN  
8 ALTERNATIVE MODE OF TRAVEL.  
9

10 12. *LAND BANKING.* SUBJECT TO APPROVAL BY THE  
11 TRAFFIC ENGINEER AND THE DIRECTOR, THE LAND  
12 AREA THAT WOULD OTHERWISE BE NEEDED IN ORDER  
13 TO PROVIDE UP TO 25 PERCENT OF THE MINIMUM  
14 NUMBER OF REQUIRED PARKING SPACES MAY BE SET  
15 ASIDE ON THE SITE TO PROVIDE FOR THE FUTURE  
16 CONSTRUCTION OF A PARKING FACILITY. THE  
17 APPLICANT SHALL SUBMIT A PARKING DEMAND STUDY  
18 PREPARED IN A FORM AND MANNER PRESCRIBED BY  
19 THE TRAFFIC ENGINEER THAT INDICATES THE  
20 REDUCED PARKING LOT WILL ACCOMMODATE  
21 EXPECTED PARKING NEEDS, AND AN ALTERNATE SITE  
22 PLAN TO BE APPROVED BY THE TRAFFIC ENGINEER  
23 THAT ACCOMMODATES THE PARKING THAT WOULD BE  
24 REQUIRED WITHOUT THE LAND BANKED PARKING  
25 REDUCTION. THE AREA SET ASIDE SHALL BE  
26 LANDSCAPED WITH SITE ENHANCEMENT  
27 LANDSCAPING AND/OR PEDESTRIAN AMENITIES  
28 APPROVED BY THE DIRECTOR. THE PARKING  
29 AGREEMENT SHALL GUARANTEE THAT, IF THE  
30 DIRECTOR AND THE TRAFFIC ENGINEER DETERMINE  
31 AT SOME POINT IN THE FUTURE THAT ADDITIONAL  
32 PARKING SPACES ARE NEEDED, THE OWNER SHALL  
33 CONSTRUCT PARKING ON THE LAND BANKED AREA IN  
34 CONFORMANCE WITH THE ALTERNATE SITE PLAN.  
35

36 13. *AFFORDABLE HOUSING.* AFFORDABLE HOUSING UNITS  
37 THAT ARE DEED-RESTRICTED FOR HOUSEHOLDS  
38 HAVING AN INCOME AT THE TIME OF INITIAL  
39 OCCUPANCY OF 30 PERCENT OR LESS OF MEDIAN  
40 FAMILY INCOME ARE ELIGIBLE FOR A REDUCTION OF  
41 UP TO 30 PERCENT OF THE MINIMUM NUMBER OF  
42 REQUIRED PARKING SPACES. AFFORDABLE HOUSING  
43 UNITS FOR LOW INCOME HOUSEHOLDS HAVING AN  
44 INCOME AT THE TIME OF INITIAL OCCUPANCY OF 60  
45 PERCENT OR LESS OF MEDIAN FAMILY INCOME ARE  
46 ELIGIBLE FOR A REDUCTION OF UP TO 15 OF THE



1 MINIMUM NUMBER OF REQUIRED PARKING SPACES.  
2 THE AFFORDABLE HOUSING UNITS SHALL BE  
3 CONSISTENT WITH THE STANDARDS OF SUBSECTION  
4 21.07.110H., STANDARDS FOR AFFORDABLE HOUSING.  
5

6 14. *SENIOR HOUSING.* DWELLING UNITS THAT MEET THE  
7 DEFINITION OF SENIOR HOUSING ARE ELIGIBLE FOR A  
8 REDUCTION OF UP TO 15 PERCENT OF THE MINIMUM  
9 NUMBER OF REQUIRED PARKING SPACES. DWELLING  
10 UNITS THAT MEET THE DEFINITION OF SENIOR  
11 HOUSING THAT IS INTENDED FOR, AND SOLELY  
12 OCCUPIED BY, PERSONS 62 YEARS OF AGE OR OLDER  
13 ARE ELIGIBLE FOR A REDUCTION OF UP TO 25  
14 PERCENT OF THE MINIMUM NUMBER OF REQUIRED  
15 PARKING SPACES.  
16

17 15. *HOUSING DENSITY.* RESIDENTIAL HOUSEHOLD USES  
18 ARE ELIGIBLE FOR A REDUCTION OF ONE PERCENT OF  
19 THE MINIMUM NUMBER OF REQUIRED PARKING  
20 SPACES FOR EVERY FOUR DWELLINGS PER ACRE  
21 ABOVE A NET DENSITY OF 40 DWELLINGS PER ACRE  
22 ON THE SITE, UP TO A MAXIMUM REDUCTION OF 20  
23 PERCENT OF THE MINIMUM NUMBER OF REQUIRED  
24 PARKING SPACES.]  
25

26 5[16]. *Parking Reduction for Shared Parking.* Where two or more  
27 land uses that have different peak parking utilization time  
28 periods share the same parking facility, the total off-street  
29 parking required for those uses may be reduced, as provided  
30 below.[SHARED USE OF REQUIRED PARKING SPACES  
31 MAY OCCUR WHERE TWO OR MORE USES ON THE  
32 SAME OR SEPARATE SITES ARE ABLE TO SHARE THE  
33 SAME PARKING SPACES BECAUSE THEIR PEAK  
34 PARKING DEMANDS OCCUR AT DIFFERENT TIMES THE  
35 TRAFFIC ENGINEER AND DIRECTOR MAY APPROVE  
36 SHARED PARKING FACILITIES FOR USES WITH  
37 DIFFERENT PEAK BUSINESS PERIODS IF THE SHARED  
38 PARKING COMPLIES WITH ALL OF THE FOLLOWING  
39 STANDARDS:]  
40

41 a. *Shared Parking Standard Calculation*[STUDY]. Where  
42 up to three separate land uses listed in Table 21.07-10,  
43 *Shared Parking Credit*, share a parking facility, the total  
44 off-street parking required for those uses is eligible to  
45 be reduced by the percentage factors shown in  
46 subsection b., Table 21.07-10, subject to the standards

1 in subsections d. through j. Where four or more land  
2 uses listed in Table 21.07-10 share a parking facility,  
3 the shared parking reduction is subject to discretionary  
4 review and approval by the Traffic Engineer and  
5 Director as provided in 21.07.090F.8. [THE  
6 APPLICANT SHALL SUBMIT A SHARED PARKING  
7 STUDY TO THE DIRECTOR THAT DEMONSTRATES  
8 THE FEASIBILITY OF SHARED PARKING. THE  
9 STUDY SHALL BE PROVIDED IN A FORM  
10 ESTABLISHED BY THE TRAFFIC ENGINEER AND  
11 SHALL BE MADE AVAILABLE TO THE PUBLIC. THE  
12 STUDY SHALL DEMONSTRATE THAT ANY  
13 PARKING REDUCTION REQUESTED WILL NOT  
14 RESULT IN THE SPILLOVER OF PARKING ONTO  
15 OTHER PROPERTIES OR PUBLIC STREETS, BY,  
16 AT A MINIMUM, ADDRESSING THE FOLLOWING:  
17 THE SIZE AND TYPE OF THE PROPOSED  
18 DEVELOPMENT AND THE COMPOSITION OF  
19 USES, LOCATION OF REQUIRED PARKING, THE  
20 COMPOSITION OF TENANTS, THE ANTICIPATED  
21 RATE OF PARKING TURNOVER, AND THE  
22 ANTICIPATED PEAK PARKING AND TRAFFIC  
23 LOADS FOR ALL USES THAT WILL BE SHARING  
24 OFF-STREET PARKING SPACES.]

25  
26 [B. *CALCULATION OF PARKING SPACES REQUIRED.*  
27 THE SHARED PARKING STUDY SHALL ONE OF  
28 THE FOLLOWING PROCEDURES:

- 29  
30 I. THE METHOD UNDER SUBSECTION 16.C.;
- 31  
32 II. THE MOST CURRENT PUBLISHED  
33 PROCEDURES OF THE URBAN LAND  
34 INSTITUTE OR THE INSTITUTE OF  
35 TRANSPORTATION ENGINEERS; OR
- 36  
37 III. OTHER PROCEDURES BASED ON  
38 INDUSTRY DATA OR OTHER SUFFICIENT  
39 EVIDENCE AND ANALYSIS OF PEAK  
40 PARKING DEMAND, AS SPECIFICALLY  
41 APPROVED BY THE TRAFFIC ENGINEER.]

42  
43 b[C]. *Computation of Shared Parking Standard*  
44 [ALTERNATIVE] *Calculation Method.* The following  
45 steps shall be used to calculate a shared parking  
46 reduction. The Planning Department shall maintain a

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publicly available worksheet form online that applicants may access and use to run calculations. Calculation steps:

- i. Determine the minimum amount of parking required for each use, as set forth in Section 21.07.090E., *Off-street Parking Requirements*, using the calculation rules established in 21.07.090C., *Computation of Parking and Loading Requirements*.
- ii. For each use select the appropriate matching land use category in table 21.07-10, *Shared Parking Credits*.
- iii. Multiply the minimum amount of required parking for each use, as set forth in Section 21.07.090E., by the appropriate percentage shown in table 21.07-10, for each of the eight time periods in the table, to estimate the typical parking demand generated by that use during each of the eight time periods.
- iv. Add the resulting products from the uses for each of the eight columns (time periods). Include each time period shown in the table, including during hours when the proposed business will be closed.
- v. The highest sum among the eight columns becomes the shared parking requirement. This represents the time period that is expected to generate the highest total parking demand.  
[FOR EACH USE SHARING THE PARKING FACILITY, CALCULATE THE NUMBER OF OFF-STREET PARKING SPACES REQUIRED FOR THAT USE IN TABLE 21.07-4. MULTIPLY THAT NUMBER ACROSS THE ROW FOR ITS LAND USE IN TABLE 21.07-5, SHARED PARKING CREDIT REQUIRED FOR THAT USE DURING THE EIGHT TIME PERIODS. FOR EACH TIME PERIOD, ADD THE RESULTING PRODUCTS FOR EACH OF THE USES SHARING THE PARKING. THE COLUMN TOTAL THAT GENERATES THE HIGHEST NUMBER OF PARKING SPACES

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THEN BECOMES THE SHARED PARKING REQUIREMENT. THIS REPRESENTS THE TIME PERIOD WITH THE HIGHEST TOTAL PARKING DEMAND.]

**TABLE 21.07-10[5]: SHARED PARKING CREDIT**

Land Uses <sup>1[12]</sup>	Weekday Time Periods				Weekend Time Periods			
	7 am to 6 pm	6 pm to 1 am	1 am to 3 am	3 am to 7 am	7 am to 6 pm	6 pm to 1 am	1 am to 3 am	3 am to 7 am
Residential	65%	100%	100%	100%	75%	90%	10%	100%
Religious assembly	25%	50%	0%	0%	100%	50%	0%	0%
Childcare; K-12 school	100%	20%	0%	0%	20%	10%	0%	0%
Health services	100%	30%	5%	5%	100%	0%	0%	0%
Assembly	100%	50%	5%	5%	100%	50%	5%	5%
Fitness center	90%	100%	25[60] %	60%	100%	100%	25[80] %	60[80] %
Movie theater	60%	100%	0%	0%	80%	100%	0%	0%
Bar or nightclub	40%	100%	90%	0%	50%	100%	90%	0%
Restaurant	80%	100%	50%	50%	85%	100%	25%	25%
Restaurant – drive-through	100%	90%	15%	15%	100%	80%	15%	15%
Office or financial; or Government administr.	100%	10%	0%	5%	15%	0%	0%	0%
Retail sales [/]; Personal services	100%	80%	0%	0%	100%	60%	0%	0%
Visitor accommodations (guest rooms)	75%	100%	100%	100%	75%	100%	100%	100%
Industrial service, manufacturing, or warehouse/storage	100%	10%	0%	5%	15%	0%	0%	0%

**NOTES:** <sup>1[12]</sup> If one or more of the land uses proposed to make use of shared parking facilities do not conform to the land use classifications in this table, as determined by the director, then the applicant shall submit sufficient data to indicate the periods of peak parking demand for the uses. Based on this information, the traffic engineer shall determine the appropriate shared parking requirement.

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- c. Alternative Shared Parking Calculation. The applicant may request a greater reduction in the total number of spaces required for two or more land uses where Table 21.07-10 does not adequately account for circumstances or mix of use types specific to the development, subject to review and approval by the traffic engineer and director as provided in 21.07.090F.8. The applicant shall submit a shared parking study following 21.07.090F.8.c. and:
- i. The most current published procedures of the Urban Land Institute or the Institute of Transportation Engineers; or

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ii. Other procedures based on industry data or other sufficient evidence and analysis of peak parking demand, as specifically approved by the traffic engineer.

d. *Distance to Parking Spaces.* Shared parking spaces may be located off-site, subject to the standards in this subsection 21.07.090F.5. Shared parking spaces for residential units shall be located within 500 feet of the dwelling unit entrance they serve. Shared spaces for other uses shall be within 800 feet of a primary entrance of the uses served. Distance shall be measured along the pedestrian connection in e. [THE TRAFFIC ENGINEER AND THE DIRECTOR MAY APPROVE A PORTION OF SHARED PARKING SPACES AT A GREATER DISTANCE BASED ON FACTORS SUCH AS THE PEDESTRIAN ENVIRONMENT, AVAILABILITY OF ATTENDANT PARKING, WEATHER PROTECTION, AND THE TYPE OF USE SERVED.]

e. *Pedestrian Connection.* Clear and safe pedestrian walkways conforming to the standards of 21.07.060E. shall connect the shared parking facility and the primary entrances of the uses it serves. The traffic engineer may require sidewalk or pedestrian street crossing improvements.

f. *Separation by Streets.* Separation of a use and its shared parking facility by a local street is allowed, subject to discretionary review and approval by the traffic engineer in 21.07.090F.8. Shared parking spaces shall not be separated from the served use by a collector or greater classification street, unless approved by the traffic engineer with consideration of the ease and safety of pedestrian access, or as [SPECIFICALLY ]allowed by a comprehensive plan element specific to an area or district.

g. *Residential Neighborhoods.* A nonresidential use shall not participate in a shared parking facility [THAT IS] located in a residential district, if the use [ITSELF] is not permitted in the residential district.

h. *Instructional Signs.* The shared parking facility shall provide instructional signs on the premises indicating

1 the availability of the facility for patrons of the uses it  
2 serves.

3  
4 i. *Shared Parking Plan.* A shared parking plan shall be  
5 submitted for review and approval [BY THE TRAFFIC  
6 ENGINEER AND THE DIRECTOR]. The shared  
7 parking plan may be combined with other parking plans  
8 required by this title.

9  
10 j. *Changes in Use or Shared Parking Facility.* Any  
11 subsequent change to the shared parking facility or  
12 any[IN] use type in the building(s) shall require a review  
13 [BY THE DEPARTMENT AND THE TRAFFIC  
14 ENGINEER] for compliance with this section, including  
15 proof that sufficient parking will be available. Any  
16 change shall be approved and if necessary a  
17 modification to the existing shared parking agreement  
18 shall be made prior to the change being implemented.

19  
20 [K. *EXPIRATION.* NOTWITHSTANDING F.1.A. ABOVE,  
21 A SHARED PARKING AGREEMENT MAY BE  
22 RECORDED FOR A TIME CERTAIN PERIOD, NOT  
23 TO BE LESS THAN TEN YEARS. AT THE END OF  
24 THE LIFE OF THE AGREEMENT, PROPERTY  
25 OWNERS WHO ARE PARTIES TO THE  
26 AGREEMENT SHALL COMPLY WITH ALL  
27 PROVISIONS OF THIS CODE GOVERNING THE  
28 REQUIRED NUMBER OF OFF-STREET PARKING  
29 SPACES.]

30  
31 6[17]. *Parking reductions for off-site parking.* A use's required  
32 parking may be located on a lot different from the lot  
33 containing the use, as provided below. [THE TRAFFIC  
34 ENGINEER AND THE DIRECTOR MAY APPROVE THE  
35 LOCATION OF REQUIRED PARKING SPACES ON A  
36 SEPARATE LOT FROM THE PRINCIPAL USE IF THE OFF-  
37 SITE PARKING COMPLIES WITH ALL OF THE  
38 FOLLOWING STANDARDS:]

39  
40 a. *Off-site Parking – Nondiscretionary Approval*  
41 [ACCESSIBLE PARKING SPACES]. A principal use is  
42 eligible for a non-discretionary parking reduction for off-  
43 site parking, provided the off-site required parking for  
44 the use is off-street, located on a lot not separated by  
45 a street from the principal use and in conformance with  
46 the standards below. [REQUIRED ACCESSIBLE

PARKING SPACES SHALL NOT BE LOCATED OFF-SITE.]

b. Distance to Off-site Spaces[LOCATION]. Off-site parking spaces for residential units shall be located within 500 feet of the dwelling unit entrance they serve. Off-site spaces for other uses shall be within 800 feet of a primary entrance of the uses served. Distance shall be measured along the pedestrian connection in c [THE MAXIMUM DISTANCE BETWEEN OFF-SITE PARKING SPACES AND THE USE(S) SERVED SHALL BE THE SAME AS PROVIDED IN SUBSECTION 21.07.090F.16.D. FOR SHARING PARKING SPACES (MEASURED ALONG THE SHORTEST LEGAL PEDESTRIAN ROUTE). SEPARATION OF A USE AND ITS OFF-SITE PARKING SPACES BY A STREET SHALL BE SUBJECT TO SUBSECTION 21.07.090F.16.F].

c. Pedestrian Connection. Clear and safe pedestrian walkways conforming to the standards of 21.07.060E. shall connect the off-site parking facility and the primary entrance(s) of the uses served. The traffic engineer may require sidewalk or pedestrian crossing improvements to enhance pedestrian safety or mobility to and from the off-site parking.

d. Separation by Streets. Separation of a use and its off-site parking spaces by a local street is allowed, subject to discretionary review and approval by the traffic engineer in 21.07.090F.8. Off-site parking spaces shall not be separated from the served use by a collector or greater classification street, unless approved by the traffic engineer with consideration of the ease and safety of pedestrian access, or as specifically allowed by an area-specific element of the comprehensive plan.

e[D]. Instructional Signs. Instructional signs shall be posted on the principal site providing notice of the availability and location of additional parking. The off-site parking facility shall provide instructional signs indicating the availability of the facility for patrons of the uses it serves.

1 [E]. *Residential Neighborhoods.* A nonresidential use shall  
2 not participate in an off-site parking facility [THAT IS  
3 ]located in a residential district, if the use [ITSELF ]is  
4 not permitted in the residential district.  
5

6 g. *Required Off-site Spaces to be Counted for One Use*  
7 *Only.* The off-site vehicle parking spaces shall not be  
8 required parking spaces for any other use, except as  
9 provided in 21.07.090F.5. (Shared Parking) or F.7.  
10 (District Parking).  
11

12 7[18]. *Parking Reduction for District Parking.* The traffic engineer  
13 may reduce the minimum number of required off-street  
14 parking spaces for uses within the boundaries of a municipally  
15 recognized [PUBLIC ]parking district (as defined in 21.15.040)  
16 that provides off-site parking facilities to serve an area. To  
17 determine eligibility for this reduction or the size of the  
18 reduction to be allowed, the traffic engineer shall consider the  
19 relative distance to the use from the district parking facility and  
20 the factors listed in 21.07.090F.8.b. [SUCH AS:  
21

- 22 A. PEAK HOURS OF USE AND TURNOVER RATE;
- 23
- 24 B. THE ABILITY OF THE USE TO MEET THE PARKING
- 25 REQUIREMENT THROUGH OTHER MEANS;
- 26
- 27 C. THE AVAILABILITY OF SPACES IN THE NEARBY
- 28 DISTRICT PARKING FACILITY;
- 29
- 30 D. THE RELATIVE DISTANCE TO THE USE FROM THE
- 31 DISTRICT PARKING FACILITY; AND
- 32
- 33 E. MEASURES PROVIDED BY THE APPLICANT TO
- 34 ENSURE EMPLOYEE AND PATRON USE OF THE
- 35 DISTRICT PARKING FACILITY, AND EASE AND
- 36 SAFETY OF PEDESTRIAN ACCESS.
- 37

38 9. *Discretionary Parking Reductions.* Subject to discretionary  
39 review and approval by the traffic engineer and director,  
40 applicants may request greater percentage reductions than  
41 shown in Table 21.07-9 and subsections F.5. through F.8.,  
42 propose other types of parking reduction strategies besides  
43 those listed, or request departures from the specific standards  
44 for the parking reductions in F.5. through F.8. The applicant  
45 shall demonstrate the proposed reduction is appropriate



1                   based on the expected parking utilization rate of the  
2                   development and the factors below.

3  
4                   a.    *Examples of Discretionary Parking Reductions.*  
5                   Examples of other parking reduction strategies besides  
6                   those listed that may be proposed include other shared  
7                   vehicle programs (e.g., bike-share), other parking  
8                   pricing strategies, other uses that have low parking  
9                   utilization, or other transportation demand  
10                  management (TDM) programs.

11  
12                  b.    *Discretionary Approval Criteria.* To determine eligibility  
13                  for a discretionary reduction or the size of the reduction  
14                  to be allowed, the traffic engineer and director shall  
15                  consider factors such as:

16  
17                  i.    The characteristics of the proposed use, travel  
18                  behavior and anticipated peak parking  
19                  utilization by users, peak hours of use, and  
20                  parking turnover rate;

21  
22                  ii.   The ability of the use to meet the parking  
23                  requirement through other means, such as  
24                  existing or potential shared parking agreements  
25                  or other parking strategies;

26  
27                  iii.   Availability, proximity, and accessibility of  
28                  alternative parking, such as any proposed  
29                  shared, off-site, or district parking;

30  
31                  iv.   Impacts from users and employees on adjacent  
32                  neighborhoods, properties, and streets;

33  
34                  v.    Strategies provided by the applicant to ensure  
35                  employee and patron use of any shared, off-site,  
36                  or district parking or parking demand  
37                  management program;

38  
39                  vi.   The surrounding area's parking needs and  
40                  parking availability; and

41  
42                  vii.   The surrounding area's availability of pedestrian  
43                  facilities and alternative modes of  
44                  transportation.

1                    c. *Parking Demand Study.* The traffic engineer may  
2                    require the applicant to provide a parking demand  
3                    study for discretionary reductions. The parking demand  
4                    study shall be prepared in a form and manner  
5                    prescribed by the traffic engineer, and meet the  
6                    following standards:

7  
8                    i. The parking study shall demonstrate that any  
9                    parking reduction requested will not result in  
10                    users parking on surrounding properties or  
11                    streets;

12  
13                    ii. The parking study shall demonstrate that the  
14                    drivers accessing the use will be adequately  
15                    served by the proposed parking due to project  
16                    location, transportation characteristics of the  
17                    persons residing, working, or visiting the site;  
18                    and

19  
20                    iii. The parking study shall consider the type,  
21                    intensity, and characteristics of each use and  
22                    projected peak parking and traffic demand of  
23                    drivers, including the use's hours of operation.

24  
25                    [19. *ON-STREET PARKING.* IF APPROVED BY THE TRAFFIC  
26                    ENGINEER, ON-STREET PARKING SPACES IN THE  
27                    STREET OR RIGHT-OF-WAY ABUTTING THE FRONTAGE  
28                    OF THE SITE MAY BE COUNTED TOWARD THE MINIMUM  
29                    REQUIRED NUMBER OF OFF-STREET PARKING  
30                    SPACES, INCLUDING GUEST PARKING SPACES. IN  
31                    ADDITION, AS DETERMINED BY THE TRAFFIC  
32                    ENGINEER, A PORTION OF THE REMAINING ON-STREET  
33                    PARKING SPACES LOCATED WITHIN THE MAXIMUM  
34                    DISTANCE PROVIDED IN SUBSECTION 21.07.090F.16.D.  
35                    FOR SHARED PARKING SPACES MAY BE COUNTED  
36                    TOWARD THE MINIMUM REQUIRED OFF-STREET  
37                    PARKING SPACES, IN AN AMOUNT CONSISTENT WITH A  
38                    FAIR APPORTIONMENT OF ON-STREET PARKING  
39                    SPACES AMONG THE PROPERTIES ON THE STREET.  
40                    UPON APPROVAL, EACH ON-STREET SPACE MAY BE  
41                    SUBSTITUTED FOR ONE REQUIRED OFF-STREET  
42                    SPACE. THE PROVISIONS APPLY ONLY TO STREET  
43                    FRONTAGES WHERE ON-STREET PARKING IS  
44                    ALLOWED. DETERMINATION OF THE LOCATION AND  
45                    DIMENSIONS OF ON-STREET PARKING SPACES TO BE  
46                    COUNTED TOWARD THE PARKING REQUIREMENT

1 SHALL BE THE AUTHORITY OF THE TRAFFIC ENGINEER  
2 BASED ON A REVIEW OF THE SITUATION. THE STREET  
3 CURB NEXT TO ON-STREET PARKING SPACES SHALL  
4 BE A VERTICAL CURB (NOT A ROLLED CURB), AND A  
5 SIDEWALK SHALL EXTEND THE FULL LENGTH OF THE  
6 SUBJECT PROPERTY.  
7

8 20. *STACKED AND TANDEM PARKING.*  
9

10 A. *NONRESIDENTIAL USES.* STACKED AND TANDEM  
11 PARKING SPACES FOR NONRESIDENTIAL USES  
12 ARE ALLOWED TO COUNT TOWARD THE  
13 MINIMUM NUMBER OF REQUIRED SPACES IF THE  
14 OWNER ENSURES THROUGH THE PARKING  
15 AGREEMENT THAT ATTENDANT PARKING IS  
16 PROVIDED FOR SUCH SPACES. AN ACCESSIBLE  
17 PASSENGER LOADING ZONE SHALL BE  
18 PROVIDED WITH ATTENDANT PARKING  
19 SERVICES AT OR NEAR A PRIMARY ENTRANCE.  
20 AVAILABILITY OF THIS SERVICE SHALL BE  
21 CONSPICUOUSLY POSTED INSIDE AND OUTSIDE  
22 THE PRIMARY ENTRANCE. THE TRAFFIC  
23 ENGINEER MAY WAIVE THE PARKING  
24 ATTENDANT REQUIREMENT FOR AUTOMATED  
25 PARKING STRUCTURES.  
26

27 B. *RESIDENTIAL USES.* TWO REQUIRED PARKING  
28 SPACES FOR ANY RESIDENTIAL DWELLING MAY  
29 BE ARRANGED IN TANDEM OR STACKED ONE  
30 ABOVE THE OTHER USING A CAR STACKER, SO  
31 LONG AS PARKING REQUIRED FOR THE  
32 DWELLING UNIT IS ARRANGED INDEPENDENTLY  
33 FROM PARKING SERVING ANY OTHER DWELLING  
34 UNIT, WITH UNOBSTRUCTED VEHICLE ACCESS  
35 FOR AT LEAST ONE OF THE SPACES REQUIRED  
36 FOR EACH DWELLING UNIT, AND THE OWNER  
37 ASSIGNS THE TWO SPACES TOWARD THE SAME  
38 DWELLING AND ENFORCES THEIR ASSIGNED  
39 USE.  
40

41 21. *SMALLER PARKING SPACES FOR PARKING*  
42 *STRUCTURES AND LOW-TURNOVER USES.* IF  
43 APPROVED BY THE TRAFFIC ENGINEER, UP TO 20  
44 PERCENT OF THE TOTAL NUMBER OF REQUIRED  
45 PARKING SPACES LOCATED IN A PARKING STRUCTURE  
46 AND/OR DESIGNATED FOR EMPLOYEE OR RESIDENT

1 PARKING ONLY MAY BE EIGHT FEET SIX INCHES WIDE,  
2 SUBJECT TO THE REQUIREMENTS OF TABLE 21.07-7,  
3 PARKING SPACE AND AISLE DIMENSIONS. SUCH  
4 SPACES SHALL BE SIGNED FOR EMPLOYEE OR  
5 RESIDENT PARKING ONLY.  
6

7 22. *BICYCLE PARKING.* A USE IS ELIGIBLE TO  
8 PERMANENTLY OR SEASONALLY SUBSTITUTE BICYCLE  
9 PARKING SPACES REQUIRED AUTOMOBILE PARKING  
10 SPACES. EACH AUTOMOBILE PARKING SPACE SHALL  
11 BE REPLACED BY A MINIMUM OF SIX BICYCLE PARKING  
12 SPACES NOT REQUIRED BY THIS TITLE. BICYCLE  
13 PARKING SPACES SHALL COMPLY WITH THE  
14 STANDARDS OF SUBSECTION 21.07.060F.15. AND BE  
15 SEPARATED FROM MOTOR VEHICLE AREAS BY  
16 BOLLARDS OR OTHER PHYSICAL BUFFER APPROVED  
17 BY THE TRAFFIC ENGINEER.  
18

19 23. *OTHER ELIGIBLE REDUCTIONS OR ALTERNATIVES.* THE  
20 TRAFFIC ENGINEER AND THE DIRECTOR MAY  
21 APPROVE ANY PARKING REDUCTION OR OTHER  
22 ALTERNATIVE IN ADDITION TO THE CHOICES ABOVE,  
23 OR THAT INCREASES THE PERCENTAGE REDUCTION IN  
24 ANY OF THE CHOICES ABOVE, IF THE APPLICANT  
25 DEMONSTRATES TO THE SATISFACTION OF THE  
26 TRAFFIC ENGINEER AND THE DIRECTOR THAT THE  
27 PROPOSED PARKING MANAGEMENT STRATEGY WILL  
28 PROTECT SURROUNDING NEIGHBORHOODS, AND  
29 MAINTAIN TRAFFIC CIRCULATION PATTERNS AT LEAST  
30 THE SAME EXTENT AS WOULD STRICT COMPLIANCE  
31 WITH OTHERWISE APPLICABLE OFF-STREET PARKING  
32 STANDARDS. ADDITIONAL PARKING MANAGEMENT  
33 STRATEGIES MAY INCLUDE, FOR EXAMPLE,  
34 TRANSPORTATION DEMAND PROGRAMS, CAR  
35 SHARING, UNBUNDLED PARKING, OR A COMBINATION  
36 OF STRATEGIES. THE APPLICANT SHALL PROVIDE A  
37 PARKING DEMAND STUDY PREPARED IN A FORM AND  
38 MANNER PRESCRIBED BY THE TRAFFIC ENGINEER  
39 THAT DEMONSTRATES A REDUCTION IS APPROPRIATE  
40 BASED ON THE EXPECTED PARKING NEEDS OF THE  
41 DEVELOPMENT, AVAILABILITY OF TRANSIT, AND  
42 SIMILAR FACTORS. IT SHALL BE DETERMINED THAT:  
43

44 A. THE USE WILL BE ADEQUATELY SERVED BY THE  
45 PROPOSED PARKING DUE TO PROJECT  
46 LOCATION, TRANSPORTATION

1 CHARACTERISTICS OF THE PERSONS RESIDING,  
2 WORKING, OR VISITING THE SITE, OR BECAUSE  
3 THE APPLICANT HAS UNDERTAKEN A PROGRAM  
4 OR STRATEGY THAT WILL REDUCE PARKING  
5 DEMAND AT THE SITE; AND  
6

7 B. PARKING DEMAND GENERATED BY THE  
8 PROJECT WILL NOT EXCEED THE CAPACITY OF  
9 OR HAVE A DETRIMENTAL IMPACT ON THE  
10 SUPPLY OF ON-STREET PARKING IN THE  
11 SURROUNDING AREA.]

12 \*\*\* \*\*

13 (Note to code revisor: Re-number existing table 21.07-6 to 21.07-11.)

14  
15 H. *Parking and loading facility design standards.*

16 \*\*\* \*\*

17 8. *Vehicular Access and Circulation – General.* Parking lots and  
18 structures access, layout, and dimensions shall be designed  
19 for a safe and orderly flow of traffic throughout the site, as  
20 provided in [THE] subsections 8. through 11. that follow.

21  
22 a. [KEY ELEMENTS. THE PARKING FACILITY  
23 LAYOUT, CIRCULATION, AND DESIGN PLAN  
24 SHALL ADDRESS THE FOLLOWING ELEMENTS AS  
25 THEY RELATE TO PARKING LOTS, INCLUDING  
26 BUT NOT LIMITED TO: FIRE LANES, EMERGENCY  
27 ACCESS, DRIVE-THROUGHS, QUEUING SPACES,  
28 PASSENGER LOADING ZONES, PEDESTRIAN  
29 CIRCULATION, AND LOADING BERTHS.]

30  
31 [B.] *Circulation Patterns.* Internal circulation patterns and  
32 the location and traffic direction of all circulation aisles,  
33 driveways, and queuing lanes shall be designed and  
34 maintained in accordance with the municipal driveway  
35 standards currently established by the traffic engineer,  
36 and with accepted principles of traffic engineering and  
37 safety, per the traffic engineer's review based on the  
38 current manuals of the Institute of Transportation  
39 Engineers and the Urban Land Institute, and the  
40 *Manual of Uniform Traffic Control Devices* or the  
41 successor documents. Circulation patterns within  
42 parking facilities shall be well defined with pavement  
43 marking and signage, curbs, landscaping, landscaped  
44 islands, and/or other similar features. [IN ORDER TO  
45 DEFINE CIRCULATION AND PROVIDE BETTER  
46 SIGHT DISTANCE, CURBED END ISLANDS SHALL

1 BE REQUIRED AT THE END OF EACH ROW OF  
2 PARKING SPACES. WHERE LOADING FACILITIES  
3 OR ON-SITE REFUSE COLLECTION ARE  
4 PROVIDED, COMMERCIAL TRUCK CIRCULATION  
5 SHALL BE CONSIDERED, AND TRUCK TURNING  
6 RADII SHALL BE SHOWN ON THE PARKING  
7 FACILITY LAYOUT, CIRCULATION, AND DESIGN  
8 PLAN WHEN REQUIRED BY THE TRAFFIC  
9 ENGINEER.]

10  
11 b. Administrative Adjustments and Exceptions. The traffic  
12 engineer and director may approve modifications from  
13 the standards of this section, provided documentation  
14 prepared by a licensed professional in the state of  
15 Alaska registered with the Alaska State Board of  
16 Registration for Architects, Engineers and Land  
17 Surveyors demonstrates the parking facility or  
18 driveway meets the intent of this section and the  
19 municipal driveway standards and that the change is  
20 appropriate. Approval shall be contingent on factors  
21 such as street classification, street typology, urban  
22 context, traffic volume and speed, curb return radii,  
23 street travel lane offset from face of curb, pedestrian  
24 and bicycle facilities, snow storage, driveway  
25 configuration and length, site and project  
26 characteristics, number of vehicles expected to use the  
27 driveway, and comprehensive plan policies.

28  
29 [C. PARKING SPACES ALONG MAJOR SITE  
30 ENTRANCE DRIVES. THE PROVISION, LOCATION,  
31 DESIGN, AND DIMENSIONS OF PARKING SPACES  
32 ON A MAJOR ACCESS DRIVEWAY THAT SERVES  
33 AS AN ENTRY OR EXIT FOR A LARGE  
34 ESTABLISHMENT WITH MULTIPLE LOTS, TRACTS,  
35 OR BUSINESSES, SHALL CONFORM TO  
36 MUNICIPAL STANDARDS FOR ON-STREET  
37 PARKING AND BE SUBJECT TO REVIEW AND  
38 APPROVAL BY THE TRAFFIC ENGINEER.]

39  
40 9. Vehicular Access and Parking Location

41  
42 a. Street Access Location. The number and spacing of  
43 driveways, including minimum distance from street  
44 intersections, shall be as provided in the *Municipal*  
45 *Driveways Standards* and this section. Access to  
46 streets owned by the state of Alaska requires

1 compliance with state driveway standards, department  
2 of transportation and public facilities approval and  
3 driveway permit.

4  
5 b. *Alley Access Requirement (in Urban Neighborhood*  
6 *Context Areas).* Where a residential use is served by  
7 an alley in a Traditional Urban or Edge Urban  
8 Neighborhood Context (21.07.015), direct vehicle  
9 access to the street shall be limited to the following:

10  
11 i. Driveway access to the secondary street  
12 frontage on corner lots, provided the driveway is  
13 not located within the primary front setback;

14  
15 ii. Residential driveway access to the primary  
16 street frontage not exceeding one driveway no  
17 wider at any point that 12 feet, except that lot  
18 frontages 100 feet or wider may have one  
19 driveway per 50 feet of lot frontage.

20  
21 iii. Additional driveway access to the primary street  
22 frontage for multifamily or townhouse  
23 developments may be allowed if the alley is  
24 unimproved, unmaintained, or physically  
25 inaccessible, and the traffic engineer  
26 determines that improvement and vehicle  
27 access is not feasible, or that use of the alley for  
28 access would create a substantial traffic impact  
29 or safety hazard. The additional driveway  
30 access shall be the minimum the traffic engineer  
31 determines is necessary to provide access for  
32 the development.

33  
34 c. *Cross-Access and Shared Access with Adjacent Sites.*  
35 Parking facilities serving a site, whether located on that  
36 same lot or on an adjacent lot, may be connected by  
37 means of a common access driveway within or  
38 between the interior of such lots. Where a property  
39 receives its access to the street through a shared  
40 driveway with another lot, a shared access easement  
41 shall be provided on the plat, or a shared access  
42 agreement running with the land shall be recorded by  
43 the municipality, as approved and executed by the  
44 director, guaranteeing the continued availability of the  
45 shared access between the properties.

1                    d. Paired Residential Driveways Allowed. Driveways for  
 2                    two single-family, two-family, or townhouse units may  
 3                    be attached side-by-side across a property line,  
 4                    provided the maximum combined width of the paved  
 5                    area is 24 feet in the Urban Neighborhood  
 6                    Development Context Areas (21.07.015), and 32 feet  
 7                    otherwise (16 feet maximum on each lot), without a  
 8                    landscaped break.

9  
 10                   e. Access and Parking Location in Urban Neighborhood  
 11                   Context Areas. In the Traditional and Edge Urban  
 12                   Neighborhood Contexts established in 21.07.015, the  
 13                   maximum percentage of the front setback area on a  
 14                   property that may be paved for vehicle access and  
 15                   parking shall be as shown in Table 21.07-12, provided  
 16                   that:

17  
 18                   i. Secondary frontages on lots with more than one  
 19                   street frontage are exempt;

20  
 21                   ii. Shared parking courtyards meeting  
 22                   21.07.060G.20. are exempt;

23  
 24                   iii. Minimum driveway widths per subsection  
 25                   21.07.090H.11.d. are allowed; and

26  
 27                   iv. Administrative adjustments may be approved as  
 28                   provided in 21.07.090H.8.b.

29  
**Table 21.07-12: Percentage of Front Setback Area in Urban Neighborhood Context Areas that May be Paved for Vehicle Access and Parking**

<u>Uses</u>	<u>Traditional Urban Neighborhood Context</u>	<u>Edge Urban Neighborhood Context</u>	
		<u>with Alley Access (per 21.07.090H.9.b)</u>	<u>without Alley Access</u>
<u>Single-family, Two-family and Mobile-Home Uses</u>	<u>50% max.</u>	<u>50% max.</u>	<u>75% max.</u>
<u>Other Uses</u>	<u>50% max.</u>	<u>50% max.</u>	<u>65% max.</u>

30  
 31                   10. Access to Parking Spaces.

32  
 33                   a. Access to Parking Spaces. Each parking space shall  
 34                   open to a parking aisle or driveway of such width and  
 35                   design as provided in section 21.07.090H.12. to  
 36                   provide safe and efficient means of vehicular access  
 37                   with no more than a standard two-movement entrance



1 or exit from the parking space and without having to  
2 move another vehicle. Stacked and tandem parking  
3 spaces are allowed exceptions as provided in  
4 21.07.090H.12.

5  
6 b. *Backing Distances.* Adequate ingress and egress to  
7 each parking space shall be provided without backing  
8 more than 25 feet.

9  
10 c. *Dead-End Parking Aisles.* Any parking bay that does  
11 not provide two means of vehicle egress shall provide,  
12 at the closed end, an extension of the parking aisle at  
13 least nine feet in depth, designated and signed as a  
14 “No Parking” turn-around area. Dead-end parking bays  
15 longer than 100 feet shall be subject to discretionary  
16 review and approval by the traffic engineer.

17  
18 d. *Vehicle Maneuvering.* Off-street parking facilities shall  
19 be designed so that all vehicle maneuvers occur on the  
20 development site and not in the public right-of-way, and  
21 vehicles enter and exit the right-of-way in a forward  
22 motion, except the following are exempt:

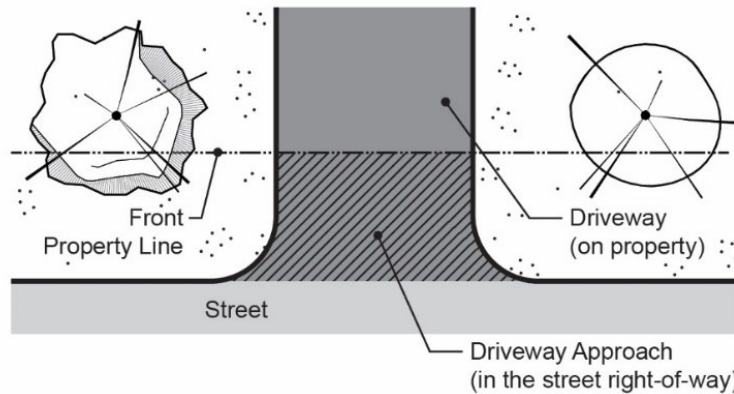
23  
24 i. *Parking for single-family, two-family, and mobile*  
25 home dwellings on individual lots, accessing a  
26 local street.

27  
28 ii. *For other developments, parking areas*  
29 comprising only one or two parking spaces  
30 whose only access is to a local street, provided  
31 that the paved vehicle area(s) occupy no more  
32 than 20 feet of the lot frontage in the front  
33 setback area.

34  
35 iii. *The usable portion of an alley may be credited*  
36 as circulation and/or parking aisle space for  
37 parking areas with up to 4 spaces, provided  
38 there is at least 24 feet of maneuvering area  
39 between the end of each parking space and the  
40 opposite side of the improved alley. Additional  
41 parking spaces may be designed so that  
42 vehicles back out into an alley, subject to  
43 approval by the traffic engineer.  
44

11. Driveway Design and Dimensions

a[D]. Parking Lot Entries/Driveway Approaches. Entries and driveway approaches providing access from the street edge to the front property line [TO PARKING LOTS] shall conform to the municipal driveway standards [CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER] and this section 21.07.090H.12. Access to streets owned by the state of Alaska requires compliance with state driveway standards, as provided in 21.07.090H.8. [DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES APPROVAL, AND A CURRENT VALID STATE OF ALASKA DRIVEWAY PERMIT. INGRESS AND EGRESS TO PARKING FACILITIES SHALL BE DESIGNED TO MAINTAIN ADEQUATE SIGHT DISTANCE AND SAFETY AND AS PRESCRIBED IN THE MUNICIPAL DRIVEWAY STANDARDS.]



b. Curb Openings and Public Walkway Crossings.

- i. Curb cut and curb returns at driveway openings to the street shall be provided as prescribed in the municipal driveway standards.
- ii. Public walkways shall be maintained or restored to the maximum running slope and cross-slope prescribed by M.A.S.S. and A.D.A., except that in the traditional urban neighborhood contexts (21.07.015), public walkways on local streets shall be restored to as close to a level running grade as practical.

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c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width.

[I. *RESIDENTIAL USES.* RESIDENTIAL DRIVEWAY ENTRANCES SHALL COMPLY WITH SUBSECTION 21.07.110F.3., DRIVEWAY WIDTH.

II. *NONRESIDENTIAL USES.*]

The total width of the driveway approach [ENTRANCES TO A NONRESIDENTIAL LOT] from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the frontage if the platting authority or traffic engineer finds that conditions warrant it, provided the following: [, UNLESS THE APPLICANT PROVIDES FOR SNOW STORAGE IN A MANNER APPROVED BY THE DECISION-MAKING BODY.]

i. The driveway approach is always allowed to have the minimum driveway width provided by subsection d. The traffic engineer may approve more than the minimum driveway width, provided the traffic engineer determines that snow storage, traffic flow and safety, and the neighborhood context are addressed.

ii. The driveway approach shall not exceed the maximum driveway width established in the Municipal Driveway Standards.

iii. Flag lots are exempt from the percentage limitations but shall have a maximum driveway approach width of 20 feet. Abutting flag lots may share a driveway approach up to 24 feet wide (12 feet per lot).

d. Minimum Driveway Width. The minimum required width of driveways, including both the driveway approach within the street ROW and the portion of the driveway on the development property, shall be the minimum widths established in the Municipal Driveway Standards, as modified and clarified by the following:

i. The minimum width of a driveway providing access to a single-family, two-family, townhouse, or mobile home dwelling from a local street or alley is 12 feet.

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ii. The minimum width of a driveway providing access for up to 10 parking spaces serving a townhouse or multifamily residential use from a local street or alley is 12 feet (e.g., one lane for two-way traffic).

iii. The minimum width of a driveway providing access for more than parking spaces serving a townhouse or multifamily use from a local street or alley is 12 feet for one-way traffic and 20 feet for two-way traffic.

e. Driveway Throat Length. The driveway throat shall be of sufficient length to enable the street and walkways in the ROW and the circulation, parking, and walkways in the development site to function without interfering with each other, as provided in the Municipal Driveway Standards.

f. Sight Distance. Ingress and egress to parking facilities shall be designed as prescribed in the municipal driveway standards to maintain adequate sight distance and safety.

g. Circulation Definition. Curbed end islands shall be provided at the end of each row of parking spaces to define circulation and provide sight distance at internal intersections of parking aisles, driveways, and/or on-site roadways. Shared parking courtyards meeting 21.07.060G.20. may depart from this requirement.

[E. **PARKING AND MANEUVERING.** ALL CIRCULATION AISLES, DRIVEWAYS, AND VEHICLE MANEUVERING AREAS REQUIRED BY THIS SECTION SHALL BE LOCATED ENTIRELY OFF-STREET AND ON THE PROPERTY UNLESS SPECIFICALLY PROVIDED OTHERWISE BY THIS SECTION.

I. **ACCESS TO PARKING SPACES.** TO ENSURE SAFE AND EFFICIENT VEHICULAR ACCESS TO PARKING SPACES, EACH OFF-STREET PARKING SPACE SHALL OPEN DIRECTLY ON A PARKING AISLE OR DRIVEWAY OF SUCH WIDTH AND DESIGN AS PROVIDED IN

1 TABLE 21.07-7 AND THE ILLUSTRATIONS  
2 THAT FOLLOW THE TABLE. ADEQUATE  
3 INGRESS AND EGRESS TO EACH PARKING  
4 SPACE SHALL BE PROVIDED WITHOUT  
5 BACKING MORE THAN 25 FEET.  
6

7 II. *MANEUVERING AREA.* OFF-STREET  
8 PARKING FACILITIES SHALL BE DESIGNED  
9 WITH SUFFICIENT MANEUVERING ROOM  
10 SO THAT ALL MANEUVERS ASSOCIATED  
11 WITH THE PARKING SHALL OCCUR IN THE  
12 OFF-STREET PARKING FACILITY, AND  
13 THAT ALL VEHICLES ENTER THE ABUTTING  
14 STREET IN A FORWARD MOTION.  
15

16 III. *SOME DWELLINGS EXEMPTED.* SINGLE-  
17 FAMILY, TWO-FAMILY, TOWNHOUSE, AND  
18 MOBILE HOME DWELLINGS ON INDIVIDUAL  
19 LOTS SHALL BE EXEMPTED FROM THIS  
20 SUBSECTION. MULTIFAMILY DWELLINGS  
21 WITH UP TO FOUR UNITS SHALL BE  
22 EXEMPTED FROM THIS SUBSECTION IN  
23 APPROPRIATE CIRCUMSTANCES IF  
24 APPROVED BY THE TRAFFIC ENGINEER.  
25 APPROPRIATE CIRCUMSTANCES MAY  
26 INCLUDE LOTS WITH ALLEY ACCESS, LOTS  
27 LOCATED ON LOW-VOLUME STREETS, AND  
28 LOTS LOCATED ON DEAD-END STREETS  
29 OR CUL-DE-SACS.  
30

31 F. *DEAD-END PARKING AISLES.* DEAD-END  
32 PARKING AISLES MAY BE ALLOWED ONLY WITH  
33 THE APPROVAL OF THE TRAFFIC ENGINEER.  
34

35 G. *ALLEYS.* SUBJECT TO SAFETY APPROVAL BY THE  
36 TRAFFIC ENGINEER, THE USABLE PORTION OF  
37 AN ALLEY MAY BE CREDITED AS CIRCULATION  
38 AND/OR PARKING AISLE SPACE.  
39

40 H. *CROSS ACCESS AND JOINT ACCESS WITH*  
41 *ADJACENT SITES.* THE PLAN SHALL SHOW  
42 EXISTING PARKING AND CIRCULATION  
43 PATTERNS ON ADJACENT PROPERTIES AND  
44 POTENTIAL CONNECTIONS. REQUIRED PARKING  
45 LOTS SERVING A SITE, WHETHER LOCATED ON  
46 THAT SAME LOT OR ON AN ADJACENT LOT, MAY

1 BE CONNECTED BY MEANS OF A COMMON  
2 ACCESS DRIVEWAY WITHIN OR BETWEEN THE  
3 INTERIOR OF SUCH LOTS. APPLICANTS ARE  
4 ENCOURAGED TO PROVIDE SHARED VEHICLE  
5 AND PEDESTRIAN ACCESS TO ADJACENT  
6 PROPERTIES FOR CONVENIENCE, SAFETY, AND  
7 EFFICIENT CIRCULATION. AN ACCESS  
8 EASEMENT SHALL BE PROVIDED ON THE PLAT,  
9 OR A SHARED ACCESS AGREEMENT RUNNING  
10 WITH THE LAND SHALL BE RECORDED BY THE  
11 MUNICIPALITY, AS APPROVED AND EXECUTED  
12 BY THE DIRECTOR, GUARANTEEING THE  
13 CONTINUED AVAILABILITY OF THE SHARED  
14 ACCESS BETWEEN THE PROPERTIES.]

15  
16 12[9]. Dimensions of Parking Spaces and Aisles. The parking space  
17 and aisle dimensional standards of this section  
18 21.07.090H.12. shall apply to all parking except as stated  
19 otherwise in sections 21.07.090I., passenger loading zones  
20 and 21.07.090J., accessible parking spaces. Parking  
21 dimensions and layout shall be as provided in subsection a.,  
22 including Table 21.07-13 and the figures that follow the table,  
23 with the additions and exceptions provided in subsections b.  
24 through m.]THE MINIMUM DIMENSIONS FOR PARKING  
25 SPACES AND PARKING AISLES SHALL BE AS PROVIDED  
26 IN TABLE 21.07-7, AND CALCULATED AS DEPICTED IN  
27 THE FIGURES THAT FOLLOW THE TABLE. THE MINIMUM  
28 PARKING SPACE. WIDTH SHALL BE 9'0 EXCEPT AS  
29 PROVIDED ELSEWHERE IN THIS SECTION. THE  
30 PARKING CONFIGURATION STATED IN THE FOLLOWING  
31 TABLES AND FIGURES SHALL APPLY TO ALL OFF-  
32 STREET PARKING, EXCEPT AS STATED ELSEWHERE IN  
33 THIS SECTION.]

34  
35 a. Minimum Dimensions for Required Parking. Parking  
36 shall meet or exceed the STANDARD parking space  
37 and aisle dimensions set forth in table 21.07-13, except  
38 as follows:

39  
40 i. Parking facilities in the Downtown (DT) zoning  
41 districts, and in the Traditional Urban  
42 Neighborhood Contexts delineated in  
43 21.07.015, may meet the SMALL parking space  
44 and aisle dimensions in table 21.07-13. Up to  
45 30 percent of parking in the Downtown (DT)  
46 zoning districts may meet the COMPACT

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parking dimensions, if signed for compact vehicles only.

- ii. In all other areas of the municipality, up to 50 percent of employee-only parking, structured parking facilities, or parking for uses with a low parking turnover rate may meet the SMALL parking space and aisle dimensions in table 21.07-13. Uses with a low parking turnover rate comprise multifamily and mixed-use household living uses, office uses, and educational facility uses other than instructional services. Required parking shared between low-turnover and non-low-turnover uses shall meet STANDARD dimensions.

TABLE 21.07-13[7] PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Size	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
90	STANDARD	9' 0"	9' 0"	20' 0"	23' 0"	24'	64' 0"	0' 0"	2' 0"
	SMALL	8' 6"	8' 6"	18' 0"	23' 6"	24'	60' 0"	0' 0"	
	COMPACT	8' 0"	8' 0"	16' 0"	23' 6"	24'	56' 0"	0' 0"	
75	STANDARD	9' 0"	9' 4"	21' 8"	19' 0"	24'	67' 4"	1' 2"	1' 11"
	SMALL	8' 6"	8' 10"	19' 7"	19' 6"	24'	63' 2"	1' 11"	
	COMPACT	8' 0"	8' 3"	19' 5"	19' 6"	24'	62' 11"	0' 6"	
60	STANDARD	9' 0"	10' 5"	21' 10"	18' 0"	24'	67' 8"	2' 3"	1' 8"
	SMALL	8' 6"	9' 10"	19' 10"	18' 6"	24'	63' 8"	2' 2"	
	COMPACT	8' 0"	9' 3"	17' 10"	18' 6"	24'	59' 9"	1' 0"	
*** ** (table continued...)									

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18

1

TABLE 21.07-13[7] PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Type	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
***	***	***	(table continued from previous page)						
45	STANDARD	9' 0"	12' 9"	20' 6"	12' 0"	24'	65' 0"	3' 2"	1' 5"
	SMALL	8' 6"	12' 0"	18' 9"	12' 6"	24'	61' 6"	3' 0"	
	COMPACT	8' 0"	11' 4"	16' 12"	12' 6"	24'	57' 11"	1' 5"	
0 (parallel)	STANDARD	9' 0"	23' 0"	9' 0"	12' 0"	24'	42' 0"	0' 0"	0' 0"
	SMALL	8' 6"	23' 0"	8' 6"	12' 6"	24'	41' 0"	0' 0"	
	COMPACT	8' 0"	23' 0"	8' 0"	12' 6"	24'	40' 0"	0' 0"	
		[9' 0"]	[23' 0"]	[9' 0"]	[12' 0"]	[24']	[42' 0"]	[0' 0"]	
		[9' 6"]	[23' 0"]	[9' 6"]	[12' 0"]	[24']	[43' 0"]	[0' 0"]	
	[10' 0"]	[23' 0"]	[10' 0"]	[12' 0"]	[24']	[44' 0"]	[0' 0"]		
[45]		[8' 6"]	[12' 0"]	[18' 9"]	[12' 6"]	[24']	[61' 6"]	[3' 0"]	[1' 5"]
		[9' 0"]	[12' 9"]	[20' 6"]	[12' 0"]	[24']	[65' 0"]	[3' 2"]	
		[9' 6"]	[13' 5"]	[20' 10"]	[12' 0"]	[24']	[65' 9"]	[3' 4"]	
		[10' 0"]	[14' 2"]	[21' 3"]	[12' 0"]	[24']	[66' 5"]	[3' 6"]	
[60]		[8' 6"]	[9' 10"]	[19' 10"]	[18' 6"]	[24']	[63' 8"]	[2' 2"]	[1' 8"]
		[9' 0"]	[10' 5"]	[21' 10"]	[18' 0"]	[24']	[67' 8"]	[2' 3"]	
		[9' 6"]	[10' 12"]	[22' 1"]	[18' 0"]	[24']	[68' 2"]	[2' 5"]	
		[10' 0"]	[11' 7"]	[22' 4"]	[18' 0"]	[24']	[68' 8"]	[2' 6"]	
[75]		[8' 6"]	[8' 10"]	[19' 7"]	[19' 6"]	[24']	[63' 2"]	[1' 1"]	[1' 11"]
		[9' 0"]	[9' 4"]	[21' 8"]	[19' 0"]	[24']	[67' 4"]	[1' 2"]	
		[9' 6"]	[9' 10"]	[21' 9"]	[18' 6"]	[24']	[67' 7"]	[1' 3"]	
		[10' 0"]	[10' 4"]	[21' 11"]	[18' 0"]	[24']	[67' 10"]	[1' 5"]	
[90]		[8' 6"]	[8' 6"]	[18' 0"]	[23' 6"]	[24']	[60' 0"]	[0' 0"]	[2' 0"]
		[9' 0"]	[9' 0"]	[20' 0"]	[23' 0"]	[24']	[64' 0"]	[0' 0"]	
		[9' 6"]	[9' 6"]	[20' 0"]	[22' 0"]	[24']	[64' 0"]	[0' 0"]	
		[10' 0"]	[10' 0"]	[20' 0"]	[22' 0"]	[24']	[64' 0"]	[0' 0"]	

\*\*\* \*\*

(Note to code revisor: Re-number existing subsections a., b., c., d., and e. to b., c., d., e., and f.)

g.[F.] Structural Columns. A structural column may encroach into the width of a parking space by up to one foot, provided the remaining parking space width is no less than eight feet. Such[IF THE] column shall be[IS] located within four feet of either end of the parking space depth. Such column shall not be located within one foot of the drive aisle.

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h.[G.] Minimum Vertical Clearance

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[H. **SMALLER PARKING SPACES FOR LOW TURNOVER USES.** REDUCED PARKING SPACE DIMENSIONS MAY BE APPROVED BY THE TRAFFIC ENGINEER PURSUANT TO SUBSECTION F.21. ABOVE.]

i. *Recreational Vehicle Spaces.* Parking spaces for recreational vehicles, if provided and delineated, shall be a minimum of 10 feet wide by 40 feet long.

j. *Stacked [, AUTOMATED,] or Tandem Spaces with Attendant Parking.* The traffic engineer may approve reduced parking space dimensions for stacked parking spaces[,] and/or tandem spaces in an attendant (valet) parking facility. An accessible passenger loading zone shall be provided with attendant parking services at or near a primary entrance. Notice of availability of valet service shall be conspicuously posted inside and outside the primary entrance. The business owner shall enter into a parking agreement with the municipality guaranteeing continuation of the valet service.

k. *Stacked or Tandem Spaces for a Residential Dwelling.* Two required parking spaces for any residential dwelling may be arranged in tandem with a combined space depth (vehicle projection length) of 38 feet, or stacked one above the other using a car stacker, subject to the following:

i. The parking required for the dwelling unit is arranged independently from parking serving any other dwelling unit; and

ii. The owner assigns the two spaces to the same dwelling.

l. *Automated Parking Structures.* Automated parking structures are exempt from the parking space and aisle dimensions and vertical clearance requirements of this section 21.07.090H.12. They are instead subject to section 21.07.090M.10.

1 m. Administrative Adjustments. The traffic engineer may  
2 approve a lesser parking dimension than set forth in  
3 this section 21.07.090H.12., due to site topography,  
4 existing structures, lot configuration, or preservation of  
5 trees and vegetation, subject to a recorded parking  
6 agreement complying with 21.07.090F.2.

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9 (Note to code revisor: Re-number existing table 21.07-8 to 21.07-  
10 14. in section 21.07.090J., Accessible Parking Spaces.)

- 11
- 12 I. **Passenger Loading Zones.** All institutional, entertainment, and  
13 commercial uses such as schools/daycare, stadiums, and theaters  
14 that have high-volume peak traffic volumes shall provide an area for  
15 drop-offs and pick-ups that meets the following requirements:
- 16
- 17 1. **Passenger Loading Zone.** In addition to the required minimum  
18 number of parking spaces established in subsection  
19 21.07.090E., the traffic engineer may require one or more  
20 passenger loading zone spaces, depending on the type,  
21 intensity, and traffic patterns of the proposed use. The  
22 passenger loading zone for large commercial establishments  
23 or other intensive uses may be required by the traffic engineer  
24 to include one or more spaces dedicated to taxi cabs, ride  
25 hailing services, and/or other specialized high occupancy  
26 vehicles.
  - 27
  - 28 2. **Passenger Loading Zone Dimensions.** Any passenger loading  
29 zone that is provided for a development shall consist of one  
30 or more passenger drop-off/pick-up spaces parallel to the  
31 driveway or circulation aisle adjacent to the building. Each  
32 space shall be a minimum of 20 feet in length and eight or  
33 more feet in width, with an access aisle at least five feet wide  
34 abutting the full length of the space. As an alternative, subject  
35 to approval of the traffic engineer, a passenger loading zone  
36 may consist of one or more parking spaces that meets the  
37 accessible parking space dimensional standards of  
38 subsection 21.07.090J.7. However, designated passenger  
39 loading zone spaces shall not count towards the accessible  
40 parking space requirement.
  - 41
  - 42 3. **Plan.** The parking facility layout, circulation, and design plan  
43 shall show the location and design of proposed passenger  
44 loading zones. For certain intensive uses, the traffic engineer  
45 may require the plan to include a traffic control plan  
46 addressing projected usage, hours of operation, peak

1 loading/unloading time, plans for directing traffic, safety  
2 measures, and other information deemed necessary by the  
3 traffic engineer to designing a safe and well-functioning drop-  
4 off area.

5  
6 4. *Accessible Route.* An accessible pedestrian route to the  
7 building or facility entrance shall be provided pursuant to  
8 subsection 21.07.090J.9., *Accessible Routes*.

9  
10 5. *Schools.* Passenger loading zones shall be required for  
11 schools (public or private). Length, location, and design of the  
12 passenger loading zones shall be approved by the traffic  
13 engineer.

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16 K. Bicycle Parking[ SPACES].

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18 1. *Bicycle Parking Required.* [ALL NONRESIDENTIAL,  
19 MULTIFAMILY, AND MIXED-USE DWELLING  
20 DEVELOPMENTS WITH MORE THAN 40 PARKING  
21 SPACES REQUIRED IN TABLE 21.07-4 SHALL PROVIDE  
22 AT LEAST FOUR BICYCLE PARKING SPACES, OR A  
23 NUMBER OF BICYCLE PARKING SPACES EQUAL TO  
24 THREE PERCENT OF THE NUMBER OF REQUIRED  
25 AUTOMOBILE PARKING SPACES, WHICHEVER IS  
26 GREATER.]

27 Table 21.07-14 establishes the minimum number of bicycle  
28 parking spaces required per principal use, and the percentage  
29 of these spaces required to be developed as long-term bicycle  
30 parking, versus short-term spaces. The required minimum  
31 number of spaces is calculated as follows:

32  
33 a. A minimum of two bicycle parking spaces shall be  
34 provided for each use in Table 21.07-14.

35  
36 b. After the first 40 bicycle parking spaces, no additional  
37 bicycle parking is required for any individual non-  
38 residential use.

39  
40 c. Section 21.07.090C., Computation of Parking and  
41 Loading Requirements, applies with respect to  
42 fractions, multiple uses, and building floor area  
43 measurements.  
44

**TABLE 21.07-14: Bicycle Parking Spaces Required**

Use Category/Type	Minimum Number of Bicycle Spaces		Minimum Percentage for Long-Term Spaces (for required bicycle parking in excess of 2 spaces)
	Areas with Area-specific Parking Requirements in Table 21.07-7	All Other Areas	
<u>Multifamily and mixed-use dwellings</u>	<u>1 space per two dwellings</u>	<u>1 space per 10 dwellings</u>	<u>90 percent</u>
<u>Group living uses, Roominghouses, Homeless and transient shelters,</u>	<u>1 space per 5 beds</u>	<u>1 space per 5 beds</u>	<u>90 percent</u>
<u>Government administration &amp; civic facility, Offices, Hospitals/Healthcare facilities,</u>	<u>1 space per 5,000 sf gfa</u>	<u>1 space per 10,000 sf gfa</u>	<u>75 percent</u>
<u>Cultural Facilities, Entertainment &amp; Recreation Facilities</u>	<u>1 space per 10,000 sf gfa</u>	<u>1 space per 20,000 sf gfa</u>	<u>10 percent</u>
<u>Schools, Colleges,</u>	<u>4 spaces per classroom</u>	<u>2 spaces per classroom</u>	<u>25 percent</u>
<u>Restaurants, Bars</u>	<u>1 space per 3,000 sf gfa</u>	<u>1 space per 10,000 sf gfa</u>	<u>10 percent</u>
<u>Retail Sales, Personal Service, Repair &amp; Rental,</u>	<u>1 space per 12,000 sf gfa</u>	<u>1 space per 24,000 gfa</u>	<u>10 percent</u>
<u>Industrial uses</u>	<u>1 space per 25,000 sf. gfa</u>	<u>1 space per 50,000</u>	<u>75 percent</u>
<u>Visitor Accommodations</u>	<u>1 space per 23,000 sf gfa</u>	<u>1 space per 23,000 sf gfa</u>	<u>75 percent</u>
<u>Uses not included above, except as provided in subsection K.2. below</u>	<u>Minimum of 2 spaces</u>	<u>Minimum of 2 spaces</u>	<u>Not required</u>

2. Exemptions. The bicycle parking [THIS] requirements in subsection 1. shall not apply to:

- a. Household living dwelling units with their own individual garages [BUILDINGS IN MULTIFAMILY OR MIXED-USE DEVELOPMENTS THAT ARE SINGLE-FAMILY, TWO-FAMILY, OR TOWNHOUSE STYLE STRUCTURES].
- b. Airstrips.
- c. Utility Substations, Telecommunication facilities, and WECS.
- e. Natural resource extraction.
- f. Impound yards and storage yards.
- g. Any other non-residential use which has no regular employees or people on premises.

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3. Administrative Adjustments. The director may approve administrative relief or adjustments to the bicycle parking requirement for changes of use or modifications to existing buildings and site, provided the applicant demonstrates the adjustment is necessary to compensate for a practical difficulty of the site.

4. Bicycle Parking Facility Development Standards. These requirements shall apply to all required bicycle parking spaces, except as specifically provided otherwise. Only long-term parking spaces required by table 21.07-14 (in right-hand column) shall follow the requirements of subsection g. [BICYCLE PARKING SPACES SHALL MEET THE STANDARDS OF SUBSECTION 21.07.060F.15.]

a. Required bicycle parking or a sign leading thereto shall be located in an area visible from a primary entrance area. Required bicycle parking other than long-term bicycle parking shall be located within 50 feet of a primary entrance as measured along an accessible pedestrian route.

b. A required bicycle parking space shall be either a bike rack or bike locker. Each shall be a securely anchored, tamper-resistant structure. Bike racks shall allow the bike frame and one wheel to be locked to the rack with a standard U-type lock when both wheels are left on the bicycle. Bike racks shall support the bicycle frame in a stable position without damage to the bicycle. Horizontal bike racks (i.e., racks not mounted to walls) shall support the bicycle at two or more points.

i. Bicycle rack tubing shall be a minimum of ¼ inch diameter and maximum of 3-inch diameter.

ii. The topmost attachable portion of bicycle racks shall be 25 inches to 40 inches in height.

c. A required bicycle parking space shall be a minimum of six feet long and two feet wide, with the following exceptions:

i. Vertical bicycle parking spaces (securing the bicycle perpendicular to the ground) shall be a minimum of three feet six inches deep

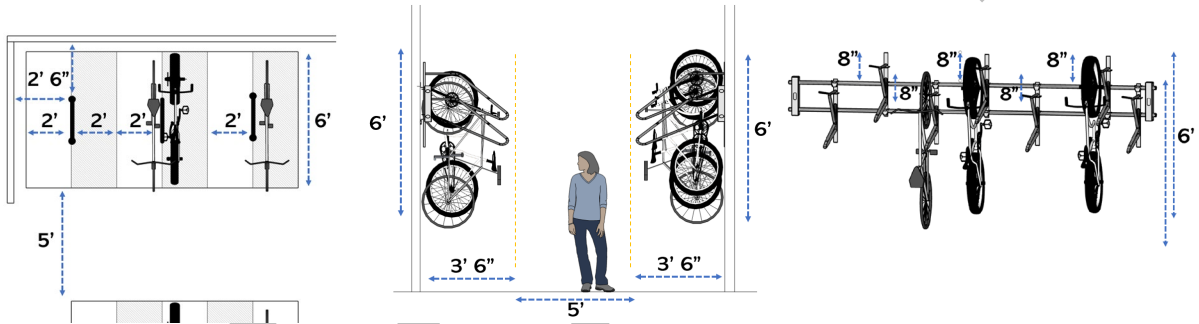
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measured from the wall and six feet tall with a vertical stagger of eight inches between side-by-side spaces, as shown in the first set of figures below.

ii. Triangle bike locker layouts are exempt from the two-foot minimum width however shall have a minimum length of six feet six inches and access doors at least two feet six inches wide.

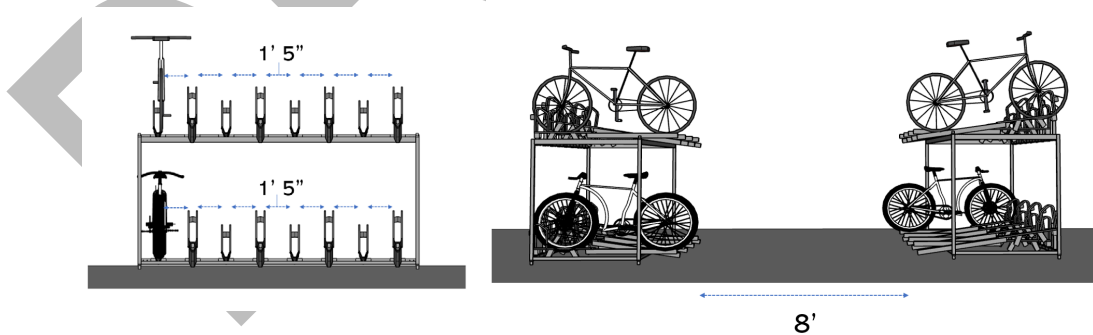
iii. Stacked bike parking spaces (stacking bicycles one tier on top of another) may meet the alternative stacked dimensions in the bottom figures provided there is a mechanically assisted lifting mechanism for mounting the bicycle on the upper tier.

Dimensions for Horizontal/Side-by-Side and Vertical/Wall-Mounted Bicycle Spaces:



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Dimensions for Stacked Bicycle Spaces:



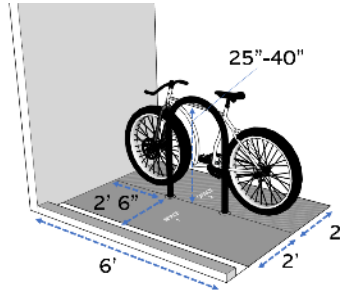
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iv. Other deviations are permitted, per director approval, provided the applicant demonstrates the spaces meet the intent of the standard.

d. Bicycle parking spaces shall be designed so that the bicycle may be added or removed without having to move another bicycle. There shall be a minimum of

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five feet of clear space inclusive of a walkway, behind the two-by-six-foot (or alternative dimensioned) parking space to allow room for bicycle maneuvering. There shall be a minimum of two-foot six-inch clearance from all sides of the bicycle rack to walls, fences, curbs, and vehicle parking and circulation areas, except that racks attached to walls have no required wall clearance.



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e. Areas devoted to bicycle parking facilities shall be hard-surfaced and designed and maintained to be clear of mud and snow.

f. Bicycle parking shall not obstruct pedestrian walkways, building access, or other use areas.

g. Long-term bicycle parking spaces are intended to accommodate residents, students, employees, and commuters who typically need to store a bicycle overnight or for an entire work shift or school day. Required long-term spaces shall comply with the following additional standards:

i. Long-term bicycle parking shall be located in well-lit, restricted access, lockable rooms or enclosures providing protection from theft, vandalism, and weather. Bicycle lockers, covered bicycle cages, or a secure room inside a building are common examples.

ii. Long-term bicycle parking spaces for residential uses may be located within dwelling units for first-floor dwelling units only, unless the unit is on a floor with an accessible elevator. Bicycle parking shall be located within 15 feet of the primary entrance and in a closet or three-sided alcove which allows a space 2 feet wide and 6

feet long. Long-term spaces shall not be located on a balcony, deck, or patio area accessory to a dwelling unit.

iii. A minimum of 30 percent of long-term spaces shall be ground-level, horizontal spaces.

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(Note to code revisor: Re-number table 21.07-9 to 21.07-15 in section 21.07.090L., Vehicle Queuing Spaces.)

(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-58, 5-20-14; AO 2015-82, 7-28-15; AO 2015-100, 10-13-15; AO 2015-131, 1-12-16; AO 2016-3(S), 2-23-16; AO 2017-55, 4-11-17; AO 2017-176, 1-9-18; AO 2019-132, 12-3-19; AO 2020-38, 4-28-20)

**21.07.110 Residential Design Standards.**

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A. Purpose.

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[4. ENHANCE PUBLIC SAFETY BY PROMOTING LINES OF SIGHT TO RESIDENTIAL ENTRIES, CLEARLY DEFINING TRANSITIONS FROM PUBLIC TO MORE PRIVATE RESIDENTIAL OUTDOOR SPACES, AND DESIGNING ENTRANCES TO BE PROMINENT.

5. LOCATE ACTIVE LIVING SPACES, ENTRANCES, AND WINDOWS TO IMPROVE THE PHYSICAL AND VISUAL CONNECTION FROM RESIDENCES TO THE STREET, AND FOSTER OPPORTUNITIES FOR CASUAL SURVEILLANCE OF THE STREET AND OUTWARDLY EXPRESSED PROPRIETORSHIP OF THE NEIGHBORHOOD.]

(Note to code revisor: Re-number subsequent subsections.)

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C. Standards for Multifamily and Townhouse Residential.

1. Purpose.

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[C. PROMOTE BUILDING PLACEMENT AND ORIENTATION THAT INTERFACES WITH THE NEIGHBORHOOD STREET AND ENGAGES THE PEDESTRIAN, TO CONTRIBUTE TO PUBLIC SAFETY, ATTRACTIVE STREET FRONTAGES, PEDESTRIAN ACCESS, AND A SENSE OF NEIGHBORHOOD AND COMMUNITY.]

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1 [G. PROMOTE A SAFE LIVING ENVIRONMENT  
2 THROUGH “EYES ON THE STREET” DESIGN  
3 ELEMENTS, SUCH AS PLACEMENT OF WINDOWS,  
4 AND TRANSITION SPACES BETWEEN THE PUBLIC  
5 REALM AND THE MORE PRIVATE AREAS OF THE  
6 DEVELOPMENT.]  
7

8 *(Note to code revisor: Re-number remaining subsections.)*  
9

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11 [3. **WINDOWS FACING THE STREET.**

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13 A. **WINDOWS.** WINDOWS OR PRIMARY ENTRANCE  
14 DOORS SHALL BE PROVIDED ON EACH BUILDING  
15 ELEVATION FACING A STREET OR THAT HAS  
16 PRIMARY ENTRANCES TO DWELLINGS (UP TO A  
17 MAXIMUM OF TWO ELEVATIONS). AT LEAST 10  
18 PERCENT OF THE WALL AREA OF THE BUILDING  
19 ELEVATION SHALL BE WINDOWS OR PRIMARY  
20 ENTRANCE DOORS. THE DIRECTOR MAY  
21 ELIMINATE OR REDUCE THE REQUIREMENT ON  
22 ONE ELEVATION (IN INSTANCE WHERE UP TO  
23 TWO APPLY) BASED ON SITE SPECIFIC  
24 CIRCUMSTANCES (SUCH AS INFILL PROJECTS  
25 WHERE AN ENTRY FACES AN ADJACENT  
26 PROPERTY RATHER THAN THE STREET).  
27

28 B. **WALL AREA CALCULATION.** THE AREA OF  
29 VERTICAL WALL SURFACE MEASURED FROM  
30 FINISHED FLOOR OF EACH LEVEL, INCLUDING  
31 GARAGES TO TOP OF TOP PLATE. IN THE CASE  
32 OF A BASEMENT WALL, CALCULATE THE AREA  
33 OF VERTICAL WALL SURFACE MEASURED FROM  
34 FINISHED GRADE TO THE TOP OF THE FINISHED  
35 FLOOR ABOVE OR TOP OF TOP PLATE,  
36 WHICHEVER IS APPLICABLE. ANY WALL AREA  
37 ABOVE THE TOP PLATE SHALL NOT APPLY,  
38 INCLUDING ANY GABLE ENDS. THE FAÇADE WALL  
39 AREA OF STORIES BELOW GRADE PLANE, SUCH  
40 AS BELOW GRADE PARKING, ARE EXEMPT FROM  
41 THE MEASUREMENT OF WALL AREA. THE ROOF,  
42 INCLUDING EAVES, FASCIA, AND VENT STACKS,  
43 IS ALSO EXEMPT, AS ILLUSTRATED BELOW.]

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(Note to code revisor: Delete the following illustration.)



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[C. **ENERGY EFFICIENCY EXCEPTION.** A REDUCTION IN REQUIRED WINDOW AREA IS PERMITTED IF DEMONSTRATED BY CALCULATION BY AN ENERGY RATER CERTIFIED BY THE STATE OF ALASKA THAT INSTALLING THE REQUIRED WINDOW AREA WILL REDUCE THE ENERGY RATING BELOW A 5-STAR ENERGY RATING.

D. **ADDITIONS AND RENOVATIONS.** ONLY THE WALL AREA AFFECTED BY EITHER AN ADDITION OR A RENOVATION SHALL BE REQUIRED TO COMPLY WITH THE OPENING REQUIREMENTS. UNAFFECTED WALL AREAS NEED NOT COMPLY. GARAGE ADDITIONS ARE EXEMPT FROM OPENING REQUIREMENTS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT FOR WINDOW OPENINGS ON ADDITIONS/RENOVATIONS BASED ON PROJECT SPECIFIC CIRCUMSTANCES (SUCH AS THE NATURE OF THE SPACES BEING ADDED, BUILDING ORIENTATION, OR STRUCTURAL REQUIREMENTS).]

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3[4]. **Building Spacing.**

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[5. **PEDESTRIAN ACCESS.** WALKWAY CONNECTIONS FROM PRIMARY FRONT ENTRANCES TO THE STREET ARE REQUIRED IN ACCORDANCE WITH SUBSECTION 21.07.060E.4., EXCEPT THAT DEVELOPMENTS MAY

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PROVIDE ONE OF THE FOLLOWING ALTERNATIVES INSTEAD:

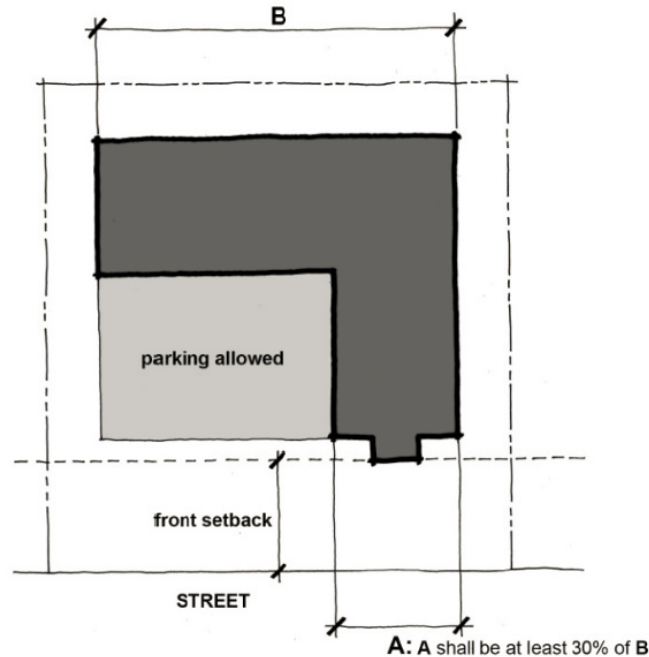
- A. PRIMARY FRONT ENTRANCES FOR INDIVIDUAL DWELLINGS MAY CONNECT TO THE STREET BY THE DWELLING UNIT'S INDIVIDUAL DRIVEWAY IF SUCH IS PROVIDED;
  - B. A PARKING COURTYARD MAY BE PROVIDED IN CONFORMANCE WITH SUBSECTION 21.07.060F.18., OR
  - C. OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE SAFE, CONVENIENT, AND ADEQUATE PEDESTRIAN ACCESS.
6. *BUILDING AND SITE ORIENTATION MENU.* BUILDINGS SHALL BE ORIENTED TO SURROUNDING STREETS, SIDEWALKS, COMMON PRIVATE OPEN SPACES, AND THE NEIGHBORHOOD PUBLIC REALM THROUGH AT LEAST THREE OF THE FOLLOWING MENU CHOICES. THE DIRECTOR MAY REDUCE THE REQUIREMENT TO TWO MENU CHOICES IF HE OR SHE DETERMINES THAT THE PRIMARY OBJECTIVE OF THE MENU OPTION IS ACHIEVED OR THAT THE SPECIFIC SITE LIMITS AVAILABILITY OF A MAJORITY OF THE OPTIONS.
- A. *COURTYARD HOUSING.* ARRANGE OR CONFIGURE THE BUILDING(S) TO ENCLOSE AND FRAME A HOUSING COURTYARD AS DESCRIBED IN SUBSECTION 21.07.060F.7.
  - B. *ORIENTATION OF LIVING SPACES AND WINDOWS—GROUND FLOOR.* PROVIDE WINDOWS AND/OR ENTRANCES TO HABITABLE LIVING SPACES COMPRISING, AT LEAST 10 PERCENT OF THE GROUND-FLOOR WALL AREA OF UP TO TWO BUILDING ELEVATIONS FACING A STREET OR HAVING A PRIMARY FRONT ENTRANCE. WINDOWS IN A GARAGE DOOR DO NOT COUNT TOWARDS THE MINIMUM AREA IN THIS SECTION.
  - C. *ORIENTATION OF LIVING SPACES AND WINDOWS—OVERALL DEVELOPMENT.* PROVIDE WINDOWS AND/OR ENTRANCES TO HABITABLE

1 LIVING SPACES, COMPRISING AT LEAST 10  
2 PERCENT OF THE TOTAL WALL AREA OF UP TO  
3 TWO BUILDING ELEVATIONS FACING A STREET  
4 OR HAVING A PRIMARY FRONT ENTRANCE.  
5 WINDOWS IN A GARAGE DOOR DO NOT COUNT  
6 TOWARDS THE MINIMUM AREA OF THIS SECTION.  
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8 D. *ORIENTATION OF LIVING SPACES AND*  
9 *WINDOWS—ADDITIONAL TRANSPARENCY.*  
10 INCREASE THE PERCENTAGE OF THE WALL  
11 AREA COMPRISED OF WINDOWS AND/OR  
12 PEDESTRIAN ENTRANCES TO 20 PERCENT IN  
13 SUBSECTIONS 6.B. AND/OR 6.C. ABOVE.  
14

15 E. *STREET FRONTAGE – PARKING BESIDE OR*  
16 *BEHIND THE BUILDING.* NO MORE THAN 70  
17 PERCENT OF A STREET-FACING BUILDING  
18 ELEVATION SHALL HAVE PARKING FACILITIES  
19 (INCLUDING GARAGES) BETWEEN IT AND THE  
20 STREET.]  
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22 (Note to code revisor: Delete the following illustration.)  
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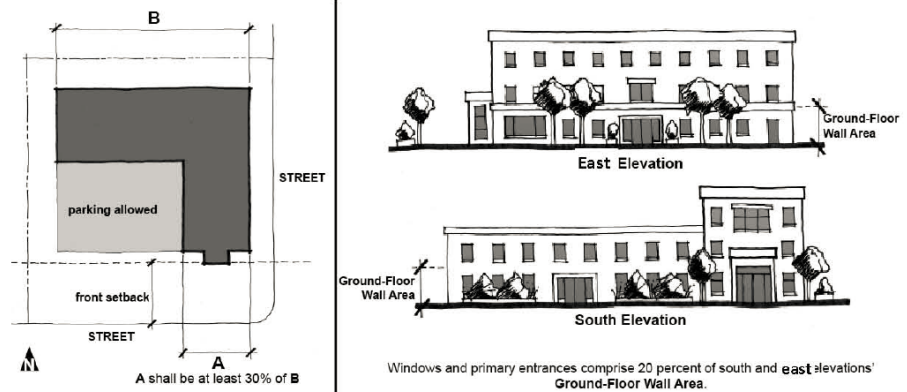


46 [F. *STREET FRONTAGE – LIMITED PARKING WIDTH.*  
47 AS AN ALTERNATIVE TO 6.E. ABOVE, LIMIT  
48 DRIVEWAYS AND PARKING FACILITIES TO NO  
49 MORE THAN 50 PERCENT OF THE TOTAL SITE  
50 AREA BETWEEN THE BUILDING AND A STREET,

1 AND GARAGES TO NO MORE THAN 50 PERCENT  
2 OF THE STREET FACING BUILDING ELEVATION.  
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4 G. *STREET CORNER BUILDING.* ON A CORNER LOT,  
5 PROVIDE CHOICES 6.E. AND 6.F. ON BOTH  
6 STREET FRONTAGES, SUCH THAT THE BUILDING  
7 (INCLUDING ITS HABITABLE FLOOR AREA WITH  
8 WINDOWS) IS PLACED NEAREST THE CORNER,  
9 AND ANY PARKING FACILITIES ARE LOCATED  
10 BESIDE OR BEHIND THE BUILDING AWAY FROM  
11 THE STREET CORNER. COMPLIANCE WITH THIS  
12 FEATURE COUNTS FOR MEETING TWO.]  
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14 (Note to code revisor: Delete the following illustration.)



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30 [H. *LIMITED FRONT-FACING GARAGE WIDTH FOR*  
31 *TOWNHOUSES.* WHERE GARAGES FOR  
32 INDIVIDUAL TOWNHOUSE-STYLE DWELLING  
33 UNITS FACE THE STREET OR ARE ON THE SAME  
34 BUILDING ELEVATION AS THE PRIMARY FRONT  
35 ENTRY TO THE DWELLINGS, LIMIT THE GARAGE  
36 DOOR WIDTH TO NO MORE THAN 50 PERCENT OF  
37 THE WIDTH OF EACH DWELLING, OR UP TO 67  
38 PERCENT PROVIDED THE BUILDING FAÇADE  
39 ACHIEVES ONE MORE FEATURE THAN REQUIRED  
40 IN SUBSECTION C.7., BUILDING ARTICULATION  
41 MENU.  
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43 I. *ENHANCED SIDEWALK.* PROVIDE AN ENHANCED  
44 SIDEWALK CONFORMING TO SUBSECTION  
45 21.07.060F.17. ON AT LEAST ONE STREET  
46 FRONTAGE THAT IS NOT LESS THAN 100 LINEAR  
47 FEET.  
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J. *SEPARATED WALKWAY TO THE STREET.* CONNECT ALL PRIMARY ENTRANCES TO THE STREET BY A CLEAR AND DIRECT WALKWAY SEPARATED FROM AND NOT ROUTED THROUGH A PARKING FACILITY.

K. *VISIBLE FRONT ENTRIES.*

I. PLACE THE PRIMARY FRONT ENTRANCE(S) (AT LEAST ONE SHARED PRIMARY FRONT ENTRANCE FOR A MULTIFAMILY STRUCTURE OR AT LEAST 50 PERCENT OF INDIVIDUAL UNIT ENTRANCES) ON A STREET-FACING BUILDING ELEVATION, OR FACING A COMMON PRIVATE OPEN SPACE THAT IS VISIBLE FROM AND HAS DIRECT ACCESS TO THE STREET.

II. AS AN ALTERNATIVE, PLACE THE PRIMARY FRONT ENTRANCE(S) ON A FAÇADE AT AN ANGLE OF UP TO 90 DEGREES FROM THE STREET, WHERE THERE IS AN UNOBSTRUCTED LINE OF SIGHT FROM THE STREET OR SIDEWALK EDGE (ABUTTING THE SITE) TO THE ENTRY DOOR, AND A SHELTERING ROOF STRUCTURE NO MORE THAN 12 FEET ABOVE THE FLOOR OF THE ENTRY AS A PERMANENT ARCHITECTURAL FEATURE PROJECTING FROM THE FAÇADE.

L. *ENHANCED FRONT YARD LANDSCAPING.* PROVIDE ONE LEVEL HIGHER OF SITE OR PARKING LOT PERIMETER LANDSCAPING ALONG THE STREET FRONTAGES (UP TO A MAXIMUM OF TWO FRONTAGES) THAN OTHERWISE REQUIRED BY THIS TITLE. FOR EXAMPLE, IF L1 LANDSCAPING IS REQUIRED, PROVIDE L2 LANDSCAPING.

M. *SITE ENTRY FEATURE.* HIGHLIGHT AND DEFINE A PEDESTRIAN AND/OR VEHICLE ENTRANCE TO A DEVELOPMENT SITE USING THREE OR MORE OF THE FOLLOWING ELEMENTS:

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- I. LANDSCAPE TREATMENT WITH SEASONAL COLOR AND TREES, WHICH CLEARLY DISTINGUISHES AND HIGHLIGHTS THE SITE ENTRY.
- II. PLAZA OR COURTYARD AS DESCRIBED IN SUBSECTION 21.07.060F6. OR 7.
- III. IDENTIFYING BUILDING PRIMARY ENTRANCE FORM INCLUDING A COVERED ENTRY, WHEN THE PRIMARY ENTRANCE IS WITHIN 40 FEET OF THE SITE ENTRANCE.
- IV. SPECIAL PAVING, PEDESTRIAN SCALE LIGHTING, AND/OR BOLLARDS.
- V. ORNAMENTAL GATE AND/OR FENCE.

N. *DIRECTOR APPROVAL*. OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE APPROPRIATE BUILDING AND SITE LAYOUTS RELATIVE TO THE SURROUNDING NEIGHBORHOOD AND STREETS.]

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4[7]. *Building Articulation Menu.*

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5[8]. *Northern Climate Weather Protection and Sunlight Menu.*

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[9. *ENTRYWAY TREATMENT*. PRIMARY FRONT ENTRANCES SHALL BE GIVEN EMPHASIS AND PHYSICAL ACCESS AS FOLLOWS. A PORCH, LANDING, PATIO, OR OTHER SEMI-PRIVATE OUTDOOR ENTRYWAY SPACE WITH A MINIMUM DIMENSION OF FOUR FEET SHALL BE PROVIDED THAT IS DISTINGUISHED FROM ADJACENT AREAS AND VEHICULAR PARKING BY A VERTICAL SEPARATION OR CHANGE IN SURFACE MATERIAL. EXAMPLES OF FEATURES THAT MEET THE INTENT OF THE SECTION INCLUDE THE FOLLOWING:

- A. OUTDOOR SHELTERING ROOF STRUCTURE SUCH AS AN OVERHANG, RECESS, PORTICO, OR OTHER PERMANENT ARCHITECTURAL FEATURE COVERING AT LEAST 12 SQUARE FEET.

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- B. FAÇADE VARIATION THROUGH WALL MODULATION (CHANGES IN WALL PLANE) OR CHANGES IN EXTERIOR FINISHES (COLOR OR MATERIALS) THAT RELATE TO AND EMPHASIZE THE ENTRY.
- C. ENTRY DETAIL ELEMENTS SUCH AS DOUBLE DOORS, ENTRY SIDE LIGHT OR TRANSOM WINDOWS, OR PLANTER BOXES.
- D. FRONT STEPS CREATING A VERTICAL SEPARATION, AND/OR VERTICAL VEGETATION OR LOW FRONT FENCING, TO DEFINE THE TRANSITION FROM PUBLIC TO MORE PRIVATE OUTDOOR ENTRYWAY SPACE.
- E. LANDSCAPING AND PEDESTRIAN FEATURES, NOT OTHERWISE REQUIRED BY THIS TITLE, SUCH AS INTEGRATED PLANTERS, PEDESTRIAN-SCALE LIGHTING OR ACCENT LIGHTING, OR SPECIAL PAVING TREATMENTS.
- F. OTHER METHODS AS APPROVED BY THE DIRECTOR THAT EMPHASIZE PRIMARY FRONT ENTRANCES.]

*(Note to code revisor: Re-number remaining subsections.)*

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D. *Standards for some single-family and two-family residential structures.*

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*(Note to code revisor: Re-number table 21.07-10 to become table 21.07-16.)*

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[3. **PRIMARY ENTRANCE.**

- A. A PORCH OR LANDING WITH AT LEAST 16 SQUARE FEET SHALL BE PROVIDED AT THE PRIMARY ENTRANCE. THE PORCH OR LANDING SHALL BE COVERED BY A ROOF OF AT LEAST 16 SQUARE FEET.
- B. THE PRIMARY ENTRANCE OF EACH RESIDENCE AND THE WALKWAY TO THAT ENTRANCE SHALL BE CLEARLY VISIBLE FROM THE STREET.



1 PRIMARY ENTRANCES SHALL NOT BE LOCATED  
2 ON THE REAR OF THE STRUCTURE.  
3

4 C. A HARD-SURFACED PEDESTRIAN WALKWAY  
5 SHALL BE PROVIDED FROM THE STREET,  
6 SIDEWALK, OR DRIVEWAY TO THE PRIMARY  
7 ENTRANCE. ROOF DRAINAGE SHALL NOT FALL  
8 UPON THE WALKWAY.  
9

10 4. GARAGES.

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12 A. WHERE A GARAGE (WITH NO HABITABLE FLOOR  
13 AREA ABOVE) EXTENDS FROM THE REST OF THE  
14 STRUCTURE TOWARDS THE STREET, THE WIDTH  
15 OF THE NON-GARAGE PORTION OF THE FRONT  
16 BUILDING ELEVATION SHALL BE NO LESS THAN  
17 THE LENGTH THAT THE GARAGE EXTENDS FROM  
18 THE REST OF THE STRUCTURE.  
19

20 B. GARAGE DOORS FACING THE STREET SHALL  
21 COMPRISE NO MORE THAN 67 PERCENT OF THE  
22 TOTAL WIDTH OF A DWELLING'S BUILDING  
23 ELEVATION.  
24

25 C. DWELLING UNITS WITH A STREET-FACING  
26 BUILDING ELEVATION THAT IS 40 FEET WIDE OR  
27 NARROWER AND WITH GARAGE DOORS THAT  
28 FACE THE STREET SHALL FEATURE AT LEAST  
29 ONE DESIGN ELEMENT FROM EACH OF THE  
30 THREE LISTS BELOW.  
31

32 I. LIST A:

33  
34 (A) AT LEAST ONE DORMER THAT IS  
35 ORIENTED TOWARD THE STREET.  
36

37 (B) THE FRONT BUILDING ELEVATION  
38 HAS TWO OR MORE FACADES THAT  
39 ARE OFFSET BY AT LEAST 16  
40 INCHES. EACH FAÇADE OR A  
41 COMBINATION OF OFFSET FACADES  
42 SHALL BE AT LEAST ONE THIRD OF  
43 THE AREA OF THE BUILDING  
44 ELEVATION.  
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(C) FRONT-FACING BALCONY, ACCESSIBLE FROM A HABITABLE ROOM, AT LEAST SIX FEET WIDE, THAT PROJECTS FROM A FAÇADE AT LEAST TWO FEET AND IS ENCLOSED BY AN OPEN RAILING.

II. LIST B:

(A) A PRIMARY ENTRANCE AREA WITH A COVERED PORCH OR LANDING AT LEAST EIGHT FEET WIDE, INCORPORATING VISUAL ENHANCEMENTS SUCH AS GABLED ROOF FORMS, ROOF BRACKETS, FASCIA BOARDS, SIDE LIGHTS, AND/OR ORNAMENTAL COLUMNS DIVIDED VISUALLY INTO TOP, MIDDLE, AND BOTTOM.

(B) A BAY WINDOW ON THE FRONT ELEVATION AT LEAST SIX FEET WIDE THAT EXTENDS A MINIMUM OF 12 INCHES OUTWARD FROM A FAÇADE, FORMING A BAY OR ALCOVE IN THE ROOM WITHIN.

(C) IF THE GARAGE IS MORE THAN ONE CAR WIDE, MULTIPLE GARAGE DOORS ARE USED.

III. LIST C:

(A) WINDOWS AND PRIMARY ENTRANCE DOOR(S) THAT OCCUPY A MINIMUM OF 25 PERCENT OF THE WALL AREA OF THE FRONT ELEVATION. WINDOWS IN THE GARAGE DOOR DO NOT COUNT TOWARDS THE 25 PERCENT.

(B) TRIM (MINIMUM THREE AND ONE HALF INCHES WIDE) OF A DIFFERENT COLOR FROM THE PRIMARY SIDING COLOR, SHALL OUTLINE ALL WINDOWS, DOORS, AND ROOF

1 EDGES ON THE FRONT BUILDING  
2 ELEVATION, AND MAY OUTLINE  
3 CORNERS AND  
4 PROJECTIONS/RECESSES ON THE  
5 FRONT BUILDING ELEVATION.  
6

7 (C) A MINIMUM OF TWO DIFFERENT  
8 SIDING MATERIALS AND/OR  
9 PATTERNS ARE USED ON THE  
10 FRONT BUILDING ELEVATION.  
11 DOORS AND TRIM DO NOT QUALIFY  
12 AS A TYPE OF SIDING MATERIAL.  
13

14 D. THE HOUSE MAY ENCROACH INTO THE PRIMARY  
15 FRONT SETBACK BY UP TO FIVE FEET WHEN  
16 THERE IS NO GARAGE, OR WHERE THERE IS A  
17 GARAGE (ATTACHED OR DETACHED) WHERE  
18 THE FRONT WALL OF THE GARAGE IS LOCATED  
19 AT LEAST 8 FEET BEHIND THE FRONT FAÇADE OF  
20 THE HOUSE.  
21

22 5. *WINDOWS.*  
23

24 A. *MINIMUM WINDOW AREA REQUIREMENT.*  
25 WINDOWS AND PRIMARY ENTRANCE DOOR(S)  
26 SHALL OCCUPY A MINIMUM OF 10 PERCENT OF  
27 THE WALL AREA OF A BUILDING ELEVATION  
28 FACING A STREET OR REQUIRED PRIVATE  
29 COMMON OPEN SPACE (UP TO A MAXIMUM OF  
30 TWO ELEVATIONS). THE DIRECTOR MAY  
31 ELIMINATE OR REDUCE THE REQUIREMENT ON  
32 ONE ELEVATION (IN INSTANCES WHERE UP TO  
33 TWO APPLY) BASED ON SITE SPECIFIC  
34 CIRCUMSTANCES, STRUCTURAL  
35 REQUIREMENTS FOR SHEAR, AND  
36 ORGANIZATION OF SPACES IN THE HOME  
37 (WINDOWS ARE NOT REQUIRED IN ROOMS NOT  
38 NORMALLY INHABITED OR IN GARAGES).  
39

40 B. *WALL AREA CALCULATION.* WALL AREA  
41 CALCULATION SHALL BE IN ACCORDANCE WITH  
42 SUBSECTION 21.07.110C.3.B.  
43

44 C. AN OVERALL REDUCTION IN REQUIRED WINDOW  
45 AREA MAY BE APPROVED IF DEMONSTRATED BY  
46 CALCULATION BY AN ENERGY RATER CERTIFIED

1 BY THE STATE OF ALASKA THAT INSTALLING THE  
2 REQUIRED WINDOW AREA WILL REDUCE THE  
3 ENERGY RATING BELOW A 5-STAR ENERGY  
4 RATING.

5  
6 D. *NARROW LOT/SMALL HOME REDUCTION.* AN  
7 OVERALL REDUCTION IN REQUIRED WINDOW  
8 AND PRIMARY ENTRANCE DOOR AREA TO 7%  
9 MAY BE APPROVED WHEN THE CALCULATED  
10 WALL AREA IS UNDER 500 SQUARE FEET. UNITS  
11 USING THIS REDUCTION SHALL COMPLY WITH  
12 SUBSECTION 21.07.110D.4.C.

13  
14 E. *ADDITIONS AND RENOVATIONS.* ON EXISTING  
15 STRUCTURES, ONLY THE WALL AREA AFFECTED  
16 BY EITHER AN ADDITION OR A RENOVATION  
17 SHALL BE REQUIRED TO COMPLY WITH THE  
18 OPENING REQUIREMENTS. UNAFFECTED WALL  
19 AREAS NEED NOT COMPLY. GARAGE ADDITIONS  
20 ARE EXEMPT FROM OPENING REQUIREMENTS.  
21 THE DIRECTOR MAY ELIMINATE OR REDUCE THE  
22 REQUIREMENT FOR WINDOW OPENINGS ON  
23 ADDITIONS/RENOVATIONS BASED ON PROJECT-  
24 SPECIFIC CIRCUMSTANCES (SUCH AS THE  
25 NATURE OF THE SPACES BEING ADDED,  
26 BUILDING ORIENTATION, OR STRUCTURAL  
27 REQUIREMENTS).]

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29 [E. *PROHIBITED STRUCTURES.* [RESERVED]]

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31 E[F]. *Site Design.*  
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33 [3. *DRIVEWAY WIDTH.*

34 A. PURPOSE. THIS SECTION LIMITS THE WIDTH OF  
35 A DRIVEWAY AT THE PROPERTY LINE AND AT  
36 THE STREET CURB. THE INTENT OF THESE  
37 LIMITATIONS IS TO PROVIDE ADEQUATE SPACE  
38 FOR SNOW STORAGE WITHIN THE RIGHT-OF-  
39 WAY, TO HAVE SPACE FOR ON-STREET PARKING  
40 WHERE APPROPRIATE, AND TO DISCOURAGE  
41 THE MAJORITY OF THE FRONT AREA OF A LOT  
42 FROM BEING PAVED AND/OR USED FOR VEHICLE  
43 PARKING.

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45 B. *APPLICABILITY.*  
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I. THIS SECTION APPLIES TO DRIVEWAY THROAT WIDTH AT THE PROPERTY LINE AND STREET CURB.

II. RESIDENTIAL DRIVEWAYS ARE ALSO SUBJECT TO THE MUNICIPAL DRIVEWAY STANDARDS CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER. WHERE THERE IS A CONFLICT, THIS SECTION SHALL GOVERN. ACCESS TO STREETS OWNED BY THE STATE OF ALASKA REQUIRES COMPLIANCE WITH STATE DRIVEWAY STANDARDS, AS PROVIDED IN SUBSECTION 21.07.090H.8.D.

III. WHEN A DRIVEWAY SERVES BOTH RESIDENTIAL AND NONRESIDENTIAL PRINCIPAL USES, THE DRIVEWAY DIMENSIONS SHALL BE AS REQUIRED FOR THE NONRESIDENTIAL USE, UNLESS APPROVED OTHERWISE BY THE TRAFFIC ENGINEER.

C. *PERCENT OF LOT FRONTAGE.* THE TOTAL WIDTH OF DRIVEWAY ENTRANCES TO A RESIDENTIAL LOT FROM A STREET SHALL NOT EXCEED 40 PERCENT OF THE FRONTAGE OF THE LOT, OR 33 PERCENT OF THE FRONTAGE IF THE PLATTING AUTHORITY OR TRAFFIC ENGINEER FINDS THAT CONDITIONS WARRANT IT.

I. A DRIVEWAY FOR MULTIFAMILY DWELLINGS, MIXED-USE DWELLINGS, OR A GROUP LIVING USE MAY ALWAYS BE AT LEAST 14 FEET WIDE.

II. A DRIVEWAY FOR A SINGLE-FAMILY, TWO-FAMILY, OR TOWNHOUSE DWELLING MAY ALWAYS BE AT LEAST TEN FEET WIDE, PROVIDED THE TRAFFIC ENGINEER DETERMINES SNOW STORAGE, TRAFFIC FLOW AND SAFETY, AND THE URBAN CONTEXT ARE ADDRESSED, AND PROVIDED TOWNHOUSE DRIVEWAYS ARE ATTACHED IN PAIRS TO THE MAXIMUM EXTENT FEASIBLE.

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III. FLAG LOTS ARE EXEMPT FROM THE PERCENTAGE LIMITATIONS, BUT SHALL HAVE A MAXIMUM DRIVEWAY WIDTH OF 20 FEET. ABUTTING FLAG LOTS MAY SHARE A DRIVEWAY UP TO 24 FEET WIDE (12 FEET PER LOT).

D. *EXCEPTIONS.* THE TRAFFIC ENGINEER AND THE PLANNING DIRECTOR MAY APPROVE A DEPARTURE FROM THE STANDARDS OF THIS SECTION, SUCH AS A NARROWER DRIVEWAY, IF DOCUMENTATION PREPARED BY A LICENSED PROFESSIONAL IN THE STATE OF ALASKA REGISTERED WITH THE ALASKA STATE BOARD OF REGISTRATION FOR ARCHITECTS, ENGINEERS AND LAND SURVEYORS DEMONSTRATES THE DRIVEWAY STILL MEETS THIS CHAPTER'S STANDARDS AND THE MUNICIPAL DRIVEWAY STANDARDS MEMO ISSUED BY THE MUNICIPAL TRAFFIC ENGINEER, AND THAT CHANGE IS APPROPRIATE. APPROVAL SHALL BE CONTINGENT ON FACTORS SUCH AS STREET CLASSIFICATION, STREET TYPOLOGY, URBAN CONTEXT, TRAFFIC VOLUME AND SPEED, CURB RETURN RADII, STREET TRAVEL LANE OFFSET FROM FACE OF CURB, PEDESTRIAN AND BICYCLE FACILITIES, SNOW STORAGE, DRIVEWAY CONFIGURATION AND LENGTH, SITE AND PROJECT CHARACTERISTICS, NUMBER OF VEHICLES EXPECTED TO USE THE DRIVEWAY, AND COMPREHENSIVE PLAN POLICIES. THE TRAFFIC ENGINEER AND PLANNING DIRECTOR MAY ALSO BE MORE RESTRICTIVE THAN THE STANDARDS OF THIS SECTION, PROVIDED THEY DOCUMENT THE RATIONALE.

4. *ALLEYS.*

A. ACCESS TO PARKING FOR RESIDENTIAL USES SHALL BE FROM THE ALLEY WHEN THE SITE ABUTS AN ALLEY, EXCEPT THAT STREET ACCESS IS PERMITTED IN ANY OF THE FOLLOWING SITUATIONS:

I. ACCESS TO A TOWNHOUSE DWELLING ON A CORNER LOT MAY BE FROM THE STREET

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FRONTAGE HAVING THE SECONDARY FRONT SETBACK OR THE ALLEY.

- II. DUE TO THE RELATIONSHIP OF THE ALLEY TO THE STREET SYSTEM AND/OR THE PROPOSED HOUSING DENSITY OF THE DEVELOPMENT, THE TRAFFIC ENGINEER DETERMINES THAT USE OF THE ALLEY FOR PARKING ACCESS WOULD BE A SIGNIFICANT TRAFFIC IMPACT OR SAFETY HAZARD.
- III. THE TRAFFIC ENGINEER DETERMINES THAT TOPOGRAPHY OR OTHER NATURAL FEATURE OR PHYSICAL BARRIER MAKES ALLEY ACCESS INFEASIBLE.
- IV. THE ALLEY IS NOT IMPROVED AND TRAFFIC ENGINEER DETERMINES THAT IMPROVEMENT IS NOT FEASIBLE.
- V. A SINGLE-FAMILY DWELLING, TWO-FAMILY DWELLING, OR TOWNHOUSE DWELLING WITH TWO UNITS, WITH ALLEY ACCESS MAY HAVE A GARAGE OR DRIVEWAY THAT FACES THE STREET IF THE GARAGE DOOR IS NO WIDER THAN 10 FEET AND THE DRIVEWAY NO WIDER THAN 12 FEET AT ANY POINT.

B. IN SITUATIONS WHERE A GROUP OF LOTS FRONT AN ENTIRE BLOCK ON ONE SIDE OF A STREET BETWEEN TWO INTERSECTIONS, ABUT A MID-BLOCK ALLEY, AND ARE BEING DEVELOPED TOGETHER, THEN PARKING ACCESS TO THE STRUCTURES SHALL BE FROM THE ALLEY, AND BUILDING(S) MAY ENCROACH INTO THE FRONT SETBACK BY UP TO FIVE FEET.

C. IF A NEW DEVELOPMENT INCLUDES ALLEYS, THE LOT DEPTH REQUIREMENT IS REDUCED BY HALF THE WIDTH OF THE ALLEY AND THE LOT AREA REQUIREMENT IS REDUCED BY 12 PERCENT FOR THOSE LOTS THAT ABUT AN ALLEY. VEHICULAR ACCESS TO ALL DWELLING UNITS ON LOTS ABUTTING ALLEYS SHALL BE FROM THE ALLEY,

1 AND VEHICULAR ACCESS TO SUCH UNITS FROM  
2 THE STREET IS PROHIBITED.]

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4 (Note to code revisor: Re-number subsequent subsections in section  
5 21.07.110, and re-number tables 21.07-11 and 21.07-12. Re-  
6 number table 21.07-13 in chapter 21.07 section 21.07.115.)  
7

8 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-132, 11-5-  
9 14; AO 2015-36, 5-14-15; AO 2015-100, 10-13-15; AO 2016-34(S), 4-12-  
10 16; AO 2016-136, 11-15-16; AO 2017-160, 12-19-17; AO 2018-59. 7-31-  
11 2018; AO 2019-132, 12-3-19; AO 2020-38, 4-28-20)

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13 **Section 4.** Anchorage Municipal Code Section 21.11.070, Development and  
14 Design Standards, is hereby amended to read as follows (*the remainder of the*  
15 *chapter is not affected and therefore not set out*):  
16

17 **21.11.070 Development and Design Standards.**

18 \*\*\* \*\*

19 **F. Off-Street Parking and Loading.**

20 \*\*\* \*\*

21 3. *Off-street parking loading design standards.* If off-street  
22 parking and/or loading is provided, it shall comply with all  
23 standards for off-street parking and loading in section  
24 21.07.090. [, EXCEPT THAT:] Small and compact parking  
25 space dimensions specific to the DT districts are provided in  
26 section 21.07.090H.12.  
27

28 [A. PERMANENT PARKING NOT LOCATED WITHIN A  
29 SETBACK FROM PROJECTED RIGHTS OF WAY  
30 MAY UTILIZE THE ALTERNATIVE PARKING ANGLE,  
31 STALL, AND AISLE DIMENSIONS OUTLINED IN  
32 TABLE 21.11-9. AN ALTERNATIVE PARKING SITE  
33 PLAN SHALL BE SUBMITTED AND APPROVED AS  
34 PART OF THE APPLICABLE LAND USE PERMIT  
35 PROCESS.  
36

37 B. PERMANENT PARKING NOT LOCATED WITHIN A  
38 SETBACK FROM PROJECTED RIGHTS OF WAY  
39 MAY UTILIZE THE COMPACT PARKING ANGLE,  
40 STALL, AND AISLE DIMENSIONS OUTLINED IN  
41 TABLE 21.11-10, PROVIDED THE PARKING AREA  
42 IS USED EXCLUSIVELY FOR EMPLOYEE PARKING  
43 FOR PERIODS IN EXCESS OF FOUR  
44 CONSECUTIVE HOURS, AND NO MORE THAN 30  
45 PERCENT OF THE TOTAL NUMBER OF SPACES IS  
46 DESIGNED FOR COMPACT CARS. AN



ALTERNATIVE PARKING SITE PLAN SHALL BE  
 SUBMITTED AND APPROVED AS PART OF THE  
 APPLICABLE LAND USE PERMIT PROCESS.]

(Note to code revisor: Delete the following tables 21.11-9 and 21.11-10.)

[TABLE 21.11-9: ALTERNATIVE PARKING ANGLE, STALL, AND AISLE DIMENSIONS]

[PARKING ANGLE (DEGREES)]	[SPACE WIDTH]	[SPACE DEPTH (VEHICLE PROJECTION)]	[AISLE WIDTH 1-WAY]	[TYPICAL PARKING BAY WIDTH (MODULE)]	[INTERLOCK REDUCTION]	[OVERHANG ALLOWANCE]
[45]	[8' 4"]	[17' 4"]	[12' 3"]	[46' 11"]	[2' 0"]	[2' 0"]
[50]	[8' 4"]	[18' 0"]	[12' 9"]	[48' 9"]	[1' 10"]	[2' 1"]
[60]	[8' 4"]	[18' 10"]	[14' 3"]	[51' 11"]	[1' 4"]	[2' 3"]
[70]	[8' 4"]	[19' 2"]	[16' 1"]	[54' 5"]	[0' 10"]	[2' 5"]
[75]	[8' 4"]	[19' 0"]	[17' 6"]	[55' 6"]	[0' 8"]	[2' 6"]
[90]	[8' 4"]	[18' 0"]	[22' 6"]	[58' 6"]	[N/A]	[2' 8"]

[\*THE 90-DEGREE PARKING ANGLE DIMENSIONS ASSUME A TWO-WAY TRAFFIC FLOW.]

[TABLE 21.11-10: COMPACT PARKING ANGLE, STALL, AND AISLE DIMENSIONS]

[PARKING ANGLE (DEGREES)]	[SPACE TYPE]	[SPACE WIDTH]	[SPACE DEPTH (VEHICLE PROJECTION)]	[AISLE WIDTH 1-WAY]	[TYPICAL PARKING BAY WIDTH (MODULE)]	[INTERLOCK REDUCTION]	[OVERHANG ALLOWANCE]
[45]	[CO]	[7' 7"]	[15' 2"]	[10' 9"]	[41' 1"]	[1' 6"]	[1' 6"]
	[STD]	[8' 4"]	[18' 4"]	[13' 0"]	[49' 8"]	[2' 0"]	[2' 3"]
[50]	[CO]	[7' 7"]	[15' 8"]	[11' 2"]	[42' 6"]	[1' 4"]	[1' 7"]
	[STD]	[8' 4"]	[19' 2"]	[13' 6"]	[51' 0"]	[2' 0"]	[2' 4"]
[60]	[CO]	[7' 7"]	[16' 4"]	[12' 6"]	[45' 2"]	[1' 0"]	[1' 8"]
	[STD]	[8' 4"]	[20' 0"]	[15' 0"]	[55' 0"]	[1' 6"]	[2' 6"]
[70]	[CO]	[7' 7"]	[16' 5"]	[14' 1"]	[46' 11"]	[0' 8"]	[1' 10"]
	[STD]	[8' 4"]	[20' 4"]	[17' 0"]	[57' 8"]	[1' 0"]	[2' 8"]
[75]	[CO]	[7' 7"]	[16' 6"]	[16' 4"]	[49' 11"]	[0' 6"]	[1' 10"]
	[STD]	[8' 4"]	[20' 2"]	[18' 0"]	[58' 4"]	[0' 9"]	[2' 9"]
[90*]	[CO]	[7' 7"]	[15' 6"]	[19' 0"]	[50' 0"]	[N/A]	[2' 0"]
	[STD]	[8' 4"]	[19' 0"]	[23' 0"]	[61' 0"]	[N/A]	[3' 0"]

[CO: COMPACT CAR.] [STD: STANDARD CAR.]

[\*THE 90-DEGREE PARKING ANGLE DIMENSIONS ASSUME A TWO-WAY TRAFFIC FLOW.]

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(AO 2020-38, 4-28-20; AO 2020-93, 10-1-20)

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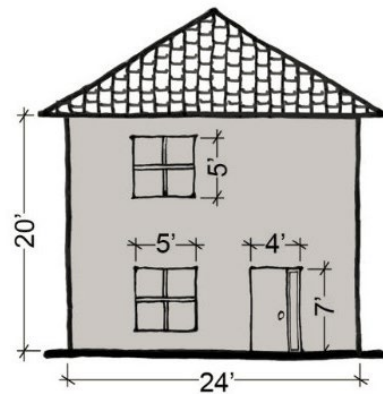
1 **Section 5.** Anchorage Municipal Code Chapter 21.15, Rules of Construction  
2 and Definitions, is hereby amended to read as follows (*the remainder of the*  
3 *chapter is not affected and therefore not set out*):  
4

5 **21.15.020 Rules of Construction and Interpretation.**

6 \*\*\* \*\*

7 **P.** Measurement of Windows and Entrances as a Percentage of  
8 Building Wall Area. The percentage of building elevation wall area  
9 that is window and entrance openings is determined by dividing the  
10 total square footage of the windows and primary entrances on the  
11 building elevation by the gross square footage of the building  
12 elevation wall area (except for parts of the building exempted in 3.),  
13 and then multiplying the resulting quotient by 100.

14  
15 1. Illustrated Example. In the  
16 illustration, the building elevation  
17 wall area is 20 feet of height x 24  
18 feet of length = 480 square feet.  
19 The window and primary  
20 entrance area is the area of the  
21 two windows (5 x 5 feet x 2) plus  
22 the area of the primary entrance  
23 and its sidelight (4 x 7 feet) = 78  
24 square feet. The percentage of  
25 the building elevation wall area is  
26 found by dividing 78 by 480 then  
27 multiplying by 100, to yield 16%.



28  
29 2. Measuring Window and Primary Entrance Area. “Window  
30 area” is defined in section 21.15.040, Definitions. Primary  
31 entrance area includes the entrance opening and door frame  
32 but excludes trim, molding, and other features beyond the  
33 door frame. “Primary entrance” is defined in 21.15.040,  
34 Definitions.

35  
36 3. Exempt Parts of the Building Elevation Wall Area. The  
37 building elevation wall area is measured as the exterior wall  
38 plane surface area from finished grade to the top of the wall  
39 on the building except that, for the purposes of measuring  
40 windows and entrances as a percentage of building wall area,  
41 the following parts of the building are not included in the wall  
42 area calculation:

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44 a. Wall area above the topmost story’s top plate, such as  
45 gable ends of a roof enclosing a cold attic, or parapet  
46 walls.

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- b. Roof surfaces, eaves, fascia, vent stacks, mechanical equipment and rooms, rooftop access enclosures, and other roof appurtenances.
- c. Wall area of stories below grade plane, such as below grade parking, up to the finished floor of the story above grade plane.
- d. Parts of a street-facing building elevation below the grade plane of the abutting sidewalk (or edge of street pavement where there is no sidewalk).



\*\*\*      \*\*\*      \*\*\*  
(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13)

**21.15.040 Definitions.**

\*\*\*      \*\*\*      \*\*\*

**Car Share Program**

A membership program that offers an alternative to car ownership under which persons or entities that become members are permitted to use vehicles from a fleet on an hourly basis, or in smaller intervals. Vehicles are available for pick-up by eligible members 24 hours per day. Automobile insurance and maintenance are covered by the car share program and individual members pay for usage and mileage fees.

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**Complete Street**

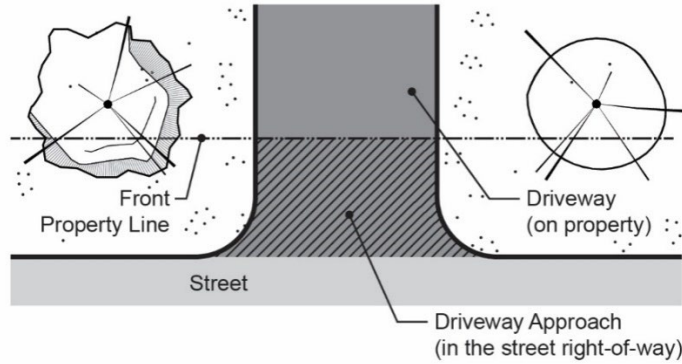
See Street, Complete.

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**Driveway Approach**

The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The driveway approach extends from the street edge to the front property line.



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**Parking Cash-Out**

A program offered by an employer that provides employees with a choice of foregoing any subsidized/free parking for a cash payment equivalent to the cost of the parking space to the employer[ALLOWING COMMUTERS TO CHOOSE CASH INSTEAD OF A FREE PARKING SPACE. FOR EXAMPLE, A COMMUTER WHO IS OFFERED A PARKING CASH-OUT MIGHT BE ABLE TO CHOOSE TO RECEIVE \$50 PER MONTH IN CASH IF THEY USE AN ALTERNATIVE MODE BESIDES A SINGLE-OCCUPANCY VEHICLE].

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**Parking, Unbundled**

Parking spaces that are leased or sold separately from the rental or purchase fees for the dwelling units or building floor area rather than included in the rental or purchase fees, such that potential tenants or buyers have the option of renting or buying a dwelling unit or building floor area at a lower price with fewer or no parking spaces. The rental or purchase of a parking space is at the discretion and direct cost of the tenant or buyer [PARKING RENTED AND SOLD SEPARATELY FROM BUILDING SPACE, SO OCCUPANTS ONLY PAY FOR THE AMOUNT OF PARKING THEY WANT].

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**Pedestrian Feature**

A permanent object that provides pedestrians with increased convenience, comfort, and utility, and which is publicly accessible and not limited to a tenant or establishment such as seating for a restaurant. Pedestrian features include:

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- Seating such as benches accommodating several people;
- Secondary/informal seating opportunities such as steps, pedestals, low walls, or edges of fountains, accommodating several people;

- A space for standing with objects to lean against, such as bollards, short fences, or irregular building facades, accommodating several people;
- A tree or raised planter;
- A work of art such as a water feature, sculpture, cultural exhibit, or clock feature;
- Pedestrian-scale lighting;
- A cross-country ski rack that can accommodate a ski lock and is located at building entrances within 500 feet of a ski trailhead;
- A winter city urban design feature such as a wind screen, or outdoor stove or space heater; or
- Other object supporting pedestrian utility, such as a gazebo or kiosk.

### **Pedestrian Movement Zone**

The middle portion of an enhanced street sidewalk (21.07.060G.19.), comprising its walkway clear width located between the sidewalk's street interface and building interface zones. The pedestrian movement zone provides for the primary function of sidewalks[,] and is kept clear of any obstructions to pedestrian movement.

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### **Ride Hailing Services**

A business that matches passengers with for-hire personal vehicles with drivers using software applications. Ride hailing services are different from other for-hire vehicles with drivers, such as taxicabs, in that they are not hailed from the street.

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[PRIMARY PEDESTRIAN WALKWAY

A WALKWAY MEETING THE STANDARDS OF SUBSECTION  
21.07.060F.4.]

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### **Street, Complete**

A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities, and for all users to safely move through the transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available on the municipal website.

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### **Walkway Clear Width**

That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed, continuous (i.e., without interruption or being split by obstructions), and kept clear for pedestrian movement.

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[WALKWAY, PRIMARY PEDESTRIAN  
 SEE PRIMARY PEDESTRIAN WALKWAY.]

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(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-132, 11-5-14; AO  
 2015-82, 7-28-15; AO 2015-100, 10-13-15; AO 2015-138, 1-12-16; AO 2015-  
 133(S), 2-23-16; AO 2015-142(S-1), 6-21-16; AO 2016-3(S), 2-23-16; AO  
 2016-144(S), 12-20-16; AO 2017-55, 4-11-17; AO 2017-75, 5-9-2017; AO  
 2018-12, 2-27-18; AO 2018-67(S-1), 10-9-18; AO 2018-92, 10-23-18; AO  
 2019-132, 12-2-19; AO 2020-38, 4-28-20)

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**Section 6.** Anchorage Municipal Code of Regulations Section 21.20.007,  
 Schedule of Fees - Miscellaneous Fees, is hereby amended to read as follows (*the  
 remainder of the chapter is not affected and therefore not set out*):

**21.20.007 Schedule of Fees – Miscellaneous Fees**

The following fees shall be paid for the services described:

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***	***	***
H.	New and revised parking layouts:	
	1. Site plan review only	\$415.00
	2. Complete site plan review:	
	a. 1[0]-10 spaces	\$415.00 plus \$16.50 per space
	b. 11—50 spaces	\$515.00 plus \$12.50 per space
	c. 51—200 spaces	\$620.00 plus \$8.25 per space
	d. 201—1,000 spaces	\$1,035.00 plus \$2.05 per space
	e. 1,001 + spaces	\$1,240.00 plus \$1.05 per space
I.	Agreements with the Municipality	
	Parking or[AND] access agreement[S]	\$235.00 per agreement
	<u>Discretionary parking reductions review by Traffic Engineer                      (This fee does not apply to non-discretionary parking reductions                      listed in table 21.07-9.)</u>	<u>\$135.00/hour                      (two-hour minimum                      fee)</u>
H[I].	Sign permits:	
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1 (GAAB 21.05.090; AO No. 77-407; AR No. 78-12; AR No. 79-55; AO No.  
2 82-49; AR No. 83-96; AR No. 83-289(S); AR No. 86-63; AR No. 86-263;  
3 AR No. 87-315; AO No. 87-154(S); AR No. 90-151; AR No. 93-327(S), §  
4 1, 2-22-94; AO No. 2001-145(S-1), § 23, 12-11-01; AO No. 2003-97, § 3,  
5 9-30-03; AO No. 2003-152S, § 25, 1-1-04; AO No. 2004-1, § 4, 1-1-03; AR  
6 No. 2006-137, § 1, 6-6-06; AR No. 2008-134, § 4, 7-29-08; AO No. 2013-  
7 100, § 16, 1-1-14; AO No. 2018-100(S) , § 15, 1-1-19; AO No. 2019-  
8 116(S), § 11, 1-1-20)

9  
10 Authority—Anchorage Municipal Code 3.40, 21.20.050; AO No. 77-407.

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12  
13 **Section 7.** This ordinance shall become effective 60 calendar days after  
14 passage and approval by the Assembly.

15  
16 PASSED AND APPROVED by the Anchorage Assembly this \_\_\_\_\_ day of  
17 \_\_\_\_\_, 2022.

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23 Chair

24 ATTEST:

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27 \_\_\_\_\_  
28 Municipal Clerk

29  
30 (Planning and Zoning Commission Case No. 2022-0026)