

**Clean Version with Proposed Amendments**  
**(Non-annotated)**

***Title 21 Text Amendment to Off-Street Parking and Site  
Access Regulations***

**Public Hearing Draft  
PZC Case No. 2022-0026**

***Anchorage 2040 Land Use Plan  
Implementation Actions 4-3 and 4-6***

*This page intentionally left blank.*

# Annotated Zoning Code Amendments

This version of the proposed amendments shows amended code sections as they would look in the published code book after final adoption. It includes indications of where sections of code are removed. For additional detail, please refer to the *Annotated Zoning Code Amendments* document (Attachment 2).

The text amendments are arranged in the order they appear in Anchorage’s *Title 21 Zoning Ordinance*. Only the sections of Title 21 that are being added or amended are included. Where a full subsection of current code is deleted, a note to the reader in italics and parentheses indicates the proposed deletion.

Asterisks (\*\*\*) indicates that all intervening parts of the chapter are unchanged between the section header above the asterisks and the language shown below the asterisks.

Proposed new sections are marked “(New)” in the table of contents below:

<u>Zoning Code Section Affected by Proposed Changes</u>	<u>Page</u>
21.04. Zoning Districts ( <i>district-specific standards</i> )	1
21.07.015. Neighborhood Development Context Areas ( <i>new section</i> )	2
21.07.060E. Standards for Pedestrian Facilities ( <i>sidewalks and walkways</i> )	9
21.07.060F. Pedestrian Frontage Standard ( <i>new section</i> )	10
21.07.060G. Pedestrian Amenities ( <i>renumbered from F. to G.; various amendments</i> )	13
21.07.090A. - C. Off-street Parking and Loading, Purpose; Applicability; and Computation	17
21.07.090D. Off-street Parking and Loading, Parking Lot Layout and Design Plan	20
21.07.090E. Off-Street Parking Requirements	21
21.07.090F. Parking Reductions and Alternatives ( <i>various amendments</i> )	24
21.07.090H.8. - 11. Vehicular Access and Circulation ( <i>renumbered to H.8. through H.11.</i> )	34
21.07.090H.12. Dimensions of Parking Spaces and Aisles ( <i>renumbered from H.9.</i> )	39
21.07.090K. Bicycle Parking Spaces	42
21.07.110 Residential Design Standards ( <i>orientation, driveway, alley standards</i> )	46
21.11.070F. Off-street Parking and Loading ( <i>downtown parking dimensions</i> )	47
21.15. Rules of Interpretation (21.15.020) and Definitions (21.15.040)	47
AMCR 21.20.007. Schedule of Fees	50

*This page intentionally left blank.*

1 **CHAPTER 21.04: ZONING DISTRICTS**

2 \*\*\* \*\*

3 **21.04.020 Residential Districts**

4 \*\*\* \*\*

5 **H. R-3A: Residential Mixed-Use District**

6 \*\*\* \*\*

7 **2. District-Specific Standards**

8 \*\*\* \*\*

9 *(Note: Subsection 21.04.020H.2.e., Reduced Parking Ratios, is deleted.)*

11 **e. Enhanced Street Sidewalk**

12 An enhanced street sidewalk may be provided in lieu of required sidewalks and site  
13 perimeter landscaping, as provided in 21.07.060G.19.

14 \*\*\* \*\*

15 **J. R-4A: Residential Mixed-Use District**

16 \*\*\* \*\*

17 **2. District-Specific Standards**

19 *(Note: Subsection 21.04.020J.2.d., Reduced Parking Ratios, is deleted.)*

20 \*\*\* \*\*

21 **21.04.030 Commercial Districts**

22 \*\*\* \*\*

23 **G. Standards for Mixed-Use Development in the B-1A and B-1B Districts**

24 \*\*\* \*\*

25 *(Note: Subsection 21.04.020G.5., Reduced Parking Ratios, is deleted.)*

27 **5. Enhanced Street Sidewalk**

28 An enhanced street sidewalk may be provided in lieu of required sidewalks and site perimeter  
29 landscaping, as provided in 21.07.060G.19.

31 *(Note: Subsection 21.04.020G.7., Building Orientation and Placement, is deleted.)*

33 **H. Standards for Mixed-Use Development in the B-3 District**

34 \*\*\* \*\*

35 *(Note: Subsections 21.04.020H.3., Building Placement and Orientation, and H.4., Reduced Parking*  
36 *Ratios, are deleted.)*

38 **3. Enhanced Street Sidewalk**

39 An enhanced street sidewalk may be provided in lieu of required sidewalks and site perimeter  
40 landscaping, as provided in 21.07.060G.19.

41 \*\*\* \*\*

42 **21.04.070 Overlay Zoning Districts**

43 \*\*\* \*\*

44 **C. Commercial Center Overlay**

45 \*\*\* \*\*

46 *(Note: Subsection 21.04.070C.4., Reduced Parking Ratios, is deleted.)*

1 **4. Uses Prohibited**

2 Notwithstanding table 21.05-1, the following uses are prohibited: self-storage facilities, vehicle  
3 sales and rentals, major vehicle service and repair, data processing facilities; commercial food  
4 production, and snow disposal sites.

5  
6 **5. Dimensional Standards**

7  
8 *(Note: Subsection 21.04.070C.5.a. is deleted.)*

9 \*\*\* \*\*

10 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

11 \*\*\* \*\*

12 **21.07.015 Neighborhood Development Context Areas**

13  
14 **A. Purpose**

15 This section establishes Neighborhood Development Context Areas that reflect the existing and desired  
16 characteristics of Anchorage’s older, urban neighborhoods as identified in the Comprehensive Plan. The  
17 Neighborhood Development Context Areas are distinguished from one another and suburban parts of the  
18 Anchorage Bowl by their physical and functional characteristics such as: street, alley, and block patterns;  
19 building placement; how buildings and structures relate to surrounding buildings and streets; the diversity,  
20 distribution, and intensity of land uses; and availability of transportation access alternatives such as  
21 pedestrian facilities and public transportation. The areas mapped in this section provide the basis for  
22 development regulations tailored to the unique characteristics and strengths of these neighborhoods and  
23 fulfill their role as recommended in the Comprehensive Plan. These areas are provided relief from some  
24 generally applicable development and design standards in this chapter 21.07 because of their unique  
25 characteristics.

26  
27 **B. Applicability**

- 28  
29 1. The neighborhood development context areas in this section apply as a basis for area-specific  
30 development standards in this chapter 21.07.  
31  
32 2. The neighborhood development context areas in this section shall not be used as a basis to change  
33 the allowed uses or review and approval procedures of the underlying zoning district, district-  
34 specific dimensional standards, or any other standards of this title besides the development and  
35 design standards of chapter 21.07.  
36  
37 3. The department shall provide and maintain maps showing the boundaries of the neighborhood  
38 development context areas at the parcel scale, in this title and in a publicly accessible location.  
39  
40

41 **C. Neighborhood Development Context Areas Established**

42 The following neighborhood development context areas are established and delineated in maps 21.07-1  
43 through 21.07-4 in subsection 21.07.015D. Each subsection below is organized to describe general  
44 character: street, block, and access patterns; building placement and location; and mix of uses.  
45 Descriptions are intent statements providing a basis for area-specific development standards elsewhere in  
46 this chapter 21.07.  
47

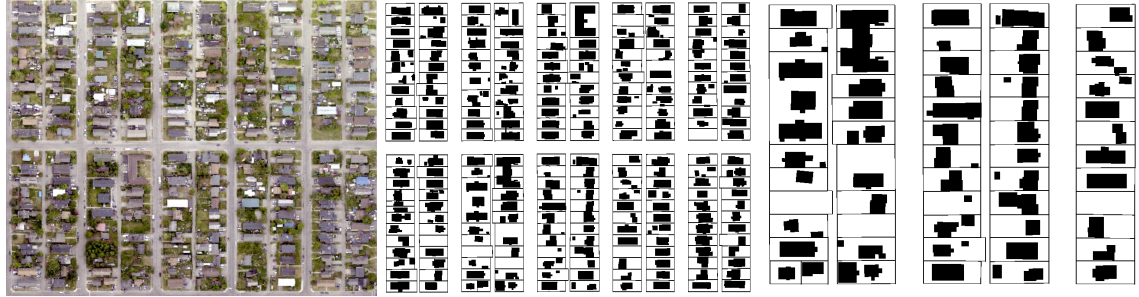
48 **1. Traditional Urban Neighborhood Context**

49 The traditional urban neighborhood context includes Anchorage’s original urban neighborhoods,  
50 such as Government Hill, South Addition, Fairview, and Mountain View. These neighborhoods are  
51 identified by the *Comprehensive Plan, Land Use Plan Map* as Traditional Neighborhood Design  
52 areas. Properties within the Downtown (DT) zoning districts are not included because they are  
53 addressed in chapter 21.11. Traditional urban neighborhood contexts feature a regular street grid  
54 development pattern of city blocks surrounded by streets, which provide pedestrian and vehicular  
55 connections at frequent, regular intervals. Block and lot sizes are small and block shapes are

1  
2  
3  
4  
5  
6  
7

consistent and often the same size. Sidewalks are present on most streets, except in limited parts of southern South Addition and in western Government Hill. There is a consistent presence of alleys. Buildings and residences typically have moderate front setbacks and orient to the street, with landscaping or pedestrian access comprising a majority of the frontage. Parking tends to be located to the side or rear of buildings. There is a balance of pedestrian, bicycle, public transit, and automobile reliance.

8  
9



1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 \*\*\* \*\*

3 **21.07.015 Neighborhood Development Context Areas**

4 \*\*\* \*\*

5 **C. Neighborhood Development Context Areas Established**

6 \*\*\* \*\*

7 **3. Edge Urban Neighborhood Context**

8 The edge urban neighborhood context includes properties generally identified in the  
9 Comprehensive Plan, Land Use Plan Map, as traditional neighborhood design areas in  
10 Anchorage’s early post-war era (1950s-1960s) neighborhoods, such as Spenard, North Star,  
11 Midtown, Rogers Park, Airport Heights, Russian Jack Park, and University Area.

12 Edge urban neighborhoods feature a variety of interconnected street systems with smaller lot and  
13 block sizes than the more suburban parts of the Anchorage Bowl. Some edge urban areas feature  
14 a regular street grid development pattern of city blocks and others provide a more relaxed and  
15 irregular street grid. There is an inconsistent presence of alleys and local street sidewalks.  
16 Buildings typically have moderate to somewhat deeper front setbacks. Buildings tend to orient  
17 toward the public street, although some front setbacks are deep enough to allow for a mix of  
18 landscaping and parking. Some edge urban neighborhood context areas include transit-supportive  
19 development corridors where the Municipality has invested in elevated levels of public  
20 transportation service and enhanced pedestrian facilities.  
21



22

23

24 **4. Transit-Supportive Development Corridors**

25 Transit-supportive development corridors designated by the *Comprehensive Plan* extend through  
26 and outward from the edge urban neighborhood contexts. These corridors are where pedestrian  
27 facilities and frequent public transportation service interact with a compact, pedestrian-friendly  
28 pattern of commercial, residential, and mixed-use development within walking distance of the public  
29 transit route. These corridors have elevated levels of municipal public transportation service with  
30 frequent headways between buses and higher ridership demand. These transit routes connect  
31 local and regional town centers, city centers, and other service/employment centers, such as the  
32 UMED District. Transit-supportive development corridors are a half-mile wide, extending ¼-mile  
33 (approximately 5 to 10 minutes walking distance) on each side the public transit route.  
34  
35



1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

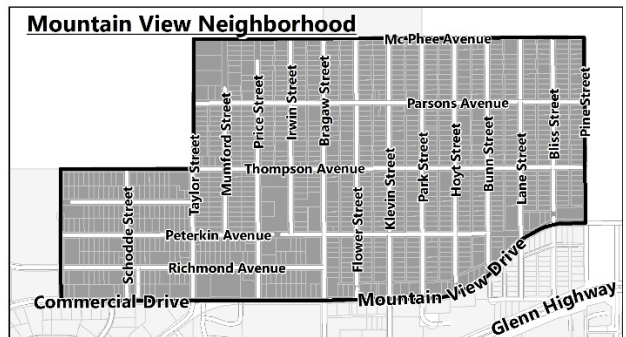
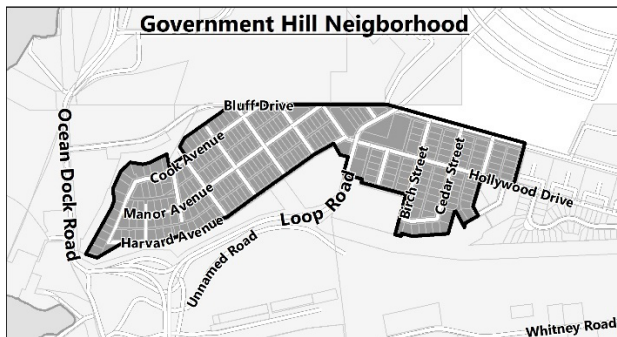
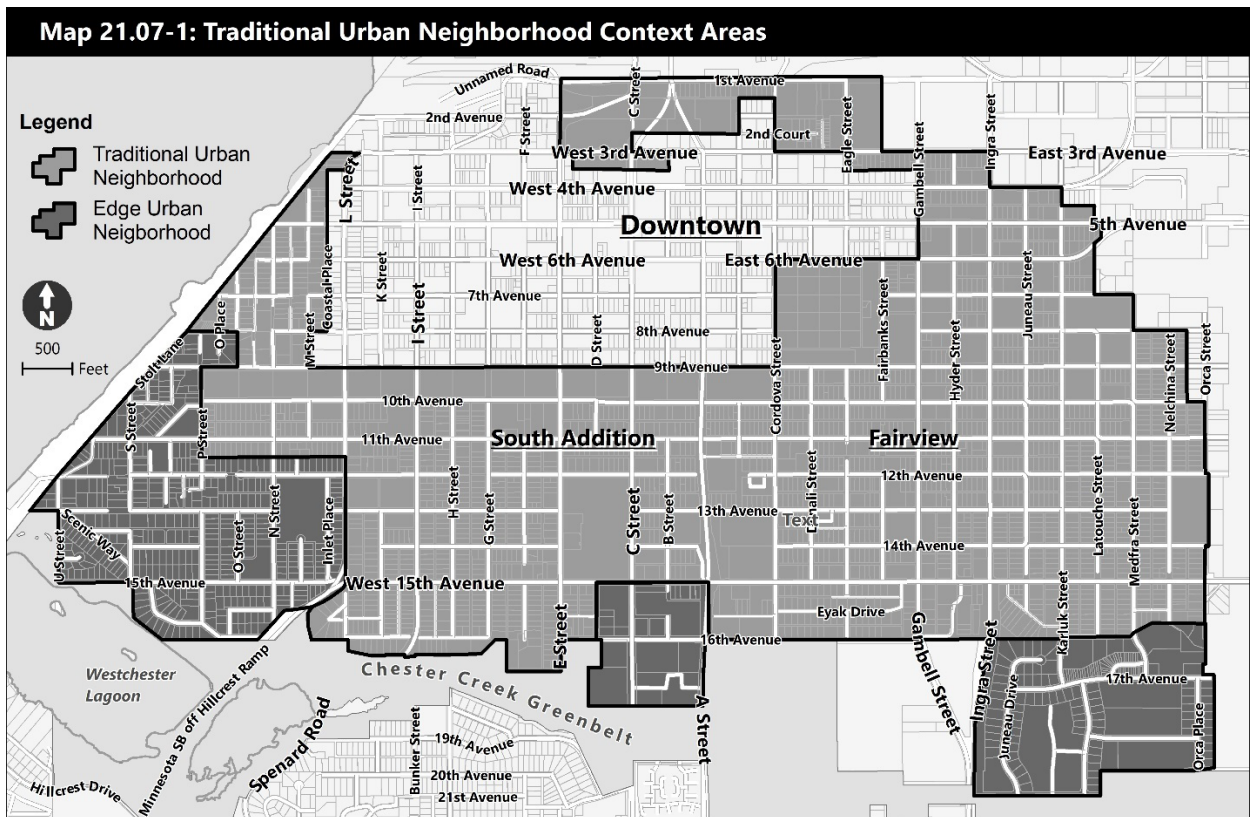
2 \*\*\* \*\*

3 **21.07.015 Neighborhood Development Context Areas**

4 \*\*\* \*\*

5 **D. Neighborhood Context Area Boundaries**

- 6
- 7 1. The boundaries of the neighborhood development context areas are established in maps 21.07-1
- 8 through 21.07-4.
- 9
- 10 2. The transit-supportive development corridors on maps 21.07-2, 21.07-3, and 21.07-4 shall include
- 11 all properties wholly or partially within ¼ mile of the street right-of-way centerline of the linear transit
- 12 corridor feature shown on the map, except that: where a transit-supportive development corridor
- 13 overlaps with a traditional urban or edge urban neighborhood context area, the traditional urban or
- 14 edge urban neighborhood regulations of this title shall govern.
- 15



1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

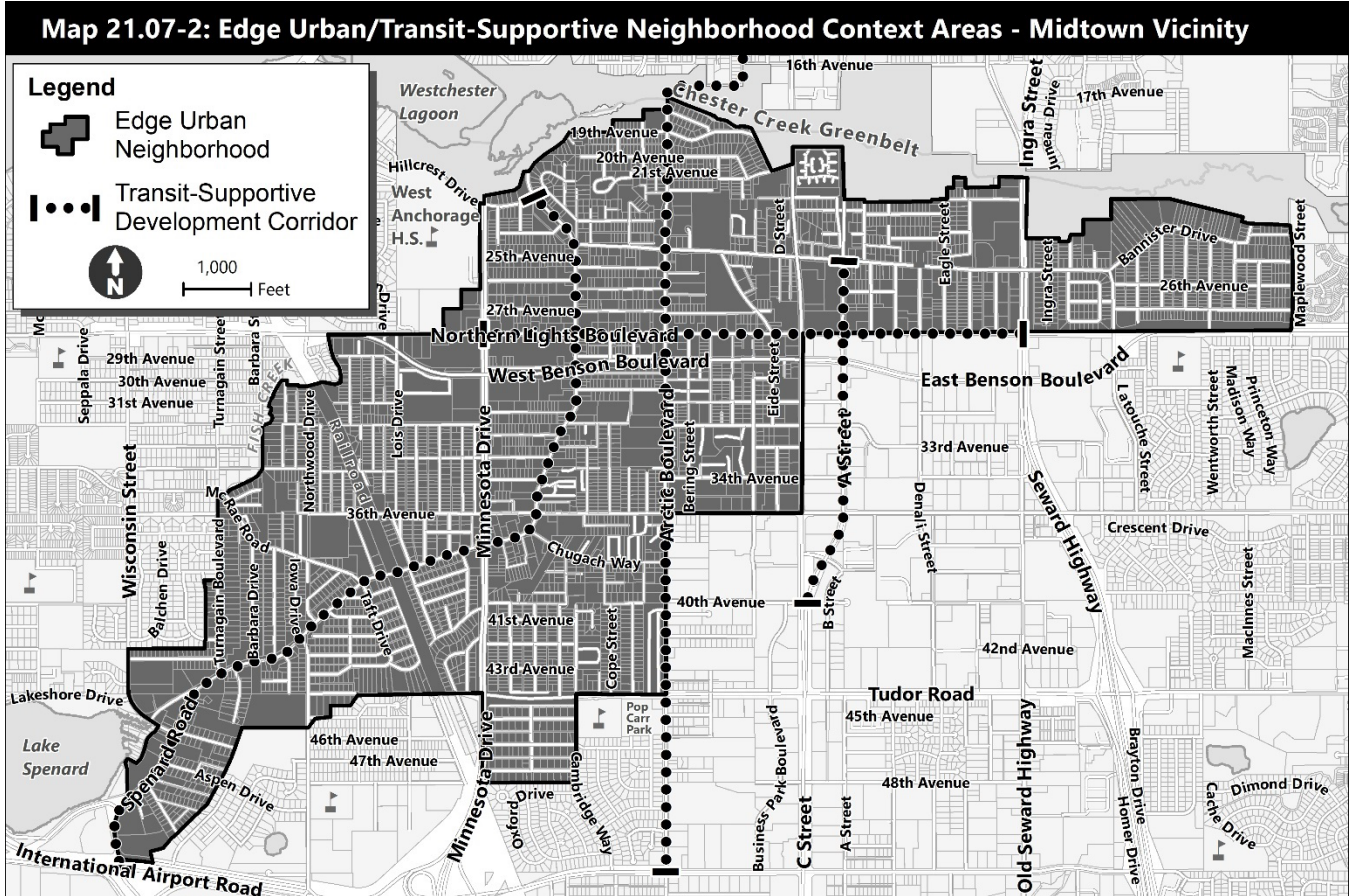
2 \*\*\* \*\*

3 **21.07.015 Neighborhood Development Context Areas**

4 \*\*\* \*\*

5 **D. Neighborhood Context Area Boundaries**

6 \*\*\* \*\*



1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

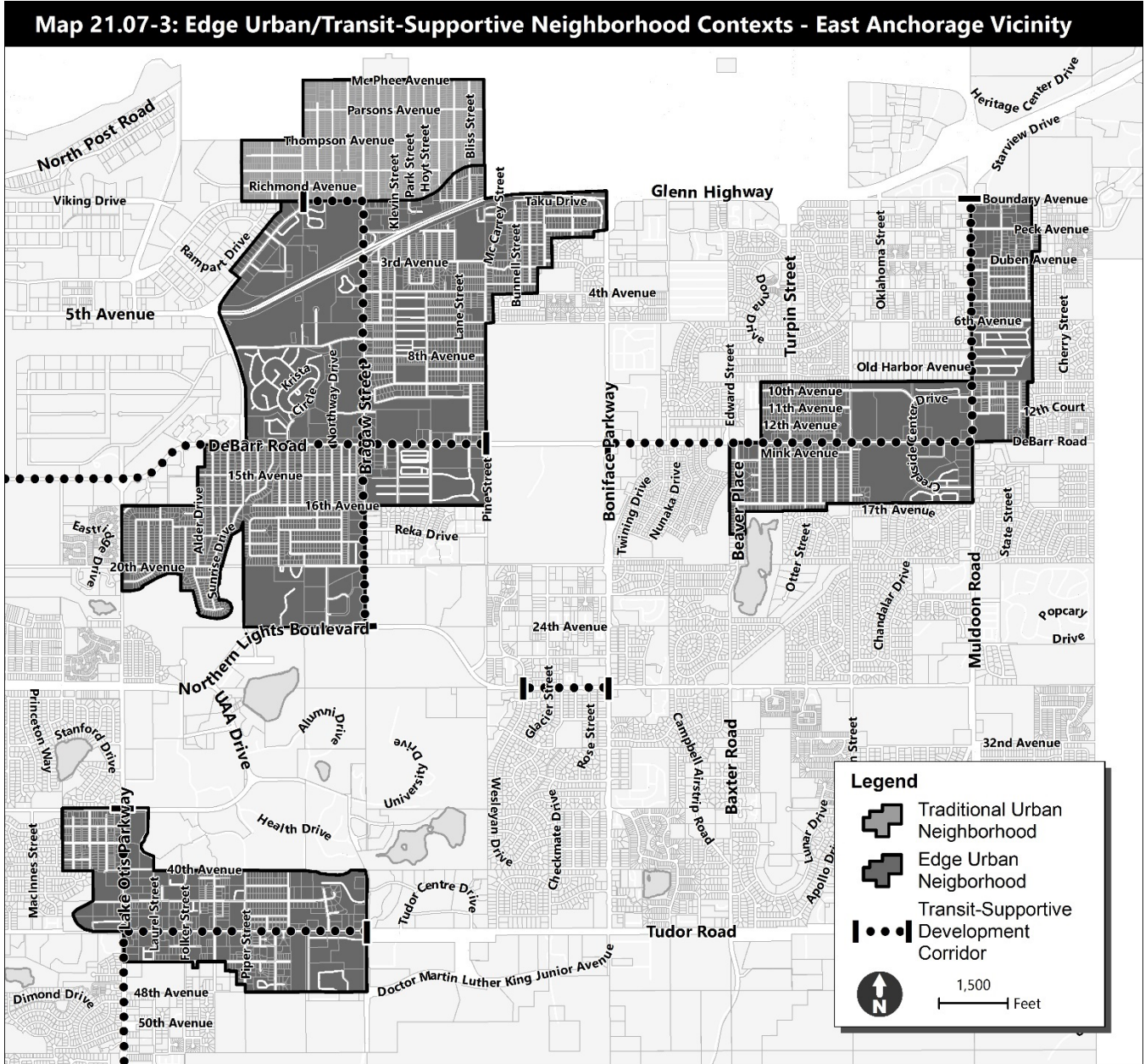
2 \*\*\* \*\*

3 **21.07.015 Neighborhood Development Context Areas**

4 \*\*\* \*\*

5 **D. Neighborhood Context Area Boundaries**

6 \*\*\* \*\*



7

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 \*\*\* \*\*

3 **21.07.015 Neighborhood Development Context Areas**

4 \*\*\* \*\*

5 **D. Neighborhood Context Area Boundaries**

6 \*\*\* \*\*



7

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 \*\*\* \*\*

3 **21.07.060 Transportation and Connectivity**

4 \*\*\* \*\*

5 **E. Standards for Pedestrian Facilities**

6 \*\*\* \*\*

7 **2. Sidewalks**

8  
9 a. All sidewalks shall be designed to comply with the standards of the Design Criteria Manual  
10 (DCM) and Municipality of Anchorage Standard Specifications (MASS) and shall be  
11 improved in accordance with subsection 21.08.050H.

12  
13 b. Street improvement projects and new streets in subdivisions in all class A zoning districts  
14 except for industrial districts shall install sidewalks on both sides of all streets (local,  
15 collector, arterial, public or private, including loop streets). Where indicated in the  
16 comprehensive plan, a pathway may replace a sidewalk on one side. Street improvement  
17 projects in industrial zoning districts shall install a sidewalk on one side of all local streets,  
18 and on both sides of local streets if the new sidewalks would connect to existing sidewalks  
19 on both ends and the needed sidewalk length is no greater than one quarter mile.

20 \*\*\* \*\*

21 f. Development on lots along existing streets in class A zoning districts shall install sidewalks  
22 on all lot frontages abutting streets in the following situations:

23  
24 i. In R-4, R-4A, commercial , and DT zoning districts.

25 \*\*\* \*\*

26 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

27 \*\*\* \*\*

28 **21.07.060 Transportation and Connectivity**

29 \*\*\* \*\*

30 **E. Standards for Pedestrian Facilities**

31 \*\*\* \*\*

32 **4. On-Site Pedestrian Walkways**

33 \*\*\* \*\*

34 **b. On-Site Pedestrian Connections**

35 \*\*\* \*\*

36 ii. All primary building entrances on a site shall be connected to the street by a  
37 convenient system of walkways. This includes multiple primary entrances into one  
38 building, and primary entrances in separate buildings on a site.

(Note: Replaced existing illustration.)

\*\*\* \*\*

iv. The primary front entrance of a residential dwelling shall be connected to the street by a walkway as provided in i. through ii. above, or by the dwelling's individual driveway, or by a shared parking courtyard meeting 21.07.060G.20.

\*\*\* \*\*

c. Walkway Clear Width and Improvements

\*\*\* \*\*

i. The minimum width of a required pedestrian walkway shall be five feet of unobstructed clear width, except where otherwise stated in this title. A walkway that provides access to no more than four residential dwelling units may provide an unobstructed clear width of three feet.

ii. Walkways shall be improved in accordance with subsection 21.08.050H.

\*\*\* \*\*

F. Pedestrian Frontage Standard

1. Purpose

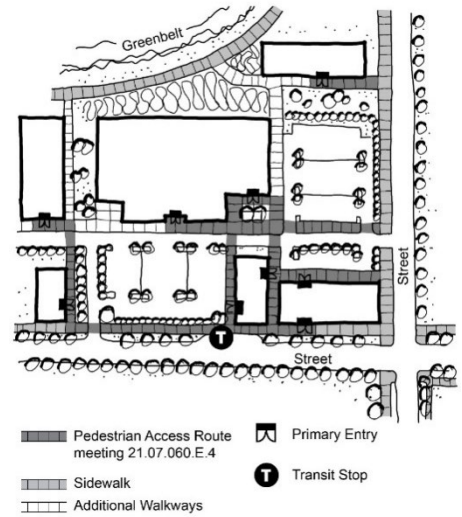
The pedestrian frontage standard promotes building placement and orientation toward neighborhood streets and sidewalks, facilitates pedestrian and bicycle access, supports lower parking utilization in urban neighborhood areas, and encourages alternative modes of transportation. Objectives include:

- a. Define public streets and their associated frontage spaces as positive, useful features around which to organize and orient buildings in a manner that promotes pedestrian activity and connects to multiple modes of transportation.
b. Provide pedestrian access to building entries that is clearly defined, safe, and inviting for people of all abilities, and minimizes conflicts between pedestrians and vehicle parking.
c. Place active living spaces, entrances, and windows on street-facing building facades to improve the physical and visual connection from residences to the street.
d. Promote a safe, secure pedestrian and neighborhood environment through "eyes on the street" design elements, such as placement of windows, that foster opportunities for casual surveillance of the street and outwardly expressed proprietorship of the neighborhood.

2. Applicability

Subsection 3. applies to development in the traditional urban and edge urban neighborhood context areas established in 21.07.015, and to developments that receive reductions to required parking in 21.07.090F. Subsection 4. applies to development in the other areas of the municipality. Exceptions and exemptions from this section 21.07.060F. include:

- a. Changes of use and other developments that comprise building modifications of less than 50 percent of the total improvement value of the building(s) on the site.
b. Development in Girdwood, Downtown (DT), and Industrial zoning districts.



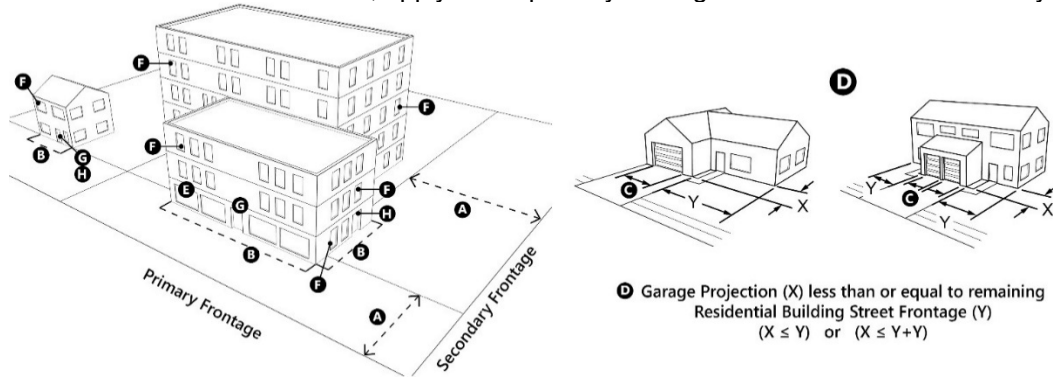
1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19

- c. Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of 20,000 square feet or larger, or in Class B zoning districts.
- d. Non-residential uses without habitable floor area, such as utility substations.
- e. The director shall reduce the window area requirement by the amount the applicant demonstrates is needed to comply with a 5-star energy rating or an affordable housing (21.15.040) financing institution's energy rating requirements.
- f. The director shall approve administrative relief provided the applicant demonstrates it is necessary to compensate for some practical difficulty of the site.
- g. The Alternative Equivalent Compliance procedure set forth in subsection 21.07.010D. may be used to propose alternative means of complying with the intent of this section.

**F. Pedestrian Frontage Standards**

**3. Standard for Urban Neighborhood Contexts and Parking Reductions**

The standards of table 21.07-2, apply to the primary frontage and at least one secondary frontage.



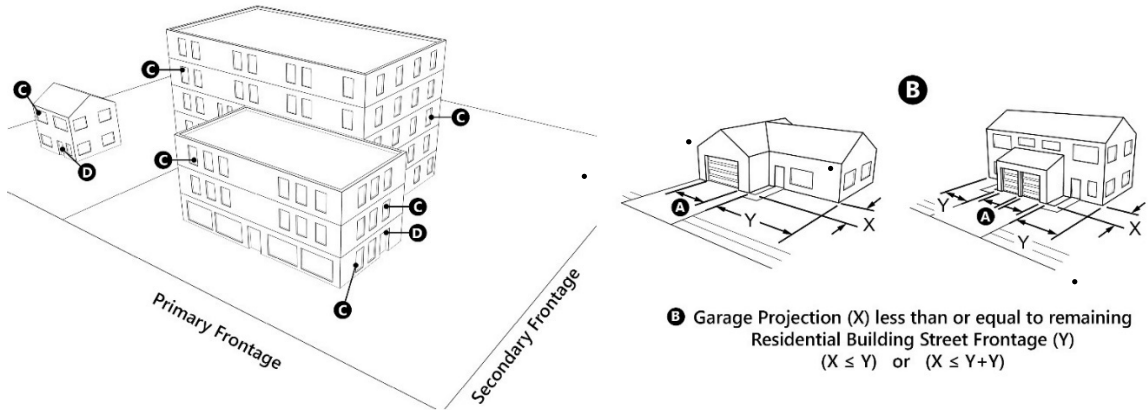
20

<b>Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts and Parking Reductions</b>		
<b>Building and Parking Placement <sup>1</sup></b>	<b>Primary Frontage</b>	<b>Secondary Frontage</b>
A. Maximum front setback (ground-floor only) <sup>1,4</sup>	30 feet in Traditional Urban, and 60 feet in other areas	90 feet
B. Minimum percentage of the width of ground-floor street-facing building elevation(s) required to comply with max. setback in A <sup>1,4</sup>	50%	50%
C. Residential garage entrance as a percentage of ground-floor street-facing building elevation width (maximum allowed)	50% in Traditional Urban, and 67% in other areas	67%
D. Maximum distance a garage may project out in front of the rest of the street-facing residential building elevation	No more than the width of the non-garage portion of the street-facing building elevation	
<b>Building Orientation and Access</b>	<b>Primary Frontage</b>	<b>Secondary Frontage</b>
E. Minimum percentage of a non-residential ground-floor, street-facing building elevation required to be windows or primary entrances <sup>2,3</sup>	30% in Traditional Urban, and 20% in other areas	15%
F. Minimum percentage of residential and upper-floor non-residential street-facing elevation required to be windows or primary entrances <sup>3</sup>	15% in Traditional Urban, and 10% in other areas	10%
G. At least one primary entrance located within max. setback in A <sup>4</sup>	Required	Not Required
H. Front primary entrances for residential dwellings meet 21.07.060G.15., Covered, Visible Residential Entrance	Required	
I. Minimum number of pedestrian amenities from 21.07.060G. in addition to 21.07.060G.15.	2 pedestrian amenities required per multifamily, mixed-use, townhouse, or group housing development	

- <sup>1</sup> Generally applicable standards and exceptions for maximum setbacks are provided in 21.06.030C.5.
- <sup>2</sup> Non-residential ground-floor windows used to meet these percentages shall be visual access windows (21.15.040) and have a sill height of no more than four feet above finished grade. Ground-floor wall area is defined in 21.15.040.
- <sup>3</sup> Rules of measurement for calculating window area as a percentage of building wall area are provided in 21.15.020P.
- <sup>4</sup> Development in the R-4A zoning district is exempt from the maximum setback related standards A, B, and G.

1  
2  
3

**4. Standard for Other Areas and Developments in the Municipality**  
The standards of table 21.07-3, apply to the primary frontage and at least one secondary frontage.



4

Table 21.07-3: Pedestrian Frontage Standard – Other Areas and Developments		
Building and Parking Placement	Primary Frontage	Secondary Frontage
A. Residential garage entrance as a percentage of ground-floor street-facing building elevation width (maximum allowed)	67%	75%
B. Maximum distance a garage may project out in front of the rest of a street-facing residential building elevation	No more than the width of the non-garage portion of the street-facing building elevation	
Building Orientation and Access	Primary Frontage	Secondary Frontage
C. Minimum percentage of residential street-facing building elevation wall areas required to be windows or primary entrances <sup>1</sup>	Window(s) required, with no minimum percentage	
D. Front primary entrances to residential dwellings meet 21.07.060G.15., <i>Covered, Visible Residential Entrance.</i>	Required	
E. Minimum number of pedestrian amenities from 21.07.060G. in addition to 21.07.060G.15.	1 pedestrian amenity required per multifamily, mixed-use, townhouse, or group housing development	

<sup>1</sup> Rules of measurement for calculating window area as a percentage of building wall area are provided in 21.15.020P.

5  
6



1 **21.07.060 Transportation and Connectivity**

2 \*\*\* \*\*

3 **G. Pedestrian Amenities Menu**

4  
5 **1. Purpose**

6 The purpose of this section is to define and provide standards for pedestrian amenities that may  
7 be required, included in a menu of choices to meet a requirement, or listed as a special feature that  
8 can count toward a bonus incentive anywhere in this title. For example, another section of this title  
9 may list a pedestrian amenity as a special feature for which bonus floor area or a reduction in  
10 required parking may be granted. The standards contained in this section give predictability for  
11 applicants, decision-makers, and the community for the minimum acceptable standards for  
12 pedestrian amenities. It also ensures the amenities will improve and enhance the community to the  
13 benefit of all, and respond to the northern latitude climate. This title provides flexibility to encourage  
14 and allow for creativity and unique situations through the alternative equivalent compliance and  
15 minor modifications process.

16  
17 **2. Applicability**

18 Pedestrian amenities shall meet the minimum standards of this section in order to be credited  
19 toward a requirement, menu choice, or incentive of this title, except where specifically provided  
20 otherwise in this title. The alternative equivalent compliance procedure set forth in subsection  
21 21.07.010D. may be used to propose alternative means of complying with the standards of this  
22 section 21.07.060G. The standards of this section apply only to pedestrian amenities that are  
23 counted toward a requirement, menu choice, or incentive under this title. It does not apply to  
24 amenities that do not receive credit for requirements in title 21.

25  
26 *(Note: Current code subsection 3., Walkway, is deleted. Current subsection 3 is re-numbered to 4.)*

27  
28 **3. Enhanced On-Site Walkway**

29 An enhanced on-site walkway is intended to provide an option for applicants to receive credit for  
30 exceeding the minimum development standards for walkways and improving pedestrian  
31 convenience, comfort, and safety on the site. Enhanced on-site walkways provide additional width  
32 for pedestrian movement and peripheral space that accommodates landscaping, furniture, and  
33 utilities.

34  
35 **a.** An enhanced on-site walkway shall have a pedestrian movement zone with a continuous,  
36 unobstructed walkway clear width of at least eight feet. Where adjacent to a ground-floor  
37 building elevation it shall also have a building interface zone a minimum of two feet in width  
38 for building foundation landscaping or space for opening doors or seating and transition  
39 pedestrian spaces. In addition to the pedestrian movement zone and any building interface  
40 zone, the enhanced on-site walkway shall have a buffer space of at least two feet in width  
41 where abutting motor vehicle parking lots, circulation aisles, or driveways. The buffer  
42 space shall [ accommodate landscaping beds, fencing or bollards, light poles, utilities,  
43 benches, and other furnishings.

44  
45 **b.** A minimum of two pedestrian features as defined by this title (21.15.040) shall be provided  
46 along the enhanced walkway, with at least one for every 50 feet of the walkway length.

47  
48 **c.** Enhanced on-site walkways shall provide continuous, direct connections from building  
49 primary entrance(s) to surrounding public streets and sidewalks and be publicly  
50 accessible or available to all residents of the development.

51 \*\*\* \*\*

1 \*\*\* \*\*

2 (Note: Pedestrian Amenities G.4. through G.13 are not shown because they are not being amended.  
3 Existing subsection 15, Bicycle Parking Amenities, is deleted.)  
4

5 **14. Separated Walkway to the Street**

6 The development shall connect the building primary entrances to the street with a clear and direct  
7 walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles.  
8 The minimum clear width portion of the walkway shall be separated from the parking facility by at  
9 least five feet.

10  
11 **15. Covered, Visible Residential Entrance**

12 A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building  
13 entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings  
14 from the street. The entrance shall meet the following standards:  
15

- 16 a. The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at  
17 least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.
- 18 b. The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle  
19 parking by vertical separation or a change in surfacing material.
- 20 c. The building entrance shall also be visible (via an unobstructed line of sight) from a street  
21 or face a common private open space (21.07.030) that is visible from a street.  
22

23  
24  
25 **16. Enhanced Primary Entrance**

26 An enhanced building entry is intended to provide an option for applicants to receive credit for a  
27 more prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary  
28 entrance shall incorporate at least three of the following features:  
29

- 30 a. Outdoor sheltering roof feature projecting from the building façade such as an overhang,  
31 portico, canopy, marquee with an inside dimension of at least 16 square feet;  
32
- 33 b. Recessed and/or projected entrance or other building wall modulation with projections or  
34 recesses in the building wall plane;  
35
- 36 c. Changes in the building’s main roofline such as arches, peaked roof forms, or terracing  
37 parapets;  
38
- 39 d. Changes in siding material or exterior finishes, or façade detail features such as tilework  
40 that emphasize the entrance;  
41
- 42 e. Entrance plaza, patio, or similar common private open space;  
43
- 44 f. Landscaping not otherwise required by this title, such as integrated planters, landscape  
45 accent lighting, or special paving treatments; or  
46
- 47 g. One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.  
48

49 **17. Site Entry Feature**

50 Highlight and define a pedestrian entrance to a development site using three or more of the  
51 following elements:  
52

- 53 a. Landscape treatment with seasonal color and trees, which clearly distinguishes and  
54 highlights the site entry.
- 55 b. Plaza or courtyard as described in subsection 21.07.060G.5.

- c. Identifying building primary entrance form including a covered entry, when the primary entrance is within 50 feet of the site entrance.
- d. Special paving, pedestrian scale lighting, and/or bollards.
- e. Ornamental gate and/or fence.

**18. Pedestrian-Interactive Building**

A pedestrian-interactive building is intended to provide interior spaces that engage the sidewalk with street-facing windows and entrances and activities and services that support neighborhood residents, and contribute to the activity level and quality of the pedestrian environment of the neighborhood or district. The standards that follow apply where the term “pedestrian-interactive building” is listed in this title as a requirement, special feature for a bonus, or a menu choice.

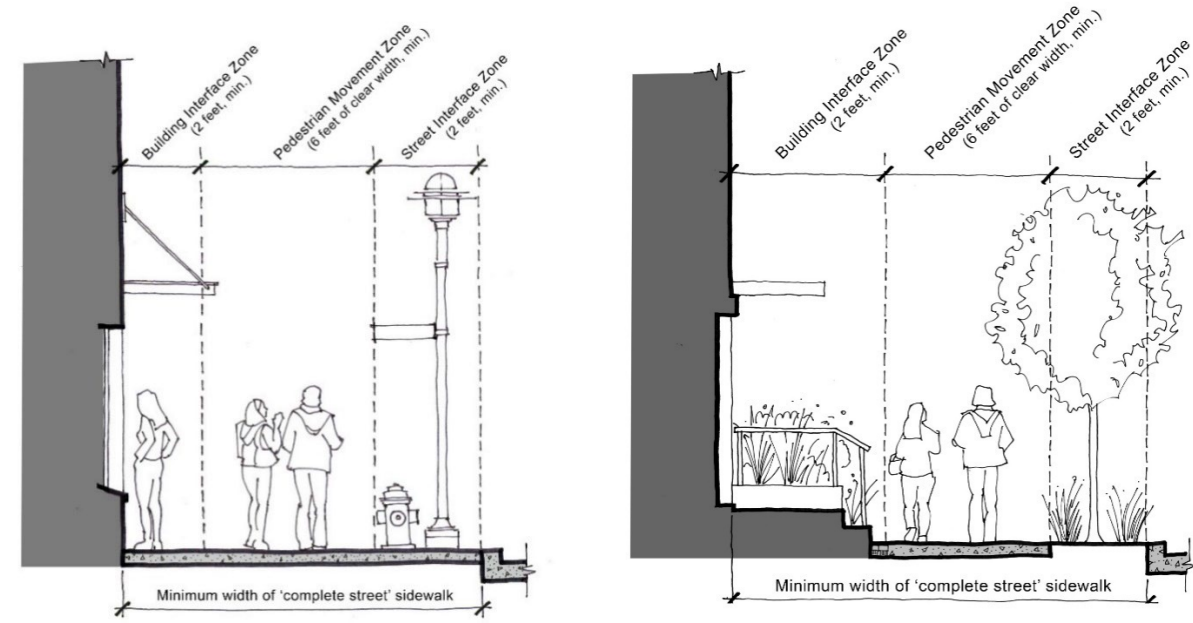
- a. A pedestrian-interactive building shall provide a primary entrance facing the street. Entrances at building corners facing a street may be used to satisfy this requirement.
- b. A pedestrian-interactive building shall contain habitable floor area at least 24 feet deep extending along a minimum of 50 percent of the length of the ground-floor, street-facing building elevation in Urban Neighborhood Contexts, and 30 percent elsewhere. The habitable floor area may include pedestrian entrances, entry lobbies or atriums, and stairwells.
- c. Street-facing wall areas of a non-residential use shall be 50 percent visual access windows or primary entrances on the ground floor, and 20 percent windows above the ground floor;
- d. Street-facing wall areas for residential uses shall be at least 20 percent windows or primary entrances in Urban Neighborhood contexts and 10 percent in other areas.
- e. Where a building has three or more street frontages, these criteria apply along only two of the frontages.

**19. Enhanced Street Sidewalk**

An enhanced street sidewalk is intended to provide an option for applicants to receive credit for exceeding the minimum development standards for sidewalks. An enhanced street sidewalk promotes sidewalk widening and streetscape enhancements to support higher levels of pedestrian activity, comfort, and safety in the district. An enhanced street sidewalk with “complete street” (21.15.040) style amenities may be provided in lieu of required site perimeter landscaping as determined through an administrative site plan review, and subject to the following:

- a. The enhanced street sidewalk cross-section shall be at least 12 feet wide, and include a pedestrian movement zone, building interface zone, and street interface zone (21.15.040). The pedestrian movement zone shall have a walkway clear width of at least six feet. The street interface zone shall be at least two feet wide from back-of-curb, and four feet wide along major arterials. Where a building adjoins the sidewalk, there shall be a building interface zone at least two feet wide.

*(Note: Existing illustration replaced with the two illustrations below.)*



*Enhanced Street Sidewalks in Commercial and Residential Settings*

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34

- b. The enhanced street sidewalk shall provide at least two-thirds the number of trees and shrubs that would otherwise be required for site perimeter landscaping.
- c. The enhanced street sidewalk may be placed wholly or in part within a right-of-way, subject to approval of the traffic engineer and municipal engineer.
  - i. The enhanced street sidewalk shall be subject to the applicable requirements of title 24, including sections 24.30.020., *Permit to use Public Places*, and 24.90, *Encroachment Permit*.
  - ii. Existing improvements that meet the standards of the enhanced street sidewalk may be counted towards the requirements of this section, subject to approval by the director.
  - iii. The owner shall maintain landscaping and amenities for the enhanced street sidewalk within the right-of-way
  - iv. Where the right-of-way is not adequate or cannot be configured to accommodate the enhanced street sidewalk, then the development shall be set back from the street frontage as necessary to accommodate part of the improvements within the property. A public use easement shall be recorded for any part of the designated sidewalk width to be located within the subject parcel.

**20. Parking Courtyard**

*(Note: No changes to Shared Parking Courtyard subsection)*

\*\*\*      \*\*\*      \*\*\*

1 **21.07.090 Off-Street Parking and Loading**

2  
3 **A. Purpose**

4 This section establishes off-street parking and loading requirements to accommodate anticipated parking  
5 utilization, and to ensure that parking facilities are designed to perform in a safe, efficient manner. It is also  
6 the intent of this section to attenuate the adverse visual, environmental, and economic impacts of parking  
7 lots, and to balance the provision of adequate off-street parking with other modes of site access in order to  
8 achieve area-wide objectives for a compact and efficient land use pattern, affordable housing, pedestrian-  
9 friendly environments, and use of multiple modes of transportation. Specific objectives of this section  
10 include:

- 11  
12 1. Ensure that off-street parking, loading, and access demands will be met without impacting  
13 adjoining and nearby streets, properties, land uses, and neighborhoods;
- 14  
15 2. Provide for safe and orderly circulation and parking in parking and loading facilities, and minimize  
16 conflicts between pedestrians and vehicles;
- 17  
18 3. Encourage the efficient use of land and discourage the encumbrance of more space than is  
19 necessary for anticipated parking utilization;
- 20  
21 4. Recognize, through parking reductions and lower parking requirements, the parking efficiencies of  
22 mixed-use development, mixed-income housing, walkable development patterns, proximity to  
23 public transportation, and other area-specific neighborhood characteristics;
- 24  
25 5. Recognize, through area-specific exemptions from parking requirements, the parking efficiencies  
26 of public parking facilities, on-street parking management, and alternative travel mode incentives;
- 27  
28 6. Improve the visual appearance of public street corridors by encouraging buildings and other  
29 attractive site features to become more prominent relative to parking lots and driveways;
- 30  
31 7. Manage the width and frequency of driveways along street frontages to provide adequate space  
32 for—and reduce conflicts with—snow clearing and snow storage in the right-of-way, on-street  
33 parking (where appropriate), and pedestrian facilities;
- 34  
35 8. Provide for better pedestrian movement and encourage alternative modes of transportation  
36 consistent with cleaner air and water, greater transportation choice, and efficient infill and  
37 redevelopment by reducing expanses of parking;
- 38  
39 9. Facilitate bicycle use by providing safe, secure, and convenient bike parking through minimum  
40 requirements for the type of bike parking facilities and the amount of bicycle spaces; and
- 41  
42 10. Allow flexibility in addressing vehicle parking, loading, and access, including reductions to parking  
43 requirements and mechanisms to manage parking needs while promoting development,  
44 reinvestment, and other community goals.

45 **B. Applicability**

46  
47 **1. Generally**

- 48  
49 a. The off-street parking and loading standards of this section 21.07.090 shall apply to all  
50 development in the municipality, including changes of use.
  - 51  
52 b. Except for the off-street loading requirements of subsection 21.07.090G., all other  
53 requirements of this section shall apply to Girdwood, unless specifically preempted in  
54 chapter 21.09.
- 55

1 c. Except when specifically exempted, the requirements of this section shall apply to all  
2 temporary parking lots and parking facilities that are a principal use on a site.  
3

4 **2. Expansions, Relocations, and Enlargements**

5 A site to which a building is relocated shall provide required parking and loading spaces per tables  
6 21.07-7, 21.07-8, and 21.07-11. An expansion or enlargement that is an increase in the floor area  
7 or other measure of off-street parking and loading requirements shall provide required parking and  
8 loading for the increase, except as provided otherwise in section 21.07.090F., *Parking Reductions*.  
9

10 **3. Use of Required Parking Spaces**

11 Required parking spaces shall be available for the parking of passenger automobiles by residents,  
12 occupants, customers, visitors, or employees of the use. Required parking spaces shall not be  
13 assigned, leased, or rented in any way to a use on another site, or to anyone who is not a resident,  
14 occupant, customer, guest, or employee, except for shared parking situations as provided in  
15 subsection 21.07.090F.5. Required parking spaces shall not be used for the parking of equipment  
16 or for storage of goods or vehicles.  
17

18 **4. Regulation of Parking Space Use**

19 The providers of required off-street parking spaces may reasonably control the users thereof by  
20 means that may include, but are not limited to, restricting all parking to the users of the facility;  
21 parking lot attendants; control gates; tow-away areas; areas for exclusive use by employees,  
22 tenants or staff; areas restricted for use by customers or visitors; and imposing time limitations on  
23 users. Fees may be charged for the use of required parking, however required parking shall not in  
24 any way be made unavailable for the use served, except as provided otherwise in this title. The  
25 municipality may enforce any approved parking plan or restrictions through any of the code  
26 enforcement provisions set forth in chapter 21.14, Enforcement.  
27

28 **5. Parking Nonconformities**

29 When a site is out of compliance as to the number of required or allowed parking spaces, section  
30 21.13.060, *Characteristics of Use*, applies.  
31

32 **C. Computation of Parking and Loading Requirements**

33 \*\*\* \*\*\* \*\*\*

34 **5. Additional Computation Standards**

35 \*\*\* \*\*\* \*\*\*

36 **c. Areas that Count Toward Minimum but Not Maximum Parking Requirements**

37 For the purpose of calculating parking requirements, the following types of parking spaces  
38 shall not count against the maximum parking requirement, but shall count toward the  
39 minimum requirement:  
40

- 41 i. Accessible parking spaces;
- 42
- 43 ii. Parking spaces set aside for passenger loading including taxicab stands and ride  
44 hailing service spaces, provided that such spaces are not required by the traffic  
45 engineer pursuant to subsection 21.07.090I.;
- 46
- 47 iii. Vanpool and carpool parking spaces;
- 48
- 49 iv. Stacked, tandem, and full-time attendant (valet) parking spaces in conformance  
50 with subsection 21.07.090H.12.;
- 51
- 52 v. User-paid parking such as unbundled parking;
- 53
- 54 vi. Parking spaces with electric vehicle supply equipment (EVSE) for EV charging;  
55

1                   vii.     Parking spaces provided as the required parking for a use on another parcel  
2                   through a municipally approved shared parking or off-site parking agreement; and  
3

4                   viii.    Parking structures, underground parking, and parking within, above, or beneath  
5                   the building(s) it serves.  
6

7                   d.       ***Reductions to Required Parking Not Applicable to Accessible Parking Spaces or***  
8                   ***Passenger Loading***

9                   The following types of parking spaces shall not be reduced by the area-specific parking  
10                  requirements or exemptions established in 21.07.090E.2. or the parking reductions and  
11                  alternatives in 21.07.090F.:

12                  i.        Required accessible (ADA) parking spaces pursuant to subsection 21.07.090J.

13                  ii.       Required passenger loading spaces pursuant to subsection 21.07.090I.  
14  
15  
16

17 **D.       Parking Lot Layout and Design Plan**

18 \*\*\*     \*\*\*     \*\*\*

19 **2.       Minimum Plan Requirements**

20 \*\*\*     \*\*\*     \*\*\*

21                  b.       The parking facility layout, circulation, and design plan shall include:

22                  i.        Location of permanent or temporary snow storage areas with calculations per  
23                  21.07.040F.;

24                  ii.       Location of required landscaping areas, refuse screening, and fences;  
25  
26

27                  iii.       Location of required pedestrian sidewalks and walkways per 21.07.060E. Show  
28                  dimensions for sidewalk widths and grades with spot elevations;  
29  
30

31                  iv.       Off-street parking and loading calculation for all uses located on the site per tables  
32                  21.07-7 and 21.07-11;  
33

34                  v.        Parking space and loading berth locations. Include number of spaces provided,  
35                  typical space dimensions, and grades per 21.07.090H.;

36                  vi.       Where loading facilities or on-site refuse collection are provided, show commercial  
37                  truck circulation and truck turning radii when required by the traffic engineer;  
38  
39

40                  vii.       Show all parking facility circulation patterns including location of curbed end  
41                  islands at end of parking rows. Provide dimensions for the width of the circulation  
42                  aisles, and show grades with spot elevations per 21.07.090H.;

43                  viii.      Location of accessible parking spaces and access aisles per 21.07.090J.;

44                  ix.        Location of passenger loading zones and spaces per 21.07.090I.;

45                  x.        Emergency access, fire lanes, and refuse collection;  
46  
47

48                  xi.       Vehicle drive-throughs, vehicle queueing spaces and dimensions including  
49                  vehicle-to-vehicle separation if required per 21.07.090 L.  
50  
51  
52

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31

- xii. Number, location and dimensions of bicycle parking spaces if required per 21.07.090K., including bicycle parking access routes;
- xiii. Driveways to streets and alleys. Provide dimensions for throat width/depth, landing grades, and driveway slope including spot elevations. Show sight distance triangles per the Municipal Driveway Standards;
- xiv. On site traffic control signage and locations. Provide a sign summary table that indicates Sign ID, MUTCD/ATMS sign designation, description, and the direction the sign is facing;
- xv. Required parking lot lighting locations, lighting calculations and glare statement, except that parking lots with fewer than 10 parking spaces serving three- and four-unit multifamily uses are exempt from providing a lighting engineering plan; and
- xvi. Location of significant drainage elements such as manholes, catch basin, and drainage swales.

\*\*\*      \*\*\*      \*\*\*

**E. Off-Street Parking Requirements**

**1. Minimum Number of Spaces Required**

Off-street parking spaces shall be provided in accordance with this section 21.07.90E. Reductions and alternatives to the requirements of this section are provided in 21.07.090F., *Parking Reductions and Alternatives*.

(Note: Current subsection 2. Is deleted and replaced as follows:)

**2. Area-specific Parking Requirements**

Lower parking requirements for specific areas that have less parking utilization and more alternative transportation options are set forth in table 21.07-7.

<b>TABLE 21.07-7: AREA-SPECIFIC PARKING REQUIREMENTS</b>		
<b><u>Areas</u></b>	<b><u>Applicability</u></b>	<b><u>Minimum Spaces Required</u></b>
Downtown (DT) zoning districts	All Developments	No off-street parking is required, as provided in 21.11.070F.
Traditional Urban Neighborhood Context (Section 21.07.015D., Map 21.07-1.)	Residential Uses east of C Street	70% of the minimum spaces required in table 21.07-8.
	All Other Developments	80% of the minimum spaces required in table 21.07-8.
Edge Urban Neighborhood Context (Section 21.07.015D., Maps 21.07-1 thru -3.)	Residential Uses	80% of the minimum spaces required in table 21.07-8.
	All Other Developments	90% of the minimum spaces required in table 21.07-8.
Transit-Supportive Development Corridors outside of Edge Urban Context areas (Section 21.07.015D., Maps 21.07-2 thru -4.)	All Developments	90% of the minimum spaces required in table 21.07-8.
Open Option Parking Districts (21.07.090E.7.)	All Developments	No off-street parking is required, subject to subsection 21.07.090E.7.
Girdwood	See section 21.09.070L. for area-specific parking requirements in Girdwood.	



1  
2  
3  
4  
5  
  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38  
39  
40  
41  
42  
43  
44  
45  
46

**3. Use-Specific Parking Requirements**

All development in the Municipality not addressed in the area-specific parking requirements in table 21.07-7 shall provide off-street parking spaces in accordance with table 21.07-8:

TABLE 21.07-8: OFF-STREET PARKING REQUIREMENTS BY LAND USE ("du" = dwelling unit; "sf" = square feet; "gfa" = gross floor area)			
	Use Type	Minimum Spaces Required	See Loading Subsection 21.07.090G.
***	***	*** (Note: No changes proposed in the body of this table.)	

\*\*\*      \*\*\*      \*\*\*

(Note: Re-numbered existing subsections 21.07.090E.3. and 21.07.090E.4. to E.4. and E.5. No other changes to these two subsections.)

**6. Parking Location**

Except as provided in subsection 21.07.090F., all required parking shall be on the same lot as the use served.

**7. Open Option Parking District**

**a. Purpose**

This subsection provides for the establishment of Open Option Parking Districts. Open Option Parking Districts remove minimum off-street parking requirements within specified areas and replace them with street management strategies within the public rights-of-way, as well as parking demand reduction strategies on larger developments. Property owners, developers, and businesses decide how much off-street parking to provide on their properties based on site-specific operations and activities. The public right-of-way gets managed separately, to ensure on-street parking, street maintenance and snow removal, and safe pedestrian and traffic movement. The Open Option promotes efficient use of urban land, economic development and housing opportunities, and transportation choices.

**b. Applicability**

Open Option Parking Districts may be established in the neighborhood development context areas delineated in section 21.07.015.

**c. Minimum Area Requirements**

The designation of an Open Option Parking District shall be considered only for whole city blocks (surrounded by public streets or public lands), or whole street face blocks (all the properties with primary frontage on a length of street between two intersections). Open Option Parking Districts may include multiple whole city blocks and/or street face blocks. The total area of the parcels within an Open Option Parking District shall be at least 1.75 acres (excluding rights-of-way).

**d. Procedure for Designating an Open Option Parking District**

**i. Application Submittal.** Any person, community council, or the municipality may initiate the designation of an Open Option Parking District. An application may be obtained from the planning department. The applicant shall select one of the two approval procedure options provided in ii. and iii.

- 1                   ii.     *Property Owner Approval Option.* The applicant shall demonstrate that fifty-one  
2 percent of the property owners in the proposed Open Option Parking District  
3 support the designation in writing through a letter or a signed petition to the  
4 department. Upon receiving the application, the department shall notify all  
5 property owners in the proposed designated area and provide 30 days for  
6 comment. The Traffic Engineer and Director shall then evaluate the proposal and  
7 act as the decision making body subject to the approval criteria in d. below.  
8
- 9                   iii.     *Assembly Approval Option.* Upon receiving the application, the Planning  
10 Department shall notify all property owners in the designated area and provide 30  
11 days to comment. The Traffic Engineer and Director shall then evaluate the  
12 proposal using the approval criteria in d. below, and forward a recommendation to  
13 the Anchorage Assembly. The Assembly shall hold a public hearing and take final  
14 action as the decision making body.  
15
- 16                  iv.     *Agency Review.* The Planning Department shall request comment by ROW  
17 agencies and utilities, including the traffic engineering, right-of-way enforcement,  
18 public works, the anchorage parking authority, municipal and street maintenance,  
19 fire department, police department, and the public transportation department.
- 20                  v.     *Documentation of Approved Open Option Parking Districts.* The Planning  
21 Department shall maintain a publicly available map of designated Open Option  
22 Parking Districts online and at the department.  
23

24           e.     ***Approval Criteria for Designating an Open Option Parking District***

25           The traffic engineer and director shall evaluate each proposed Open Option Parking District  
26 based on the following approval criteria:  
27

- 28                  i.     *On-Street Parking Management.* There shall be a plan for on-street parking  
29 management address parking utilization, congestion, and availability, with the  
30 approval of agencies assigned.  
31
- 32                  ii.     *ROW Management Plan.* There shall be a plan for coordinating on-street parking  
33 with street maintenance, snow removal, refuse collection; public transportation,  
34 and other street-based operations, approved by the requisite public agencies.  
35
- 36                  iii.     *Consistency with Comprehensive Plan.* The proposal shall conform to applicable  
37 neighborhood, district, or other area-specific or function plans.  
38

39           f.     ***Requirements for Developments in Open Option Parking Districts***

- 40                  i.     No off-street parking is required for any development, except that accessible (ADA)  
41 parking spaces required by 21.07.090J. shall be provided.  
42
- 43                  ii.     Development projects that result in 10 or more dwelling units, 10 or more  
44 bedrooms of group living, new construction of 10,000 or more square feet of non-  
45 residential gross floor area, or substantial building renovations involving a change  
46 of use of 50,000 or more square feet of non-residential gross floor area, shall  
47 provide disincentives for off-street parking utilization and incentives for alternative  
48 means of site access, by providing parking reduction strategies from section  
49 21.07.090F., Table 21.07-9. Structured parking, industrial uses, and affordable  
50 housing units that meet 21.07.110F., are exempt from this requirement.  
51
- 52                  (A)     The parking reduction strategies selected from section 21.07.090F.1.,  
53 Table 21.07-9 shall amount to a total reduction of at least 10 percent from  
54 what would otherwise be the minimum parking requirement were it not for  
55 the Open Option Parking District, using the "Reduction Amount" column in  
56

Table 21.07-9. The total reduction required shall increase an additional 1 percent for each 10 off-street parking spaces above 20 parking spaces in the development.

(B) The requirements of 21.07.090F. for receiving parking reductions apply.

(C) The parking reductions in section F. of Table 21.07-9 do not count. However, the applicant may propose strategies not included in Table 21.07-9, subject to 21.07.090F.8, Discretionary Parking Reductions.

**F. Parking Reductions and Alternatives**

**1. Parking Reductions Allowed**

This section 21.07.090F. allows administrative reductions to the number of off-street parking spaces required by section 21.07.090E. Developments are allowed to apply the reductions shown in Table 21.07-9, provided the following:

- a. The development meets the additional requirements set forth in Table 21.07-9;
- b. The property owner enters into a parking agreement with the Municipality of Anchorage as provided in 21.07.090F.2.;
- c. Multiple parking reductions are calculated as provided in F.3.;
- d. Pedestrian access improvements are provided as set forth in 21.07.090F.4.; and
- e. Proposals for larger percentage reductions than shown, that modify any of the provisions for the reductions shown, or that propose other types of parking reductions from those in table 21.07-9 are subject to a discretionary review and approval by the traffic engineer and director as provided in subsection 21.07.090F.8., *Discretionary Parking Reductions*.

<b>Table 21.07-9: Parking Reductions and Alternatives</b>			
Type of Reduction	Applicability	Additional Requirements	Reduction Amount
<b>A. Shared Vehicle Programs:</b> Participation in one or more of the shared vehicle programs below may substitute for required parking spaces, provided that information about the programs is available in a location visible to all residents and employees.			
Carpool Program	Non-residential uses	The employer or property owner sponsors a carpool program that is available to all employees and provides designated carpool parking spaces signed for exclusive use by the carpool program.	Each carpool space may count as two spaces toward meeting the minimum number of required parking spaces, up to a 2% reduction in the number of required parking spaces.
Rideshare Program	Non-residential uses	The employer or property owner sponsors a rideshare program that is available to all employees and provides designated rideshare parking spaces that meet the passenger loading space dimensional standards of 21.07.090I.2. and are signed for exclusive use by the rideshare program.	Each rideshare space may count as six spaces toward meeting the minimum number of required parking spaces, up to a 5% reduction in the number of required parking spaces.
Car-Share Program	Residential uses located in the Neighborhood Development Contexts <sup>1</sup>	The property owner sponsors memberships to an active car-share program for all households or group living residents on the site and provides designated car-share spaces that are signed for exclusive use.	Each carshare space may count as five spaces toward meeting the minimum number of required parking spaces, up to a 10% reduction in the number of required parking spaces.
Transit Pass Benefits	Any use located in the Neighborhood Development Contexts <sup>1</sup>	The property owner sponsors public transit passes cost-free to all employees or residents.	10% reduction in the number of required parking spaces.
***	***	*** (table continued on next page)	

Table 21.07-9: Parking Reductions and Alternatives			
Type of Reduction	Applicability	Additional Requirements	Reduction Amount
***	***	*** (table continued from previous page)	
<b>B. Pedestrian Amenities:</b> Developments that provide improved conditions for walking and bicycling are eligible for reductions in the minimum number of required parking spaces, as provided below.			
Additional Bicycle Parking	Non-residential use, group living use, and any household living use with four or more dwelling units.	The development provides more than the minimum number of required bicycle parking spaces. Each bicycle space meets the standards of 21.07.090K.4.	Each four bicycle parking spaces count as one automobile parking space, with a minimum allowed reduction of one space and a maximum of a 10% reduction in the number of required automobile parking spaces.
Enhanced On-Site Walkway	Any use in the Neighborhood Development Contexts <sup>1</sup>	The development provides an enhanced on-site walkway per 21.07.060G.3.	2% reduction in the number of required parking spaces
Enhanced Street Sidewalk	Any use in the Neighborhood Development Contexts <sup>1</sup>	The development provides an enhanced street sidewalk per 21.07.060G.19.	2% reduction in the number of required parking spaces
Transit Stop or Shelter	Any use in the Neighborhood Development Contexts <sup>1</sup>	Based on a determination of need by the public transportation department, the development provides a public use easement or transit stop improvements per 21.07.060G.7.	2% reduction in the number of required parking spaces
Other Pedestrian Amenities	Any use in the Neighborhood Development Contexts <sup>1</sup>	The development provides one or more additional pedestrian amenities from section 21.07.060G. not otherwise required by this title.	1% reduction in the number of required parking spaces for each pedestrian amenity.
<b>C. Parking Pricing:</b> Developments that offer the parking pricing strategies below are eligible for reductions in the minimum number of required parking spaces.			
Parking Cash-out	Non-residential uses	The use implements a parking cash-out program as defined in 21.15.040 and informs all employees of the program. The cash-out value of the parking space is allowed to be up to one-year in duration.	10% reduction in the number of required parking spaces.
Unbundled Parking	Household living uses and non-residential uses	All parking accessory to the use is unbundled parking as defined in 21.15.040, by which the parking spaces are leased or sold separately from the rental or purchase fees for the dwelling units or building space.	10% reduction in the number of required parking spaces.
***	***	*** (table continued on next page)	

1

Table 21.07-9: Parking Reductions and Alternatives			
Type of Reduction	Applicability	Additional Requirements	Reduction Amount
***	***	*** (table continued from previous page)	
<b>D. Housing:</b> Housing units with characteristics that reduce parking utilization are eligible for reductions in the minimum number of required parking spaces, as provided below.			
Affordable Rental Housing	Household living uses	Rental housing units that meet the standards of 21.07.110G., <i>Standards for Affordable Housing</i> , are rented at rates affordable to lower income households.	Each affordable dwelling unit is eligible for a 25% reduction in the number of required parking spaces.
Senior Housing	Residential uses	Housing that meets the definition of senior housing (21.15.040) and is solely occupied by persons 62 years or older.	Each senior housing unit is eligible for a 25% reduction in the number of required parking spaces.
<b>E. Shared and Off-site Parking Facilities:</b> Properties that utilize off-site parking facilities or shared parking between uses are eligible for reductions to the minimum number of required parking spaces, as provided below.			
Shared Parking		See section 21.07.090F.5.	
Off-site Parking		See section 21.07.090F.6.	

**F. Prioritization of Other Goals:** Developments with the following public benefit features are eligible for a reduction in the minimum number of parking spaces, as follows:

Land Banking	Any development that sets aside an area to provide for the future construction of deferred parking spaces.	The area set aside is landscaped with site enhancement landscaping or pedestrian amenities. The applicant provides an alternate site plan for approval that accommodates the deferred parking, landscaping, pedestrian facilities, and other site elements that would be required by this title without the land banking.	The development may set aside the land area that would otherwise be needed in order to provide up to 25% of the number of required parking spaces.
Adaptive Reuse of Older Buildings	Any use located in the Neighborhood Development Contexts <sup>1</sup> , except not drive-through service or vehicle-related uses.	The development is a building expansion, alteration, or change of use, in an existing building that was originally permitted prior to June 13, 1978. The development does not convert housing units to non-residential uses.	Exemption from the first 10 percent increase in the total number of spaces required on the development site, for a maximum allowed exemption of five parking spaces. A parking reduction for adaptive reuse shall be used only once per individual building.
Historic and Cultural Landmark Preservation	Any use that involves preservation of a landmark listed in the Anchorage local landmarks register.	The development does not decrease the number of parking spaces that existed on the site as of <i>[effective date of this ordinance]</i> to less than the number otherwise required by this title.	25% reduction in the number of required parking spaces.

<sup>1</sup>Neighborhood Development Contexts include the Traditional Urban Neighborhood, Edge Urban Neighborhood, and Transit-Supportive Development Context areas delineated in section 21.15.015.

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34

**2. Parking Agreements**

All parking reductions require a written parking agreement between the property owner(s) and the municipality, subject to the following standards:

**a. Recordation**

The municipality shall record the parking agreement at the district recorder’s office as a covenant that runs with the land and is binding on the owner and all successors and assigns for as long as the required number of off-street parking spaces is not provided. Recordation of the agreement shall take place before issuance of an entitlement that is contingent upon a parking reduction.

**b. Content**

The format (template)of the parking agreement shall be provided by the municipality, as approved by the traffic engineer and director. The parking agreement shall guarantee installation and maintenance of any required improvements by the property owner, and the owner’s continued participation in any parking management strategy required for a parking reduction. The parking agreement shall be accompanied by a site plan showing the parking and facilities required for the parking reduction.

**c. Additional Content for Agreements Subject to Discretionary Review**

Discretionary parking reductions subject to 21.07.090F.8. shall be preceded by a formal letter requesting the reduction for concurrence by the traffic engineer and director. The letter shall include justification for the parking reduction, including any parking demand study if prescribed by the traffic engineer. The parking agreement shall include a contingency plan, and shall guarantee future implementation of the contingency plan by the property owner if so ordered by the traffic engineer. The contingency plan may include strategies such as:

- i. A deferred parking site plan;
- ii. Payment to the municipality for the full cost of providing the required parking;
- iii. Transportation demand management programs; or

1  
2 iv. Other parking management strategies identified in the parking reductions or  
3 alternatives.  
4

5 **d. Termination**

6 The municipality may terminate the parking agreement if it is demonstrated that the parking  
7 reduction does not support the parking utilization for the site, or if the development is in  
8 violation of the parking agreement. The traffic engineer or planning director may require a  
9 contingency plan per AMC 21.07.090F.1.c. and a new parking agreement, or the site may  
10 be required to provide all parking per AMC 21.07.090E. If for any reason the parking  
11 agreement terminates, owners and all successors and assigns who are parties to the  
12 parking agreement shall comply with all provisions of this title governing the required  
13 number of off-street parking spaces.  
14

15 **3. Calculation of Parking Reductions**

16 **a. Calculation of Multiple Reductions**

17 A development may be eligible for more than one parking reduction listed in this section  
18 21.07.090F. The total impact of multiple parking reductions on a development's parking  
19 requirement shall be the sum of the individual reductions.  
20

21 **b. Maximum Nondiscretionary Reduction**

22 A request for a greater than 25 percent reduction in the required amount of parking from  
23 one or more reductions listed in table 21.07-9 shall be subject to discretionary review by  
24 the traffic engineer and director, except that the following reductions listed in table 21.07-  
25 9 are not subject to this percentage limitation:  
26

27 **i.** Housing (Table 21.07-9).  
28

29 **ii.** Land Banking (Table 21.07-9).  
30

31 **iii.** Shared parking (21.07.090F.5.).  
32

33 **iv.** Off-site parking (21.07.090F.6).  
34

35 **c. Rounding of Fractional Reductions**

36 Rounding of fractional numbers shall occur only after subtracting the parking reduction(s)  
37 from the minimum number of required parking spaces, as provided in 21.07.090C.1.  
38

39 \*\*\* \*\*

40  
41 **4. Pedestrian Access Improvements Required** Developments shall improve pedestrian access as  
42 provided below, in order to be eligible for parking reductions. Industrial uses, public safety facilities,  
43 transportation facilities, and utility facilities are exempt from this section 21.07.090F.5.  
44

45 *(Note: Existing subsections a. through e. deleted and replaced or modified by the following.)*  
46

47 **a. Administrative Relief and Adjustment**

48 The traffic engineer and director may approve administrative relief or adjustments to the  
49 standards of this subsection F.4. for changes of use or modifications to existing buildings  
50 and sites, as part of the review and approval of a parking reduction, provided the applicant  
51 demonstrates the adjustment is necessary to compensate for some practical difficulty of  
52 the site, or some unusual aspect of the site not shared by landowners in general. The  
53 justification for the administrative adjustment shall be recorded as an appendix to the  
54 parking reduction agreement in 21.07.090F.2.  
55

- 1                   **b. Pedestrian Frontage Standard**  
2                   Developments shall meet the standards of section 21.07.060F., Pedestrian Frontage  
3                   Standard, except where not applicable pursuant to 21.07.060F.2.  
4  
5                   **c. Walkway and Sidewalk Access**  
6                   Developments shall comply with subsections 21.07.060E.2., *Sidewalks* if applicable, and  
7                   21.07.060E.4., *On-Site Pedestrian Walkways*.  
8  
9                   **d. Bicycle Parking**  
10                  Developments shall comply with subsection 21.07.090K., *Bicycle Parking Spaces*.

11  
12                  (*Note: Existing subsections 21.07.090F.4., Downtown through 21.07.090F.15., Housing Density, are*  
13                  *deleted.*)

14                  **5. Parking Reduction for Shared Parking**

15                  Where two or more land uses that have different peak parking utilization time periods share the  
16                  same parking facility, the total off-street parking required for those uses may be reduced, as  
17                  provided below.

18  
19                  **a. Shared Parking Standard Calculation**

20                  Where up to three separate land uses listed in Table 21.07-10, *Shared Parking Credit*,  
21                  share a parking facility, the total off-street parking required for those uses is eligible to be  
22                  reduced by the percentage factors shown in subsection b., Table 21.07-10, subject to the  
23                  standards in subsections d. through j. Where four or more land uses listed in Table  
24                  21.07-10 share a parking facility, the shared parking reduction is subject to discretionary  
25                  review and approval by the traffic engineer and director as provided in 21.07.090F.8.

26  
27                  (*Note: Existing subsection b. deleted and subsection c. re-numbered to b.*)

28  
29                  **b. Computation of Shared Parking Standard Calculation Method**

30                  The following steps shall be used to calculate a shared parking reduction. The Planning  
31                  Department shall maintain a publicly available worksheet form online that applicants may  
32                  access and use to run calculations. Calculation steps:

- 33  
34                  i. Determine the minimum amount of parking required for each use, as set forth in  
35                  Section 21.07.090E., *Off-street Parking Requirements*, using the calculation rules  
36                  established in 21.07.090C., *Computation of Parking and Loading Requirements*.  
37  
38                  ii. For each use select the appropriate matching land use category in table  
39                  21.07-10, Shared Parking Credits.  
40  
41                  iii. Multiply the minimum amount of required parking for each use, as set forth in  
42                  Section 21.07.090E., by the appropriate percentage shown in table 21.07-10, for  
43                  each of the eight time periods in the table, to estimate the typical parking demand  
44                  generated by that use during each of the eight time periods.  
45  
46                  iv. Add the resulting products from the uses for each of the eight columns (time  
47                  periods) Include each time period shown in the table, including during hours when  
48                  the proposed business will be closed.  
49  
50                  v. The highest sum among the eight columns becomes the shared parking  
51                  requirement. This represents the time period that is expected to generate the  
52                  highest total parking demand.  
53  
54

TABLE 21.07-10: SHARED PARKING CREDIT								
Land Uses <sup>1</sup>	Weekday Time Periods				Weekend Time Periods			
	7 am to 6 pm	6 pm to 1 am	1 am to 3 am	3 am to 7 am	7 am to 6 pm	6 pm to 1 am	1 am to 3 am	3 am to 7 am
Residential	65%	100%	100%	100%	75%	90%	10%	100%
Religious assembly	25%	50%	0%	0%	100%	50%	0%	0%
Childcare; K-12 school.	100%	20%	0%	0%	20%	10%	0%	0%
Health services	100%	30%	5%	5%	100%	0%	0%	0%
Assembly	100%	50%	5%	5%	100%	50%	5%	5%
Fitness center	90%	100%	25	60%	100%	100%	25%	60%
Movie theater	60%	100%	0%	0%	80%	100%	0%	0%
Bar or nightclub	40%	100%	90%	0%	50%	100%	90%	0%
Restaurant	80%	100%	50%	50%	85%	100%	25%	25%
Restaurant – drive-through	100%	90%	15%	15%	100%	80%	15%	15%
Office or financial; or Government administr.	100%	10%	0%	5%	15%	0%	0%	0%
Retail sales ; Personal services	100%	80%	0%	0%	100%	60%	0%	0%
Visitor accommodations (guest rooms)	75%	100%	100%	100%	75%	100%	100%	100%
Industrial service, manufacturing, or warehouse/storage	100%	10%	0%	5%	15%	0%	0%	0%

**NOTES:** <sup>1</sup> If one or more of the land uses proposed to make use of shared parking facilities do not conform to the land use classifications in this table, as determined by the director, then the applicant shall submit sufficient data to indicate the periods of peak parking demand for the uses. Based on this information, the traffic engineer shall determine the appropriate shared parking requirement.

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29

**c. Alternative Shared Parking Calculation**

The applicant may request a greater reduction in the total number of spaces required for two or more land uses where Table 21.07-10 does not adequately account for circumstances or mix of use types specific to the development, subject to review and approval by the traffic engineer and director as provided in 21.07.090F.8. The applicant shall submit a shared parking study following 21.07.090F.8.c. and:

- i. The most current published procedures of the Urban Land Institute or the Institute of Transportation Engineers; or
- ii. Other procedures based on industry data or other sufficient evidence and analysis of peak parking demand, as specifically approved by the traffic engineer.

**d. Distance to Parking Spaces**

Shared parking spaces may be located off-site, subject to the standards in this subsection 21.07.090F.5. Shared parking spaces for residential units shall be located within 500 feet of the dwelling unit entrance they serve. Shared spaces for other uses shall be within 800 feet of a primary entrance of the uses served. Distance shall be measured along the pedestrian connection in e.

**e. Pedestrian Connection**

Clear and safe pedestrian walkways conforming to the standards of 21.07.060E. shall connect the shared parking facility and the primary entrances of the uses it serves. The traffic engineer may require sidewalk or pedestrian street crossing improvements.

**f. Separation by Streets**

Separation of a use and its shared parking facility by a local street is allowed, subject to discretionary review and approval by the traffic engineer in 21.07.090F.8. Shared parking



spaces shall not be separated from the served use by a collector or greater classification street, unless approved by the traffic engineer with consideration of the ease and safety of pedestrian access, or as allowed by a comprehensive plan element specific to an area or district.

**g. Residential Neighborhoods**

A nonresidential use shall not participate in a shared parking facility located in a residential district, if the use is not permitted in the residential district.

**h. Instructional Signs**

The shared parking facility shall provide instructional signs on the premises indicating the availability of the facility for patrons of the uses it serves.

**i. Shared Parking Plan**

A shared parking plan shall be submitted for review and approval. The shared parking plan may be combined with other parking plans required by this title.

**j. Changes in Use or Shared Parking Facility**

Any subsequent change to the shared parking facility or any use type in the building(s) shall require a review for compliance with this section, including proof that sufficient parking will be available. Any change shall be approved, and if necessary a modification to the existing shared parking agreement shall be made, prior to the change being implemented.

*(Note: Existing subsection k., Expiration, is deleted.)*

**6. Parking Reduction for Off-Site Parking**

A use's required parking may be located on a lot different from the lot containing the use, as provided below.

**a. Off-site Parking – Nondiscretionary Approval**

A principal use is eligible for a non-discretionary parking reduction for off-site parking, provided the off-site required parking for the use is off-street, located on a lot not separated by a street from the principal use, and in conformance with the standards below.

**b. Distance to Off-site Spaces**

Off-site parking spaces for residential units shall be located within 500 feet of the dwelling unit entrance they serve. Off-site spaces for other uses shall be within 800 feet of a primary entrance of the uses served. Distance shall be measured along the pedestrian connection in c.

**c. Pedestrian Connection**

Clear and safe pedestrian walkways conforming to the standards of 21.07.060E. shall connect the off-site parking facility and the primary entrance(s) of the uses served. The traffic engineer may require sidewalk or pedestrian crossing improvements to enhance pedestrian safety or mobility to and from the off-site parking.

**d. Separation by Streets**

Separation of a use and its off-site parking spaces by a local street is allowed, subject to discretionary review and approval by the traffic engineer in 21.07.090F.8. Off-site parking spaces shall not be separated from the served use by a collector or greater classification street, unless approved by the traffic engineer with consideration of the ease and safety of pedestrian access, or as specifically allowed by an area-specific element of the comprehensive plan.

**e. Instructional Signs**

1 Instructional signs shall be posted on the principal site providing notice of the availability  
2 and location of additional parking. The off-site parking facility shall provide instructional  
3 signs indicating the availability of the facility for patrons of the uses it serves.  
4

5 **f. Residential Neighborhoods**

6 A nonresidential use shall not participate in an off-site parking facility located in a residential  
7 district, if the use is not permitted in the residential district.  
8

9 **g. Required Off-site Spaces to be Counted for One Use Only**

10 The off-site vehicle parking spaces shall not be required parking spaces for any other use,  
11 except as provided in 21.07.090F.5. (Shared Parking) or F.7. (District Parking).  
12

13 **7. Parking Reduction for District Parking**

14 The traffic engineer may reduce the minimum number of required off-street parking spaces  
15 for uses within the boundaries of a municipally recognized parking district (as defined in  
16 21.15.040) that provides off-site parking facilities to serve an area. To determine eligibility  
17 for this reduction or the size of the reduction to be allowed, the traffic engineer shall  
18 consider the relative distance to the use from the district parking facility and the factors  
19 listed in 21.07.090F.8.b.

20 **8. Discretionary Parking Reductions**

21 Subject to discretionary review and approval by the traffic engineer and director, applicants may  
22 request greater percentage reductions than shown in Table 21.07-9 and subsections F.5 through  
23 F.8, propose other types of parking reduction strategies besides those listed, or request departures  
24 from the specific standards for the parking reductions in F.5. through F.8. The applicant shall  
25 demonstrate the proposed reduction is appropriate based on the expected parking utilization rate  
26 of the development and the factors below.  
27

28 **a. Examples of Discretionary Parking Reductions**

29 Examples of other parking reduction strategies besides those listed that may be proposed  
30 include other shared vehicle programs (e.g., bike-share), other parking pricing strategies,  
31 other uses that have low parking utilization, or other transportation demand management  
32 (TDM) programs.  
33

34 **b. Discretionary Approval Criteria**

35 To determine eligibility for a discretionary reduction or the size of the reduction to be  
36 allowed, the traffic engineer and director shall consider factors such as:  
37

38 **i.** The characteristics of the proposed use, travel behavior and anticipated peak  
39 parking utilization by users, peak hours of use, and parking turnover rate;  
40

41 **ii.** The ability of the use to meet the parking requirement through other means, such  
42 as existing or potential shared parking agreements or other parking strategies;  
43

44 **iii.** Availability, proximity, and accessibility of alternative parking, such as any  
45 proposed shared, off-site, or district parking;  
46

47 **iv.** Impacts from users and employees on adjacent neighborhoods, properties, and  
48 streets;  
49

50 **v.** Strategies provided by the applicant to ensure employee and patron use of any  
51 shared, off-site, or district parking or parking demand management program;  
52

53 **vi.** The surrounding area's parking needs and parking availability; and  
54

1                   vii.     The surrounding area’s availability of pedestrian facilities and alternative modes of  
2 transportation.

3  
4           c.     **Parking Demand Study**

5           The traffic engineer may require the applicant to provide a parking demand study for  
6 discretionary reductions. The parking demand study shall be prepared in a form and  
7 manner prescribed by the traffic engineer, and meet the following standards:

8  
9           i.     The parking study shall demonstrate that any parking reduction requested will not  
10 result in users parking on surrounding properties or streets;

11  
12           ii.    The parking study shall demonstrate that the drivers accessing the use will be  
13 adequately served by the proposed parking due to project location, transportation  
14 characteristics of the persons residing, working, or visiting the site; and

15  
16           iii.   The parking study shall consider the type, intensity, and characteristics of each  
17 use and projected peak parking and traffic demand of drivers, including the use’s hours of  
18 operation.

19   \*\*\*     \*\*\*     \*\*\*

20  
21   **H.     Parking and Loading Facility Design Standards**

22   \*\*\*     \*\*\*     \*\*\*

23       **8.     Vehicular Access and Circulation – General**

24       Parking lots and structures access, layout, and dimensions shall be designed for a safe and orderly  
25 flow of traffic throughout the site, as provided in subsections 8. through 11. that follow.

26  
27       a.     **Circulation Patterns**

28       Internal circulation patterns and the location and traffic direction of all circulation aisles,  
29 driveways, and queuing lanes shall be designed and maintained in accordance with the  
30 municipal driveway standards currently established by the traffic engineer, and with  
31 accepted principles of traffic engineering and safety, per the traffic engineer’s review based  
32 on the current manuals of the Institute of Transportation Engineers and the Urban Land  
33 Institute, and the *Manual of Uniform Traffic Control Devices* or the successor documents.  
34 Circulation patterns within parking facilities shall be well defined with pavement marking  
35 and signage, curbs, landscaping, landscaped islands, and/or other similar features.

36  
37       b.     **Administrative Adjustments and Exceptions**

38       The traffic engineer and director may approve modifications from the standards of this  
39 section, provided documentation prepared by a licensed professional in the state of Alaska  
40 registered with the Alaska State Board of Registration for Architects, Engineers and Land  
41 Surveyors demonstrates the parking facility or driveway meets the intent of this section and  
42 the municipal driveway standards and that the change is appropriate. Approval shall be  
43 contingent on factors such as street classification, street typology, urban context, traffic  
44 volume and speed, curb return radii, street travel lane offset from face of curb, pedestrian  
45 and bicycle facilities, snow storage, driveway configuration and length, site and project  
46 characteristics, number of vehicles expected to use the driveway, and comprehensive plan  
47 policies.

48  
49       *(Note: Existing subsection 8.c., Parking Spaces Along Major Entrance Drives, is deleted.)*

50  
51       **9.     Vehicular Access and Parking Location**

52  
53       a.     **Street Access Location**

54       The number and spacing of driveways, including minimum distance from street  
55 intersections, shall be as provided in the *Municipal Driveways Standards* and this section.

1 Access to streets owned by the state of Alaska requires compliance with state driveway  
2 standards, department of transportation and public facilities approval and driveway permit.  
3

4 **b. *Alley Access Requirement (in Urban Neighborhood Context Areas)***

5 Where a residential use is served by an alley in a Traditional Urban or Edge Urban  
6 Neighborhood Context (21.07.015), direct vehicle access to the street shall be limited to  
7 the following:  
8

- 9 i. Driveway access to the secondary street frontage on corner lots, provided the  
10 driveway is not located within the primary front setback;  
11  
12 ii. Residential driveway access to the primary street frontage not exceeding one  
13 driveway no wider at any point that 12 feet, except that lot frontages 100 feet or  
14 wider may have one driveway per 50 feet of lot frontage.  
15  
16 iii. Additional driveway access to the primary street frontage for multifamily or  
17 townhouse developments may be allowed if the alley is unimproved, unmaintained,  
18 or physically inaccessible, and the traffic engineer determines that improvement  
19 and vehicle access is not feasible, or that use of the alley for access would create  
20 a substantial traffic impact or safety hazard. The additional driveway access shall  
21 be the minimum the traffic engineer determines is necessary to provide access for  
22 the development.  
23

24 **c. *Cross-Access and Shared Access with Adjacent Sites***

25 Parking facilities serving a site, whether located on that same lot or on an adjacent lot, may  
26 be connected by means of a common access driveway within or between the interior of  
27 such lots. Where a property receives its access to the street through a shared driveway  
28 with another lot, a shared access easement shall be provided on the plat, or a shared  
29 access agreement running with the land shall be recorded by the municipality, as approved  
30 and executed by the director, guaranteeing the continued availability of the shared access  
31 between the properties.  
32

33 **d. *Paired Residential Driveways Allowed***

34 Driveways for two single-family, two-family, or townhouse units may be attached side-by-  
35 side across a property line, provided the maximum combined width of the paved area is 24  
36 feet in the Urban Neighborhood Development Context Areas (21.07.015), and 32 feet  
37 otherwise (16 feet maximum on each lot), without a landscaped break.  
38

39 **e. *Access and Parking Location in Urban Neighborhood Context Areas***

40 In the Traditional and Edge Urban Neighborhood Contexts established in 21.07.015, the  
41 maximum percentage of the front setback area on a property that may be paved for vehicle  
42 access and parking shall be as shown in Table 21.07-12, provided that:  
43

- 44 i. Secondary frontages on lots with more than one street frontage are exempt;  
45  
46 ii. Shared parking courtyards meeting 21.07.060G.20. are exempt;  
47  
48 iii. Minimum driveway widths per subsection 21.07.090H.11.d. are allowed; and  
49  
50 iv. Administrative adjustments may be approved as provided in 21.07.090H.8.b.  
51  
52  
53  
54  
55

Uses	Traditional Urban Neighborhood Context	Edge Urban Neighborhood Context	
		with Alley Access (per 21.07.090H.9.b)	without Alley Access
Single-family, Two-family and Mobile-Home Uses	50% max.	50% max.	75% max.
Other Uses	50% max.	50% max.	65% max.

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38  
39  
40  
41  
42  
43  
44  
45

**10. Access to Parking Spaces**

**a. Access to Parking Spaces**

Each parking space shall open to a parking aisle or driveway of such width and design as provided in section 21.07.090H.12. to provide safe and efficient means of vehicular access with no more than a standard two-movement entrance or exit from the parking space and without having to move another vehicle. Stacked and tandem parking spaces are allowed exceptions as provided in 21.07.090H.12.

**b. Backing Distances**

Adequate ingress and egress to each parking space shall be provided without backing more than 25 feet.

**c. Dead-End Parking Aisles**

Any parking bay that does not provide two means of vehicle egress shall provide, at the closed end, an extension of the parking aisle at least nine feet in depth, designated and signed as a “No Parking” turn-around area. Dead-end parking bays longer than 100 feet shall be subject to discretionary review and approval by the traffic engineer.

**d. Vehicle Maneuvering**

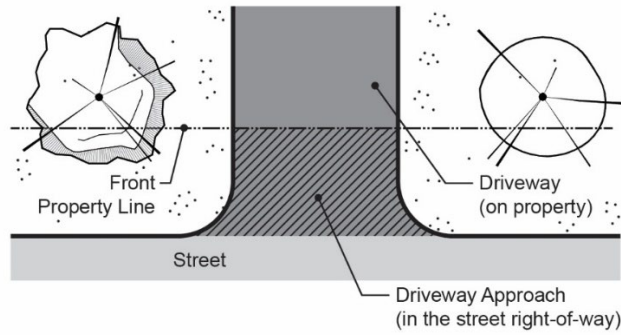
Off-street parking facilities shall be designed so that all vehicle maneuvers occur on the development site and not in the public right-of-way, and vehicles enter and exit the right-of-way in a forward motion, except the following are exempt:

- i. Parking for single-family, two-family, and mobile home dwellings on individual lots, accessing a local street.
- ii. For other developments, parking areas comprising only one or two parking spaces whose only access is to a local street, provided that the paved vehicle area(s) occupy no more than 20 feet of the width of the lot frontage in the front setback.
- iii. The usable portion of an alley may be credited as circulation and/or parking aisle space for parking areas with up to 4 spaces, provided there is at least 24 feet of maneuvering area between the end of each parking space and the opposite side of the improved alley. Additional parking spaces may be designed so that vehicles back out into an alley, subject to approval by the traffic engineer.

**11. Driveway Design and Dimensions**

**a. Parking Lot Entries/Driveway Approaches**

Entries and driveway approaches providing access from the street edge to the front property line shall conform to the municipal driveway standards and this section 21.07.090H.12. Access to streets owned by the state of Alaska requires compliance with state driveway standards as provided in 21.07.090H.8.



**b. Curb Openings and Public Walkway Crossings**

- i. Curb cut and curb returns at driveway openings to the street shall be provided as prescribed in the municipal driveway standards.
- ii. Public walkways shall be maintained or restored to the maximum running slope and cross-slope prescribed by M.A.S.S. and A.D.A., except that in the traditional urban neighborhood contexts (21.07.015), public walkways on local streets shall be restored to as close to a level running grade as practical.

**c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width**

*(Note: Deleted subsection i. and the subsection number/header for existing subsection ii.)*

The total width of the driveway approach from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the frontage if the platting authority or traffic engineer finds that conditions warrant it, provided the following:

- i. The driveway approach is always allowed to have the minimum driveway width provided by subsection d. The traffic engineer may approve more than the minimum driveway width, provided the Traffic Engineer determines that snow storage, traffic flow and safety, and the neighborhood context are addressed.
- ii. The driveway approach shall not exceed the maximum driveway width established in the municipal driveway standards.
- iii. Flag lots are exempt from the percentage limitations but shall have a maximum driveway approach width of 20 feet. Abutting flag lots may share a driveway approach up to 24 feet wide (12 feet per lot).

**d. Minimum Driveway Width**

The minimum required width of driveways, including both the driveway approach within the street ROW and the portion of the driveway on the development property, shall be the minimum widths established in the Municipal Driveway Standards, as modified and clarified by the following:

- i. The minimum width of a driveway providing access to a single-family, two-family, townhouse, or mobile home dwelling from a local street or alley is 12 feet.

1                   ii.       The minimum width of a driveway providing access for up to 10 parking spaces  
2                   serving a townhouse or multifamily residential use from a local street or alley is 12  
3                   feet (e.g., one lane for two-way traffic).  
4

5                   iii.       The minimum width of a driveway providing access for more than 10 parking  
6                   spaces serving a townhouse or multifamily use from a local street or alley is 12  
7                   feet for one-way traffic and 20 feet for two-way traffic.  
8

9                   e.       **Driveway Throat Length**

10                   The driveway throat shall be of sufficient length to enable the street and walkways in the  
11                   ROW and the circulation, parking, and walkways in the development site to function without  
12                   interfering with each other, as provided in the *Municipal Driveway Standards*.  
13

14                   f.       **Sight Distance**

15                   Ingress and egress to parking facilities shall be designed as prescribed in the municipal  
16                   driveway standards to maintain adequate sight distance and safety.  
17

18                   g.       **Circulation Definition**

19                   Curbed end islands shall be provided at the end of each row of parking spaces to define  
20                   circulation and provide sight distance at internal intersections of parking aisles, driveways,  
21                   and/or on-site roadways. Shared parking courtyards meeting 21.07.060G.20. may depart  
22                   from this requirement.  
23

24                   (*Note: Existing subsections 21.07.090H.8.e., Parking and Maneuvering, through 21.07.090H.8.h., Cross*  
25                   *Access and Joint Access with Adjacent Sites, are deleted.*)  
26  
27

28                   **12.       Dimensions of Parking Spaces and Aisles**

29                   The parking space and aisle dimensional standards of this section 21.07.090H.12. shall apply to  
30                   all parking except as stated otherwise in sections 21.07.090I., passenger loading zones, and  
31                   21.07.090J, accessible parking spaces. Parking dimensions and layout shall be as provided in  
32                   subsection a., including Table 21.07-13 and the figures that follow the table, with the additions and  
33                   exceptions provided in subsections b. through m.                   !

34                   a.       **Minimum Dimensions for Required Parking**

35                   Parking shall meet or exceed the STANDARD parking space and aisle dimensions set forth  
36                   in table 21.07-13, except as follows:  
37

38                   i.       Parking facilities in the Downtown (DT) zoning districts, and in the Traditional  
39                   Urban Neighborhood Contexts delineated in 21.07.015, may meet the SMALL  
40                   parking space and aisle dimensions in table 21.07-13. Up to 30 percent of parking  
41                   in the Downtown (DT) zoning districts may meet the COMPACT parking  
42                   dimensions, if signed for compact vehicles only.  
43

44                   ii.       In all other areas of the municipality, up to 50 percent of employee-only parking,  
45                   structured parking facilities, or parking for uses with a low parking turnover rate  
46                   may meet the SMALL parking space and aisle dimensions in table 21.07-13. Uses  
47                   with a low parking turnover rate comprise multifamily and mixed-use household  
48                   living uses, office uses, and educational facility uses other than instructional  
49

services. Required parking shared between low-turnover and non-low-turnover uses shall meet STANDARD dimensions.

TABLE 21.07-13: PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Size	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
90	STANDARD	9' 0"	9' 0"	20' 0"	23' 0"	24'	64' 0"	0' 0"	2' 0"
	SMALL	8' 6"	8' 6"	18' 0"	23' 6"	24'	60' 0"	0' 0"	
	COMPACT	8' 0"	8' 0"	16' 0"	23' 6"	24'	56' 0"	0' 0"	
75	STANDARD	9' 0"	9' 4"	21' 8"	19' 0"	24'	67' 4"	1' 2"	1' 11"
	SMALL	8' 6"	8' 10"	19' 7"	19' 6"	24'	63' 2"	1' 11"	
	COMPACT	8' 0"	8' 3"	19' 5"	19' 6"	24'	62' 11"	0' 6"	
60	STANDARD	9' 0"	10' 5"	21' 10"	18' 0"	24'	67' 8"	2' 3"	1' 8"
	SMALL	8' 6"	9' 10"	19' 10"	18' 6"	24'	63' 8"	2' 2"	
	COMPACT	8' 0"	9' 3"	17' 10"	18' 6"	24'	59' 9"	1' 0"	
45	STANDARD	9' 0"	12' 9"	20' 6"	12' 0"	24'	65' 0"	3' 2"	1' 5"
	SMALL	8' 6"	12' 0"	18' 9"	12' 6"	24'	61' 6"	3' 0"	
	COMPACT	8' 0"	11' 4"	16' 12"	12' 6"	24'	57' 11"	1' 5"	
0 (parallel)	STANDARD	9' 0"	23' 0"	9' 0"	12' 0"	24'	42' 0"	0' 0"	0' 0"
	SMALL	8' 6"	23' 0"	8' 6"	12' 6"	24'	41' 0"	0' 0"	
	COMPACT	8' 0"	23' 0"	8' 0"	12' 6"	24'	40' 0"	0' 0"	

\*\*\*      \*\*\*      \*\*\*

(Note: No changes proposed to the existing Title 21 figures (illustrations) that follow this table. Existing subsections 21.07.090H.12.a., b., c., d., and e. are renumbered to become subsections b., c., d., e., and f. No other changes are proposed to those subsections.)

\*\*\*      \*\*\*      \*\*\*

**g. Structural Columns**

A structural column may encroach into the width of a parking space by up to one foot, provided the remaining parking space width is no less than eight feet. Such column shall be located within four feet of either end of the parking space depth. Such column shall not be located within one foot of the drive aisle.

**h. Minimum Vertical Clearance**

(Note: Re-numbered. No other amendments are proposed to this existing subsection g. Existing subsection h. is deleted.)

\*\*\*      \*\*\*      \*\*\*

**i. Recreational Vehicle Spaces**

Parking spaces for recreational vehicles, if provided and delineated, shall be a minimum of 10 feet wide by 40 feet long.

**j. Stacked or Tandem Spaces with Attendant Parking**

The traffic engineer may approve reduced parking space dimensions for stacked parking spaces and/or tandem spaces in an attendant (valet) parking facility. An accessible passenger loading zone shall be provided with attendant parking services at or near a primary entrance. Notice of availability of valet service shall be conspicuously posted inside and outside the primary entrance. The business owner shall enter into a parking agreement with the municipality guaranteeing continuation of the attendant (valet) service.



1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38  
39  
40  
41  
42  
43  
44  
45  
46  
47  
48  
49  
50  
51  
52  
53  
54

**k. Stacked or Tandem Spaces for a Residential Dwelling**

Two required parking spaces for any residential dwelling may be arranged in tandem with a combined space depth (vehicle projection length) of 38 feet, or stacked one above the other using a car stacker, subject to the following:

- i. The parking required for the dwelling unit is arranged independently from parking serving any other dwelling unit; and
- ii. The owner assigns the two spaces to the same dwelling.

**l. Automated Parking Structures**

Automated parking structures are exempt from the parking space and aisle dimensions and vertical clearance requirements of this section 21.07.090H.12. They are instead subject to section 21.07.090M.10.

**m. Administrative Adjustments**

The traffic engineer may approve a lesser parking dimension than set forth in this section 21.07.090H.12., due to site topography, existing structures, lot configuration, or preservation of trees and vegetation, subject to a recorded parking agreement complying with 21.07.090F.2.

**I. Passenger Loading Zones**

All institutional, entertainment, and commercial uses such as schools/daycare, stadiums, and theaters that have high-volume peak traffic volumes shall provide an area for drop-offs and pick-ups that meets the following requirements:

**1. Passenger Loading Zone**

In addition to the required minimum number of parking spaces established in subsection 21.07.090E., the traffic engineer may require one or more passenger loading zone spaces, depending on the type, intensity, and traffic patterns of the proposed use. The passenger loading zone for large commercial establishments or other intensive uses may be required by the traffic engineer to include one or more spaces dedicated to taxi cabs, ride hailing services, and/or other specialized high occupancy vehicles.

**2. Passenger Loading Zone Dimensions**

Any passenger loading zone that is provided for a development shall consist of one or more passenger drop-off/pick-up spaces parallel to the driveway or circulation aisle adjacent to the building. Each space shall be a minimum of 20 feet in length and eight or more feet in width, with an access aisle at least five feet wide abutting the full length of the space. As an alternative, subject to approval of the traffic engineer, a passenger loading zone may consist of one or more parking spaces that meets the accessible parking space dimensional standards of subsection 21.07.090J.7. However, designated passenger loading zone spaces shall not count towards the accessible parking space requirement.

**3. Plan**

The parking facility layout, circulation, and design plan shall show the location and design of proposed passenger loading zones. For certain intensive uses, the traffic engineer may require the plan to include a traffic control plan addressing projected usage, hours of operation, peak loading/unloading time, plans for directing traffic, safety measures, and other information deemed necessary by the traffic engineer to designing a safe and well-functioning drop-off area.

**4. Accessible Route**

An accessible pedestrian route to the building or facility entrance shall be provided pursuant to subsection 21.07.090J.9., *Accessible Routes*.

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22

**5. Schools**

Passenger loading zones shall be required for schools (public or private). Length, location, and design of the passenger loading zones shall be approved by the traffic engineer.

\*\*\*      \*\*\*      \*\*\*

**K. Bicycle Parking**

**1. Bicycle Parking Required**

Table 21.07-14 establishes the minimum number of bicycle parking spaces required per principal use, and the percentage of these spaces required to be developed as long-term bicycle parking, versus short-term spaces. The required minimum number of spaces is calculated as follows:

- a. A minimum of two bicycle parking spaces shall be provided for each use in Table 21.07-14.
- b. After the first 40 bicycle parking spaces, no additional bicycle parking is required for any individual non-residential use.
- c. Section 21.07.090C., Computation of Parking and Loading Requirements, applies with respect to fractions, multiple uses, and building floor area measurements.

<b>TABLE 21.07-14: Bicycle Parking Spaces Required</b>			
Use Category/Type	Minimum Number of Bicycle Spaces		Minimum Percentage for Long-Term Spaces (for required bicycle parking in excess of 2 spaces)
	Areas with Area-specific Parking Requirements in Table 21.07-7	All Other Areas	
Multifamily and mixed-use dwellings	1 space per two dwellings	1 space per 10 dwellings	90 percent
Group living uses, Roominghouses, Homeless and transient shelters,	1 space per 5 beds	1 space per 5 beds	90 percent
Government administration & civic facility, Offices, Hospitals/Healthcare facilities,	1 space per 5,000 sf gfa	1 space per 10,000 sf gfa	75 percent
Cultural Facilities, Entertainment & Recreation Facilities	1 space per 10,000 sf gfa	1 space per 20,000 sf gfa	10 percent
Schools, Colleges,	4 spaces per classroom	2 spaces per classroom	25 percent
Restaurants, Bars	1 space per 3,000 sf gfa	1 space per 10,000 sf gfa	10 percent
Retail Sales, Personal Service, Repair & Rental,	1 space per 12,000 sf gfa	1 space per 24,000 gfa	10 percent
Industrial uses	1 space per 25,000 sf. gfa	1 space per 50,000	75 percent
Visitor Accommodations	1 space per 23,000 sf gfa	1 space per 23,000 sf gfa	75 percent
Uses not included above, except as provided in subsection K.2. below	Minimum of 2 spaces	Minimum of 2 spaces	Not required

23  
24  
25

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38  
39  
40  
41  
42  
43  
44  
45  
46  
47  
48  
49  
50  
51  
52  
53

**2. Exemptions**

The bicycle parking requirements in subsection 1. shall not apply to:

- a. Household living dwelling units with their own individual garages
- b. Airstrips.
- c. Utility Substations, Telecommunication facilities, and WECS.
- e. Natural resource extraction.
- f. Impound yards and storage yards.
- g. Any other non-residential use which has no regular employees or people on premises.

**3. Administrative Adjustments**

The director may approve administrative relief or adjustments to the bicycle parking requirement for changes of use or modifications to existing developments, provided the applicant demonstrates the adjustment is necessary to compensate for a practical difficulty of the site.

**4. Bicycle Parking Facility Development Standards**

These requirements shall apply to all required bicycle parking spaces, except as specifically provided otherwise. Only long-term parking spaces required by table 21.07-14 (in right-hand column) shall follow the requirements of subsection g.

- a. Required bicycle parking or a sign leading thereto shall be located in an area visible from a primary entrance area. Required bicycle parking other than long-term bicycle parking shall be located within 50 feet of a primary entrance as measured along an accessible pedestrian route.
- b. A required bicycle parking space shall be either a bike rack or bike locker. Each shall be a securely anchored, tamper-resistant structure. Bike racks shall allow the bike frame and one wheel to be locked to the rack with a standard U-type lock when both wheels are left on the bicycle. Bike racks shall support the bicycle frame in a stable position without damage to the bicycle. Horizontal bike racks (i.e., racks not mounted to walls) shall support the bicycle at two or more points.
  - i. Bicycle rack tubing shall be a minimum of ¼ inch diameter and maximum of 3-inch diameter.
  - ii. The topmost attachable portion of bicycle racks shall be 25 inches to 40 inches in height.

**4. Bicycle Parking Facility Development Standards**

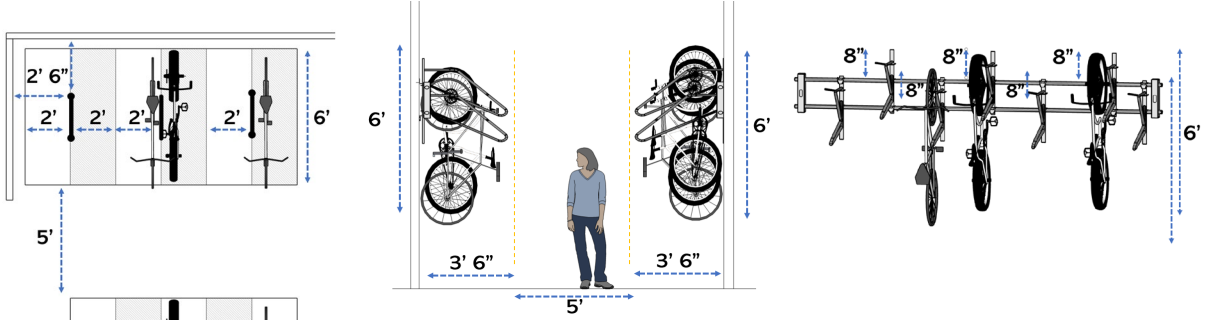
\*\*\*    \*\*\*    \*\*\*

- c. A required bicycle parking space shall be a minimum of six feet long and two feet wide, with the following exceptions:
  - i. Vertical bicycle parking spaces (securing the bicycle perpendicular to the ground) shall be a minimum of three feet six inches deep measured from the wall and six feet tall with a vertical stagger of eight inches between side-by-side spaces, as shown in the first set of figures below.

1  
2  
3  
4  
5  
6  
7  
8  
9

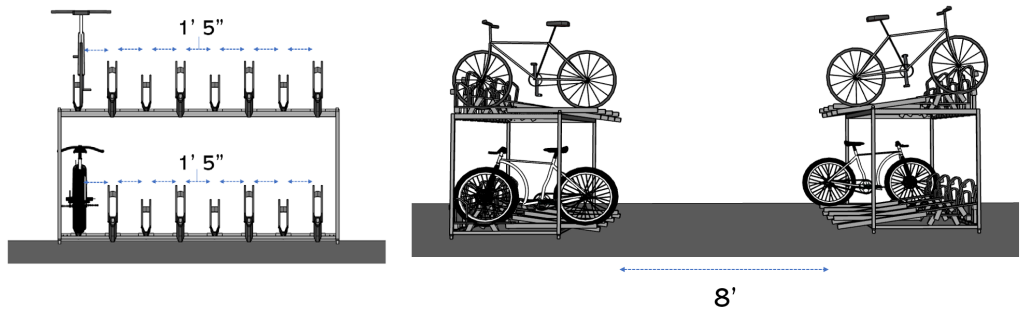
- ii. Triangle bike locker layouts are exempt from the two-foot minimum width however shall have a minimum length of six feet six inches and access doors at least two feet six inches wide.
- iii. Stacked bike parking spaces (stacking bicycles one tier on top of another) may meet the alternative stacked dimensions in the bottom figures provided there is a mechanically assisted lifting mechanism for mounting the bicycle on the upper tier.

**Dimensions for Horizontal/Side-by-Side and Vertical/Wall-Mounted Bicycle Spaces:**



10  
11  
12

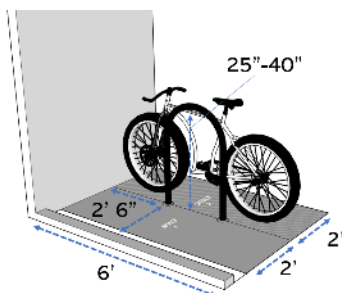
**Dimensions for Stacked Bicycle Spaces:**



13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25

- iv. Other deviations are permitted, per director approval, provided the applicant demonstrates the spaces meet the intent of the standard.

- d. Bicycle parking spaces shall be designed so that the bicycle may be added or removed without having to move another bicycle. There shall be a minimum of five feet of clear space inclusive of a walkway, behind the two-by-six-foot (or alternative dimensioned) parking space to allow room for bicycle maneuvering. There shall be a minimum of two-foot six-inch clearance from all sides of the bicycle rack to walls, fences, curbs, and vehicle parking and circulation areas, except that racks attached to walls have no required wall clearance.



- e. Areas devoted to bicycle parking facilities shall be hard-surfaced and designed and maintained to be clear of mud and snow.
- f. Bicycle parking shall not obstruct pedestrian walkways, building access, or other use areas.
- g. Long-term bicycle parking spaces are intended to accommodate residents, students, employees, and commuters who typically need to store a bicycle overnight or for an entire work shift or school day. Required long-term spaces shall comply with the following additional standards:
  - i. Long-term bicycle parking shall be located in well-lit, restricted access, lockable rooms or enclosures providing protection from theft, vandalism, and weather. Bicycle lockers, covered bicycle cages, or a secure room inside a building are common examples.
  - ii. Long-term bicycle parking spaces for residential uses may be located within dwelling units for first-floor dwelling units only, unless the unit is on a floor with an accessible elevator. Bicycle parking shall be located within 15 feet of the primary entrance and in a closet or three-sided alcove which allows a space 2 feet wide and 6 feet long. Long-term spaces shall not be located on a balcony, deck, or patio area accessory to a dwelling unit.
  - iii. A minimum of 30 percent of long-term spaces shall be ground-level, horizontal spaces.

\*\*\*      \*\*\*      \*\*\*

**21.07.110 Residential Design Standards**

**A. Purpose**

*(Note: Subsections 4. and 5. of the purpose statement for the residential design standards are deleted, and the remaining subsections re-numbered.)*

\*\*\*      \*\*\*      \*\*\*

**C. Standards for Multifamily and Townhouse Residential**

**1. Purpose**

\*\*\*      \*\*\*      \*\*\*

*(Note: Subsections c. and g. of the purpose statement for Multifamily and Townhouse Residential design standards are deleted, and the remaining subsections re-numbered.)*

\*\*\*      \*\*\*      \*\*\*

*(Note: The following subsections of the Multifamily and Townhouse Residential design standards are deleted: 21.07.110C.3., Windows Facing the Street; 21.07.110C.5., Pedestrian Access; 21.07.110C.6., Building and Site Orientation Menu; and 21.07.110C.9., Entryway Treatment, are deleted. The remaining subsections of 21.07.110C. are re-numbered accordingly.)*

\*\*\*      \*\*\*      \*\*\*

**D. Standards for Some Single-Family and Two-Family Residential Structures**

*(Note: Subsections 3., 4., and 5. are deleted.)*

\*\*\*      \*\*\*      \*\*\*

*(Note: Subsection 21.07.110E., Prohibited Structures, is deleted, and Site Design (below) is re-numbered.)*

**E. Site Design**

*(Note: Site Design subsections 3., Driveway Width, and 4., Alleys, are deleted.)*

1 \*\*\* \*\*

2 **21.11.070 Development and Design Standards**

3 \*\*\* \*\*

4 **F. Off-Street Parking and Loading**

5 \*\*\* \*\*

6 **3. Off-Street Parking and Loading Design Standards**

7 If off-street parking and/or loading is provided, it shall comply with all standards for off-street parking  
8 and loading in section 21.07.090 Small and compact parking space dimensions specific to the DT  
9 districts are provided in section 21.07.090H.12.

10  
11 *(Note: Subsections a. and b. and Tables 21.11-9 and 21.11-10 are deleted.)*

12 \*\*\* \*\*

13  
14  
15 **21.15.020 Rules of Construction and Interpretation**

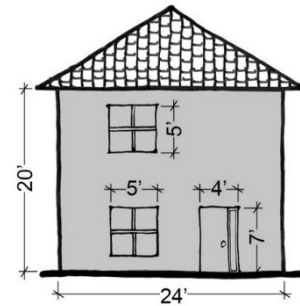
16 \*\*\* \*\*

17 **P. Measurement of Windows and Entrances as a Percentage of Building Wall Area**

18 The percentage of building elevation wall area that is window and entrance openings is determined by  
19 dividing the total square footage of the windows and primary entrances on the building elevation by the  
20 gross square footage of the building elevation wall area (except for parts of the building exempted in 3.),  
21 and then multiplying the resulting quotient by 100.

22  
23 **1. Illustrated Example**

24 In the illustration, the building elevation wall area is 20 feet of  
25 height x 24 feet of length = 480 square feet. The window and  
26 primary entrance area is the area of the two windows (5 x 5  
27 feet x 2) plus the area of the primary entrance and its sidelight  
28 (4 x 7 feet) = 78 square feet. The percentage of the building  
29 elevation wall area is found by dividing 78 by 480 then  
30 multiplying by 100, to yield 16%.



31  
32 **2. Measuring Window and Primary Entrance Area**

33 "Window area" is defined in section 21.15.040, Definitions. Primary entrance area includes the  
34 entrance opening and door frame but excludes trim, molding, and other features beyond the door  
35 frame. "Primary entrance" is defined in 21.15.040, Definitions.

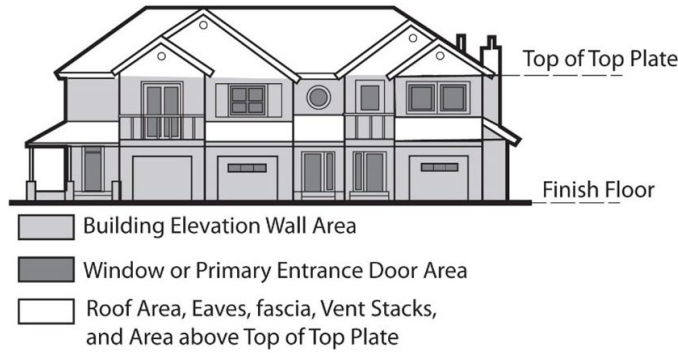
36  
37 **3. Exempt Parts of the Building Elevation Wall Area**

38 The building elevation wall area is measured as the exterior wall plane surface area from finished  
39 grade to the top of the wall on the building except that, for the purposes of measuring windows and  
40 entrances as a percentage of building wall area, the following parts of the building are not included  
41 in the wall area calculation:

- 42
- 43 **a.** Wall area above the topmost story's top plate, such as gable ends of a roof enclosing a cold attic, or parapet walls.
- 44
- 45
- 46 **b.** Roof surfaces, eaves, fascia, vent stacks, mechanical equipment and rooms, rooftop access enclosures, and other roof appurtenances.
- 47
- 48
- 49 **c.** Wall area of stories below grade plane, such as below grade parking, up to the finished floor of the story above grade plane.
- 50
- 51

1  
2

d. Parts of a street-facing building elevation below the grade plane of the abutting sidewalk (or edge of street pavement where there is no sidewalk).



3 \*\*\* \*\*

4

5 **21.15.040 Definitions**

6 \*\*\* \*\*

7 **Alley**

8 A permanent service right-of-way providing a secondary means of access to abutting properties.

9 \*\*\* \*\*

10 **Car Share Program**

11 A membership program that offers an alternative to car ownership under which persons or entities that become members are permitted to use vehicles from a fleet on an hourly basis, or in smaller intervals. Vehicles are available for pick-up by eligible members 24 hours per day. Automobile insurance and maintenance are covered by the car share program and individual members pay for usage and mileage fees.

15 \*\*\* \*\*

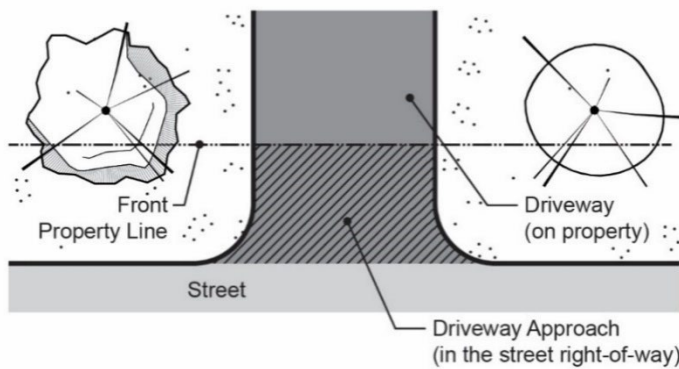
16 **Complete Street. See Street, Complete.**

17 \*\*\* \*\*

18 **Driveway Approach**

19 The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The driveway approach extends from the street edge to the front property line.

20



21 \*\*\* \*\*

22 **Parking Cash-Out**

23 A program offered by an employer that provides employees with a choice of foregoing any subsidized/free parking for a cash payment equivalent to the cost of the parking space to the employer

26 \*\*\* \*\*

27

1 **21.15.040 Definitions**

2 \*\*\* \*\*

3 **Parking, Unbundled**

4 Parking spaces that are leased or sold separately from the rental or purchase fees for the dwelling units or building  
5 floor area rather than included in the rental or purchase fees, such that potential tenants or buyers have the option  
6 of renting or buying a dwelling unit or building floor area at a lower price with fewer or no parking spaces. The rental  
7 or purchase of a parking space is at the discretion and direct cost of the tenant or buyer.

8  
9 **Pedestrian Feature**

10 A permanent object that provides pedestrians with increased convenience, comfort, and utility, and which is publicly  
11 accessible and not limited to a tenant or establishment such as seating for a restaurant. Pedestrian features include:

- 12 • Seating such as benches accommodating several people;
- 13 • Secondary/informal seating opportunities such as steps, pedestals, low walls, or edges of fountains,  
14 accommodating several people;
- 15 • A space for standing with objects to lean against, such as bollards, short fences, or irregular building  
16 facades, accommodating several people;
- 17 • A tree or raised planter;
- 18 • A work of art such as a water feature, sculpture, cultural exhibit, or clock feature;
- 19 • Pedestrian-scale lighting;
- 20 • A cross-country ski rack that can accommodate a ski lock and is located at building entrances within 500  
21 feet of a ski trailhead;
- 22 • A winter city urban design feature such as a wind screen, or outdoor stove or space heater; or
- 23 • Other object supporting pedestrian utility, such as a gazebo or kiosk.

24  
25 **Pedestrian Movement Zone**

26 The middle portion of an enhanced street sidewalk (21.07.060G.19.), comprising its walkway clear width located  
27 between the sidewalk’s street interface and building interface zones. The pedestrian movement zone provides for  
28 the primary function of sidewalks[,] and is kept clear of any obstructions to pedestrian movement.

29 \*\*\* \*\*

30 *(Note: The definition of Primary Pedestrian Walkway is deleted.)*

31 \*\*\* \*\*

32 **Ride Hailing Services**

33 A business that matches passengers with for-hire personal vehicles with drivers using software applications. Ride  
34 hailing services are different from other for-hire vehicles with drivers, such as taxicabs, in that they are not hailed  
35 from the street.

36 \*\*\* \*\*

37 **Street, Complete**

38 A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists,  
39 motorists, and public transportation users of all ages and abilities, and for all users to safely move through the  
40 transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a  
41 complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available  
42 on the municipal website.

43 \*\*\* \*\*

44 **Walkway Clear Width**

45 That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed,  
46 continuous (without interruption or being split by obstructions), and kept clear for pedestrian movement.

47  
48 *(Note: The definition reference “Walkway, Primary Pedestrian” is deleted.)*

49 \*\*\* \*\*

50  
51



1 **ANCHORAGE MUNICIPAL CODE OF REGULATIONS (AMCR) Title 21**

2 \*\*\* \*\*

3 **21.20 Regulations Governing Land Use Fees**

4 \*\*\* \*\*

5 **21.20.007 Schedule of Fees – Miscellaneous Fees**

6 The following fees shall be paid for the services described:

\*\*\* \*\*

<b>H. New and revised parking layouts:</b>	
1. Site plan review only	\$415.00
<b>2. Complete site plan review:</b>	
a. 1-10 spaces	\$415.00 plus \$16.50 per space
b. 11—50 spaces	\$515.00 plus \$12.50 per space
c. 51—200 spaces	\$620.00 plus \$8.25 per space
d. 201—1,000 spaces	\$1,035.00 plus \$2.05 per space
e. 1,001 + spaces	\$1,240.00 plus \$1.05 per space
<b>I. Agreements with the Municipality</b>	
Parking or access agreement	\$235.00 per agreement
Discretionary parking reductions review by Traffic Engineer (This fee does not apply to non-discretionary parking reductions listed in table 21.07-9.)	\$135.00/hour (two-hour minimum fee)
<b>H. Sign permits:</b>	
*** **	

7



Planning Department  
Long-Range Planning Division  
PO Box 196650  
Anchorage, Alaska 99519-6650

[www.muni.org/Planning/2040Actions.aspx](http://www.muni.org/Planning/2040Actions.aspx)